

Date June 28, 2010

APPROVING BEAVER AVENUE STREETScape CONCEPT PLAN

WHEREAS, on March 9, 2009, by Roll Call No. 09-367, the City Council approved a professional services agreement with Genus Landscape Architects, P.C. to provide a conceptual design for the Beaver Avenue Streetscape; and

WHEREAS, the Beaver Avenue Streetscape Concept Plan has been developed through public involvement and a steering committee comprised of Beaverdale business and neighborhood stakeholders, as well as City staff from various departments and

WHEREAS, two concepts, a narrowed intersection and a roundabout, were developed to improve pedestrian safety and vehicular circulation at the intersection of Beaver Avenue and Urbandale Avenue; and

WHEREAS, the Beaverdale Neighborhood Association and the Beaverdale Business Coalition have submitted letters in support of the roundabout concept and the Traffic Safety Committee recommended the roundabout as the preferred alternative for this intersection.

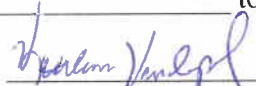
NOW THEREFORE BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DES MOINES, IOWA: That the Beaver Avenue Streetscape Concept Plan, including the construction of a roundabout at the intersection of Beaver Avenue and Urbandale Avenue as the preferred alternative, a copy of which is on file in the office of the City Engineer, be and is hereby approved.

(Council Letter Number 10-382 attached)

Activity ID 01-2009-015

Moved by _____ to adopt.

FORM APPROVED:


Kathleen Vanderpool
Deputy City Attorney



Funding Source: 2010-11 CIP, Page Street-6, Beaver Avenue Streetscape, STR245

COUNCIL ACTION	YEAS	NAYS	PASS	ABSENT
COWNIE				
COLEMAN				
GRIESS				
HENSLEY				
MAHAFFEY				
MEYER				
MOORE				
TOTAL				
MOTION CARRIED			APPROVED	

CERTIFICATE

I, DIANE RAUH, City Clerk of said City hereby certify that at a meeting of the City Council of said City of Des Moines, held on the above date, among other proceedings the above was adopted.

IN WITNESS WHEREOF, I have hereunto set my hand and affixed my seal the day and year first above written.

Mayor

City Clerk



June 17, 2010

Hon. Halley Griess, Ward 1
1628 37th Street
Des Moines, IA 50310

By email

Dear Councilman Griess:

As the Beaverdale streetscape steering committee continues to work on our streetscape project, the intersection of Beaver Avenue and Urbandale Avenue continues to be a focal point of opinion. We recently held two public meetings to update everyone on the project, provide another opportunity for individuals to ask questions and to solicit feedback on the project, especially related to the intersection design. The presentations were neutral related to the intersection.

On the morning of May 26th, the members of the steering committee, Genus Landscape Architects and several city staff members met with business owners from the neighborhood. Ignoring those associated with the project, we had 22 business owners on the attendance sheet. The attendance was larger than that so we missed a few people signing in. In this meeting we did bring up the SSMID and had examples of the additional costs that could be incurred following Ingersoll's formula based on property assessed value. After discussions related to the Beaver & Urbandale Avenue intersection (options: do nothing, narrowed intersection or roundabout), a vote by show of hands for what people preferred was conducted. The unofficial results were:

- 25 in favor of the roundabout
- 3 in favor of a narrowed intersection
- 0 in favor of keeping it as is

On the evening of May 27th, we held the second public meeting. The presentation by Genus was nearly the same as the day before. Not counting the city/design team, we had a rough head count of 45 individuals, with some individuals not staying through the whole meeting. Unfortunately our sign-in sheet had 28 names (I noticed your name was one of the missing signatures ☺). As you may recall, there was quite of bit of discussion related to intersection. When a hand vote was taken the results were:

- 32 in favor of the roundabout
- 4 in favor of a narrowed intersection
- 1 in favor of keeping it as is
- 2 or 3 abstain from voting

On June 3rd, the Beaverdale Neighborhood Association Board endorsed the concept of the roundabout for the intersection at Beaver and Urbandale. The Beaver Avenue Streetscape Plan will be presented at the June 28th City Council meeting. I imagine there will be a Council vote to approve the plan. If such a vote is necessary, the BNA hopes you vote in favor of the plan presented, including the roundabout concept for the Beaver/Urbandale intersection. If you have any questions, please feel free to contact me. Thank you for your time and consideration on this project.

Mike Kamper
President, BNA
Kamperdsm@yahoo.com
515-279-0410 (h)
515-283-5651

Cc:

Mayor Frank Cownie

Council members: Robert Mahaffey; Christine Hensley; Brian Meyer; Chris Coleman; Skip Moore

BNA Board

Brett Douglas, Genus Landscaping Architects

Beaverdale Streetscape Steering Committee

Kyle Larson, City Planner

**Beaverdale
Betterment
Coalition**

June 22, 2010

Hon. Halley Griess, Ward 1
1628 37th Street
Des Moines, IA 50310

By email

Dear Councilman Griess:

The Beaverdale Betterment Coalition (BBC), an organization representing the business community of Beaverdale, would like to take this opportunity to formally endorse the Beaverdale Streetscape Master Plan, including the roundabout intersection. We believe the plan represents an investment in the future of Beaverdale’s business core. The plan will address many of the concerns of our community regarding the current hardscape of Beaverdale. Additionally, a new streetscape will create a unique sense of place for our business community and in doing so drive business investment in our neighborhood.

The BBC identifies two major concerns in Beaverdale’s current streetscape. The first is safety. We believe the current intersection of Beaver and Urbandale is a safety hazard for drivers, pedestrians and cyclists. It is a difficult intersection for all of these users to navigate and it impedes the safe flow of traffic through that intersection. Throughout the business core of Beaverdale pedestrian crosswalks are difficult and identify and motorists routinely travel through these crosswalks at high speeds without regard for pedestrians. The BBC is confident the Master Plan will addresses these and other safety concerns identified through the public planning process.

The second significant concern for Beaverdale’s business community is traffic along Beaver Ave. In recent years we have seen an increase in the volume of traffic traveling through Beaverdale. Our primary concern is the speed at which this traffic moves and the resulting congestion. When traffic isn’t zooming past our businesses it’s often in a chaotic jumble as a result of confusion and poor functionality at the Beaver and Urbandale intersection. This prevents patrons of our area businesses from accessing parking and often creates a sour mood among those traveling into Beaverdale. The BBC is confident that the roundabout and other planned improvements to Beaverdale’s streetscape will help ease the flow of traffic through Beaverdale.

Lastly, we would like to draw your attention to the fact that as this project progresses the length of time for which business as usual is disrupted for our members will affect our ability to weather construction and emerge as healthy businesses. It is critical to the business community that this project is committed to a strict timeline and that the entirety of construction in any one phase of the project is well coordinated, well communicated and as concise as possible. The impact this project will have on the health of our businesses will, in our opinion, be very positive in the long term. We at the BBC are committed to working with the City of Des Moines to ensure the short term challenges to the businesses are minimized whenever possible.

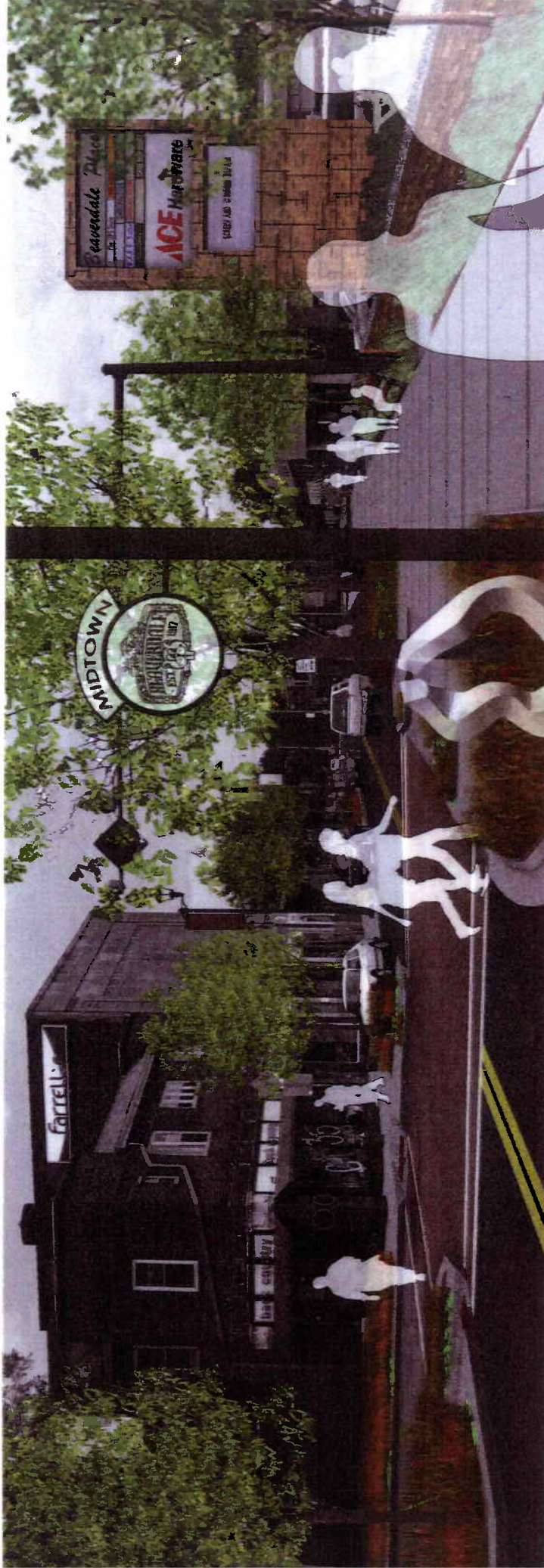
Thank you for your time and consideration,

Lexi Wornson
BBC President
Lexi.KW1@gmail.com
515-720-2013

Beaverdale Betterment Coalition • PO Box 13395 Des Moines, IA 50310

BEAVER AVENUE STREETSCAPE PLAN

DESIGN BY TOWN OF BEAVER 2010



gēnus
(landscape architect)

ACKNOWLEDGEMENTS



CITY OF
DES MOINES

**Beaverdale
Betterment
Coalition**



STEERING COMMITTEE

Kevin Currie, BNA/BBC
Dave Harmeyer, BNA/BBC
Mike Kamper, BNA President
Cork Smalley, BNA / BBC Design Committee
Delos Swanson, BNA

FUNDRAISING COMMITTEE

Mike Kamper, BNA President
Sheila Navis, BNA
Mary Anne Brekke, BBC
Lexi Wornson, BBC
Cork Smalley, BNA, BBC
Eric Stizmann, BBC

CITY OF DES MOINES

Matt Anderson, Economic Development Administrator
Bruce Braun, Street Maintenance Administrator
Gary Fox, City Traffic Engineer
Gary Hlavka, Sr. Civil Engineer
Darwin Larson, Engineering Design & Construction
Kyle Larson, City Planner
Kevin Moran, Senior Parks Planner
Erin Olson Douglas, Urban Designer
Mike Ring, Principal Traffic Engineer

CONSULTANT TEAM

Genus Landscape Architects - Planning + Design
Veenstra & Kimm - Survey + Traffic
Sayles Graphic Design - Branding



1 PLANNING PROCESS

- Planning Context
- Executive Summary
- Site Assessment
- Public Process

2 ORIENTATION AND FRAMEWORK

- Project Objectives
- Streetscape Elements
- Site Opportunities

3 PLAN RECOMMENDATIONS

- Plan Overview
- North Corridor
- Central Core
- South Corridor
- Plan Details

4 IMPLEMENTATION

- Streetscape Cost Opinion



City Hall Brick Stairs in Des Moines

It's difficult to find words that portray the Beaverdale Neighborhood character any better than those found on the official neighborhood website ...

"Welcome to Beaverdale"

... one of Des Moines' finest neighborhoods since the early 1920's.

Well-built, distinctive brick homes, tree-lined streets, wonderful parks and community spirit characterize the Beaverdale charm. Centrally located in the heart of Des Moines, Beaverdale thrives today as a diverse, well-maintained neighborhood and continues to attract new residents wanting a special place to live.

Beaverdale has long been a place where people come together for celebration, a glimpse into the neighborhood's heritage, and a taste of urban living. Year by year, the neighborhood continues to evolve and the commercial core strives to attract businesses that will serve the neighborhood and provide a place of destination for those outside the neighborhood boundaries.

As part of the overall revitalization effort, multiple streetscape planning efforts have been completed in recent years. Much of this work has become the foundation of the planning work included within this summary report. Extensive interaction with a steering committee composed of City of Des Moines leaders, local neighborhood association, business association, and Beaverdale residents was key in developing this plan.

The Beaver Avenue Streetscape Project is a one and a half mile corridor through Beaverdale, one of Des Moines' largest and most popular neighborhoods. With leadership provided by the City of Des Moines Planning and Engineering Department, in partnership with the Beaverdale Neighborhood Association (BNA) and the Beaverdale Betterment Coalition (BBC), the planning team was contracted to produce two specific design plans.

The first was a conceptual streetscape design for the Beaver Avenue Corridor from 41st Street to Douglas Avenue.

The second plan involved a more detailed evaluation of existing conditions, development of a preliminary design, and associated costs for implementation of proposed plans in the core business area between Beaver Crest Drive and Adams Avenue.

The Preliminary Plan addresses:

- Overall Plan Vision - including guiding principles and overall design concept.
- Public Input - multiple public input opportunities to ensure the resulting plan reflects users needs.
- Transportation - roads, transit, pedestrian, and bicycle facilities.
- Public Safety - enhanced safety features to make Beaverdale more walkable and even more pedestrian friendly.
- Implementation - project phasing, levels of enhancement, possible funding sources



To meet the goals and objectives outlined within this document, the Consultant Team provides the following recommendations to assist the City of Des Moines and Beaverdale Stakeholders with prioritization and implementation.

These recommendations are made with the understanding that the Beaverdale Business Community and Neighborhood will continue to evolve and that their needs will likely change over time. It is also understood that the streetscape plan will be implemented in phases, as determined by the success of private fundraising and applications for grant monies.

1. SAFETY ENHANCEMENTS

If there was one thing residents were consistent with in the public meetings, it was their desire for improved safety along Beaver Avenue. Given that a number of safety improvements are 'low hanging fruit' and the benefit is immeasurable, the following items are recommended as part of any future phases.

- painted crosswalks
- code compliant curb ramps
- curb bump outs at vehicular transition areas
- increased signal time at pedestrian crossings
- improved streetscape lighting
- reduced traffic speeds

2. URBANDALE AVENUE AND BEAVER AVENUE INTERSECTION

Centrally located as the primary crossroads of Beaverdale, this intersection is currently a missed opportunity and a challenge for pedestrians. While there are multiple solutions that would address safety and aesthetics, the proposed roundabout concept provides an image enhancement to the community core and a significant opportunity for improved safety on all levels - automobile, bicycle and pedestrian.



Existing streetscape, Beaver Avenue looking south

3. GREENING THE CORRIDOR

The residential side streets of Beaverdale are well known for their tree canopy and fall foliage. The experience for anyone traveling along Beaver Avenue will be greatly enhanced with the addition of new street tree plantings.

Existing trees that show signs of stress or damage from pruning around overhead utilities should be removed in the near future. It is important that new plantings be coordinated with underground and overhead utilities, and that necessary setbacks from curb lines be maintained to address snow removal concerns.

5. IDENTITY AND WAYFINDING

Over the years there have been a number of logos and graphic styles used to represent Beaverdale. As part of future streetscape phases, a singular logo and 'brand' should be developed for the Beaverdale Neighborhood. This identity piece can be used on items such as light pole banners, street signs, gateway markers, and promotional literature for special events.

In addition, the Beaverdale brand should be integrated to an outdoor wayfinding system that assists visitors and residents with location of commercial districts, business locations, bicycle and auto parking.

4. STORMWATER AND PAVEMENT

With it's position near the top of a large and important watershed, Beaverdale has a unique opportunity to be the first streetscape project in Des Moines to implement green infrastructure practices. Whether on a small scale or throughout the entire corridor, designed elements that enhance rain water infiltration, minimize runoff and slow peak flows are greatly needed in Beaverdale. Additional improvements to conventional storm piping may also be needed.

Along with the stormwater improvements, consideration should be given to improving pavement and curb sections to successfully accommodate surface water runoff and minimize ponding.

6. SITE FURNISHING PALETTE

As a means of adding character and pedestrian scale to the sidewalk environment, timeless site furniture should be added throughout the corridor. When possible, old furnishings that are inconsistent with the new palette, should be replaced with new standards. A relatively inexpensive addition, well-placed street furniture offers a 'livability factor' to an urban environment.

Elements addressed within the streetscape plan:

- Decorative Urns
- Benches
- Site Lighting
- Identity + Wayfinding
- Recycling Containers
- Litter Receptacles



The Beaverdale Neighborhood has long been anchored by a central business core surrounded by single family residential. The neighborhood road network consists of a uniform grid that is divided into quadrants, with the intersection of Beaver Avenue and Urbandale Avenue at the nexus. Along the Beaver Avenue study area there are three distinct districts, each separated by residential. Although there is inconsistency in the development of the commercial areas, the proximity of residential areas to commercial makes beaverdale a very walkable neighborhood.

The following are the significant findings of the site analysis process:

- Drainage [local and regional]
- Infrastructure
 - [storm sewer, sanitary, municipal utilities]
- Vegetation Mapping
- Pedestrian Movement
- Vehicular Circulation
- Overhead Utilities
- Identity and Wayfinding Signage
- Traffic and Parking Analysis
- Site Furnishings
 - [benches, litter receptacles, bike racks]
- Architectural Patterns
- Multifuse / Bicycle Trail Network



VEGETATION

- The landscape patterns along Beaver Avenue are closely linked to the development patterns along the corridor. In most cases, the residential districts are paralleled by lawn parkways. These parkways are well cared for and offer a nice buffer between street traffic and sidewalks. Plantings on residential properties provide a well established tree canopy. Existing plantings are predominantly maple and oak trees.
- Overhead utility lines are a constraint for existing and future plantings. Trees that have been severely pruned to avoid contact with power lines should be phased out and replaced with understory trees.
- Large surface parking lots exist throughout the study area. Plantings to screen parked cars is greatly needed. Low maintenance shrub plantings will add continuity and beauty to the neighborhood. Future plantings should be conscious of any maintenance and snow piling requirements.
- There is great potential for landscape improvements within the business districts. Individual businesses have started to place containers along storefronts in the core area. This spirit should be expanded and combined with a mix of tree canopy as part of future improvements.
- The median in Urbandale Avenue consists of an open lawn and tree canopy aesthetic. Noteworthy as a historical marker of the early trolley route, this median should be protected and referenced as part of future signage within the neighborhood. Additionally, existing trees should be evaluated by a certified arborist and tree succession plan created.

COMMUNITY IDENTITY

- When asked to describe themselves, the Beaverdale community sees itself as friendly, fun, artistic and family oriented. There is a general sentiment that the Beaver Avenue improvements should reflect these qualities and have the flexibility to cater to all demographics.



Pooling water at intersection & Beaver Avenue



Pooling water at 6th and 1/2 E. Beaver Avenue

DRAINAGE

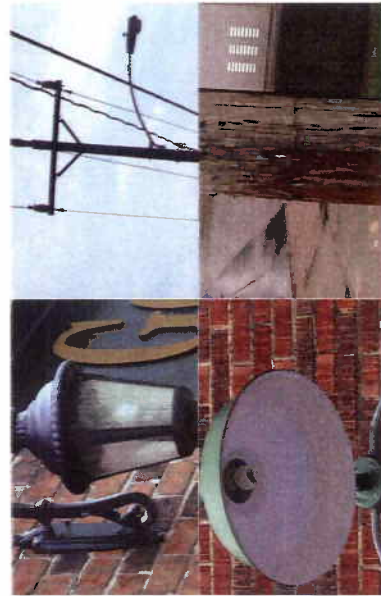
- In its existing state, storm sewer capacity and inlet capacity is not sufficient in multiple areas. A number of private parking lots and building rooftops currently contribute to unrestricted rainwater flow onto Beaver Avenue. Along the corridor, it is not uncommon for downspouts to be routed directly onto sidewalks, which typically sheet flow into the street. Collectively, this approach to site drainage places additional strain on a storm sewer system that is currently challenged.
- In some cases there have been multiple asphalt overlays along Beaver Avenue, reducing curb height and gutter capacity adding to the surface water problems in the district.
- Illustrated by the images above, drainage is particularly insufficient within the core area. This single rain event of just over an inch in several hours resulted in water reaching sidewalks and significant ponding at pedestrian crosswalks. It is worth noting that in many cases, surface water can be reduced with the addition of drain inlets or implementation of green infrastructure.



Examples of signage types along the corridor.



Cones, medians, and posts are being walks along the corridor.



Lighting examples along the corridor.

SIGNAGE & ORIENTATION

Beaver Avenue supports multiple commercial zones that value pedestrian circulation and relies on a variety of shared parking opportunities. There is great opportunity and a need to provide wayfinding signage. This type of signage should be an opportunity to direct people to particular businesses, off-street parking lots and neighborhood landmarks.

Existing signage typically falls into one of the following categories:

- IDENTITY
 - Building Signage - Perpendicular signage & Building Face Signage
 - Monument or Freestanding signage
 - Business signage varies in scale (vehicular vs. pedestrian)
- MUNICIPAL
 - Directional, transportation, safety, street signage
- NEIGHBORHOOD
 - Current neighborhood identity signage
 - Signage similar in style with red - brown color, house motif, text styles, and materials

PEDESTRIAN WALKS

Sidewalks are present along the entire Beaver Avenue Corridor. The condition of pavement varies greatly. Many walks along residential neighborhoods and portions of the business districts are as narrow as four feet. Where possible, replacement walks should be 5-6' in width. Current conditions in multiple areas contain walks with a 4' width which are too narrow for commercial areas. In nearly all instances, curb ramps and crosswalks need updated to meet current American with Disabilities Act (ADA) standards.

Integration of curb cuts and sidewalks is high on the priority list for improvements. In particular, there are a large number of driveways that intersect public walks. In many cases these crossings are in disrepair and/or uneven grades. In their current condition, the quantity and condition of curb cuts are a hazard to pedestrians and pose a significant challenge to automobiles as they enter and exit businesses.

LIGHTING

A hodge-podge of lighting exists along the Beaver Avenue corridor. The majority of street lighting along the corridor consists of utilitarian wood poles and cobra head style fixtures. The light source is generally high pressure sodium, resulting in an orange light source. While inexpensive and highly efficient, this style of street lighting is dated and does little to evoke character in Beaverdale and detracts from the walkable pedestrian nature of the area. Adequate light levels and consistent min./max. ratios should be part future lighting upgrades to ensure safety for all users.

The majority of lighting along Beaver Avenue is focused on lighting the streets and offers little value for the pedestrian experience.

A variety of privately owned building mounted light sources enhances the sidewalk experience. Lighting of this type is mounted too low in some locations and causes a hazard to pedestrians and an attractive nuisance for vandals.

Energy efficiency and dark sky standards should be considered for any exterior light modification.

SITE FURNISHING

Distribution of benches, litter receptacles, bike racks and planters throughout the central core are few and far between. Regular placement of site furnishings should be considered where ample concentration of users warrants. Existing conditions include:

BENCHES

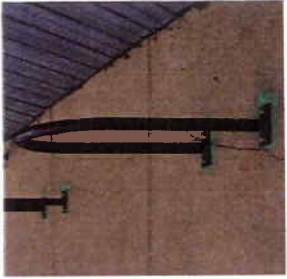
- Bench location is random and rarely corresponds with intensity of use.
- Bench model and color is inconsistent.

LITTER RECEPTACLES

- Litter receptacles in central core are in good condition
- Receptacle character is consistent with residential character and may be suitable for reuse as part of future improvements.
- Receptacles lack lids and are subject to the collection of rain and snow.

BIKE RACKS

- Traditional 'U' style bike racks have recently been placed throughout the central core.



Existing site furnishings: In core of district

ARCHITECTURAL PATTERNS

SOUTH DISTRICT

The south district contains a mix of single-family residential on the east side of Beaver Avenue and an eclectic blend of small and large businesses. The majority of the buildings in this district are in good condition. Future streetscape enhancements should strive to bring continuity to the various scales of architecture.



South District: Large brick building profiles

CENTRAL CORE

A remnant of the original Beaverdale business core, the central core contains a number of buildings that are in keeping with the spirit and character of the neighborhood. A few of the buildings are two story with a change of use or occupancy on the second floor. This type of development is the source of inspiration for recent successful redevelopment projects like Joe's Square. The old movie theater (currently Farrell's) provides a glimpse of the Art Deco architecture that exists sporadically throughout the neighborhood.



Central Core: Art Deco inspired building profiles

NORTH DISTRICT

The north district is dominated by one story commercial and office space. The majority of development is automobile oriented and demonstrates little architectural character. Many of the buildings are showing their age and redevelopment is likely, which offers great opportunity for an improved aesthetic and character. With redevelopment looming, aesthetic guidelines that address building materials, setbacks and aesthetic guidelines should be developed prior to redevelopment.



North District: One story building profiles





VEHICULAR

The north core is primarily commercial development and automobile oriented. Current uses include a gas station, multiple bar and restaurants and office space. The relationship between building and front property lines is rather suburban, with most properties being fronted by parking and little to no parking lot screening. Existing Beaver Avenue traffic is one lane each way with a center turn lane. A number of existing curb cuts are remnants from prior development and should be reexamined to improve safety and aesthetics along the corridor.

PUBLIC TRANSPORTATION

Public transportation currently exists along Douglas Avenue with stops just east and west of Beaver Avenue.

PEDESTRIAN

Sidewalks are uneven and narrow in width. Many are in poor condition, needing replaced.

OVERHEAD UTILITIES

Overhead utilities are in need of replacement, many poles are old and weathered. The power lines currently transect Beaver Avenue in multiple locations.

VEGETATION

The southern half is primarily residential with lawn parkway and front yards with abundant tree canopy. The northern half is nearly void of vegetation, which contributes to the lack of scale and disjointed feeling when travelling through the corridor.

BIKE LANES

Impromptu bike lanes exist along Beaver Avenue both north and south of Douglas Avenue. These lanes disappear upon reaching the intersection, and are not marked as bike lanes - resulting in a challenging condition.





VEHICULAR

Vehicular traffic in the central core area is consistently busy. The peak use is late afternoon with traffic backing up beyond the business core making it challenging to turn left off of Beaver. Curb cuts are located in close proximity to one another, with many that have little function. Parking is a perceived problem at peak hours of operation and during special events. However, there are a number of empty parking lots within one to two blocks of most businesses. Appropriate signage would go a long way in improving the perceived parking issue.

BICYCLE

Bicyclists travelling south of Urbandale Avenue can ride on sidewalks or within traffic lanes. A bike lane along Urbandale Avenue leads to the Inter Urban Trail.

PUBLIC TRANSPORTATION

Public transportation currently exists along Urbandale Avenue with stops just east and west of Beaver Avenue.

PEDESTRIAN

Sidewalks along the east side of Beaver Avenue from Joe's Square to The Backroom are in fair to good condition and need little to no improvement. The majority of sidewalks along the west side of Beaver Avenue need replaced.

Pedestrian crossing is challenging given the traffic volume. Safety enhancements are needed to improve the walkability of the core area.

OVERHEAD UTILITIES

The overhead utilities in the core area serve a large number of businesses, with many of them operating on a higher end service, which is more expensive to relocate or bury. Where possible, utility lines crossing Beaver Avenue should be reinstalled below grade. If budget permits, lines along Beaver Avenue should be reorganized and/or buried.

VEGETATION

With the exception of residential properties and the very southern tip of the central core, this area is nearly void of vegetation. Trees and ornamental plantings have been added as part of recent development projects. Similar improvements should carry forward with future improvements along Beaver Avenue.

STORM SEWER

Current infrastructure is minimal throughout the core area, with the greatest challenge being placement of grates and deteriorating pavement. Additional attention is needed to resolve rain water ponding after one inch rain events. Green infrastructure practices may help resolve drainage challenges in areas that do not have municipal storm sewer.







VEHICULAR

The intersections of 41st and Beaver and Beaver and Franklin are two intersections that many Beavertale residents try to avoid due to the nonstandard configuration of the 41st Street intersection and the short distance between 41st Street and the Franklin Avenue intersection. While a number of residents note a high accident rate at the 41st Street intersection, official crash data does not warrant a reconfiguration at this time.

BICYCLE

There currently are no Bicycle lanes present in the south core.

PUBLIC TRANSPORTATION

Public transportation occurs along Franklin Avenue with DART route stops both east and west of Beaver Avenue.

PEDESTRIAN

Sidewalks are narrow in width or do not exist. Sidewalks are in fair to good condition near Dahl's Grocery, while the remainder of walks are in fair to poor condition.

Pedestrian crosswalk markings and signalization needs updated to create a safer environment.

OVERHEAD UTILITIES

Overhead utilities are in need of replacement, many poles are old and weathered. Future phases should consider burial of power lines that transect Beaver Avenue.

VEGETATION

The east side of the south core is predominantly residential and well landscaped. As trees mature, many will be in conflict with existing utility lines, as noted by several that have already been heavily pruned. Along the west side, the presence of overhead lines has once again limited the opportunity for plantings. While many mature trees exist within fifty feet of Beaver Avenue, very few exist in the first twenty five feet, which is the more visible zone to those traveling the corridor. The west side has a large number of parking lots, most of which contain no screening. A skillful planting plan will go a long ways on the south core zone of Beaver Avenue.

BEAVER AVENUE STREETSCAPE PROJECT

Come see initial plans for the Beaver Avenue corridor... these were developed with your input at past meetings!

WHEN: Tuesday, August 18, 2009
5:30 – 8 PM Open House

WHERE: NW Community Center
5110 Franklin Avenue



Photo: Beavertown Input Meeting

Stakeholder input guided the project process throughout planning. The most prominent sources of input and direction included the project steering committee and Beaverdale residents at public forums.

STEERING COMMITTEE

In addition to the planning team, the steering committee consisted of City of Des Moines leadership from numerous departments and representatives from the Beaverdale Neighborhood Association (BNA) and Beaverdale Betterment Coalition (BBC). This group met five times during the planning process to provide leadership and guide key decision-making.

The steering committee identified the following as top priorities for the streetscape planning process:

- Traffic, transportation, parking
- Accessibility
- Green design / sustainability
- Generating enthusiasm / project support
- Economic Feasibility
- Gateways

PUBLIC INPUT MEETING #1 June 25th, 2009

Public input forums were held at the Northwest Community Center. The meetings were open house format with various stations for input. The first meeting allowed visitors to spend time at five stations. Each station was led by a member of the planning team.

KEY TOPICS DISCUSSED

- Branding – How does Beaverdale identify with itself?
- Neighborhood Gateways – Location and Character.
- Traffic + Infrastructure – Identify successful and unsuccessful locations
- Beaver Buck – Identify top priorities to allocate funds
- Site Furnishings – Identify site furnishing families preferred by local residents



WHAT WE HEARD

Branding

- Beaverdale identifies itself as friendly, residential, established, well-kept, and welcoming as a community.

Neighborhood Gateways

- New markers should resonate the character of Beaverdale utilizing brick and limestone.
- Gateways should display brick - "with a twist" and possibly metal.
- Gateway design should be timeless.

Parking + Infrastructure

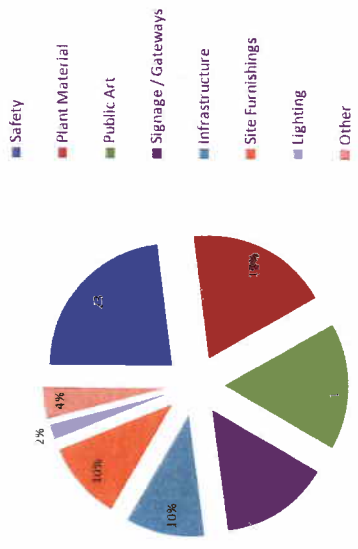
- Pedestrian safety needs improvement - #1 issue.
- Pedestrian crosswalks. Automobile traffic at the intersection of Beaver Avenue and Urbandale Avenue is busy and long crosswalks are hard to navigate.
- Bike lanes along Beaver Avenue would be desirable.
- Reduce traffic speed through core of Beaverdale and near Beaver Bend.
- Longer pedestrian crossing times at signaled intersections.
- Aesthetic improvements (hardscape / sidewalks / plantings).

Site Furnishings

- Custom and historic styles were the preferred style of furnishings.

Beaver Buck

- Participants were asked to place their "beaver buck" where they felt their money would be best allocated within the streetscape. The results of the public input are noted below.



BUSINESS STAKEHOLDER OPEN HOUSE

August 13, 2009
 An open house was held with business stakeholders to begin a dialogue for modifications near their respective businesses and address any concerns of the future streetscape implementation. Feedback from this session was positive and the majority of the stakeholders present were in support of suggested improvements displayed within the preliminary plans and imagery.

PUBLIC INPUT MEETING #2

August 18, 2009
 A second public forum was held at the Northwest Community Center to receive feedback on design concepts. The event combined three hours of open house, a written survey and two formal presentations by the planning team. Topics discussed included; a summary of information from the previous stakeholder meetings, corridor planning between 41st Street and Douglas Avenue, preliminary design concepts for the area between Beavercrest to Adams Avenue, neighborhood gateway imagery, and preliminary cost structure.

WHAT WE HEARD

Gateways

- The gateway markers need to better reflect the brick character of Beaverdale.
- The vertical and horizontal concepts are appealing based on location, however the text is hard to read from a distance.
- The brick markers are great, however red brick is more significant to the character of Beaverdale.

Preliminary design plans (Beavercrest to Adams Avenue): Improve plantings, implement the roundabout, bury utilities, safe pedestrian crossings, create rain gardens and green infrastructure, build new civic spaces.

if redevelopment were to occur in Beaverdale

- More outdoor seating and cafes.
- More entertainment district restaurants and shops.
- New specialty shops.
- Civic spaces for social gatherings.

The Intersection of Urbandale and Beaver Avenue

- The large majority of input received was in favor of the roundabout.

BEAVERDALE NEIGHBORHOOD MEETING

May 11, 2010
 A meeting was held in conjunction with the neighborhood association meeting to present the streetscape concepts to the neighborhood to gain feedback and discuss the next steps.

BEAVERDALE BUSINESS MEETING

May 26, 2010
 A second public business meeting was held at First American Bank to receive feedback on the preferred intersection concept. Topics discussed included; planning process overview, preliminary designs, project phasing + preliminary costs, SSMID, preferred intersection concept poll. Results from the poll were: Narrowed Concept - (3). No Change - (0), Roundabout - (25).

BEAVERDALE INPUT MEETING #3

May 27, 2010
 A third public meeting was held at the Northwest Community Center to receive feedback on the streetscape plans, costs, and intersection concepts. Topics discussed included; planning process overview, preliminary designs, project phasing + costs, intersection improvement options, and a poll for preferred intersection concept. Results from the poll were: Narrowed Concept - (4). No Change - (1). Roundabout - (32).







EMPHASIZE PEDESTRIAN FRIENDLY ENVIRONMENT



CREATE A STRONG SENSE OF PLACE - FOR RESIDENTS AND VISITORS

REVITALIZE BUSINESS DISTRICTS ALONG THE CORRIDOR

ENHANCE AND LINK RECREATIONAL TRAIL ROUTES



IMPROVE SIGNAGE AND WAY-FINDING

SECURE FINANCING FOR IMPROVEMENTS



Streetscape elements are the physical features that help to create character and appeal to a streetscape environment, while fulfilling the day-to-day needs of users.

The streetscape elements addressed are:

- Site Paving
- Plant Material
- Site Lighting
- Litter Receptacles
- Benches
- Bike Racks
- Public Art

These components are described in terms of materials, color, finish and in some cases, size. It is important to note that improvements beyond the City right-of-way are beyond the scope of the project and will be addressed as funding allows and as desired by individual property owners.



SITE PAVING

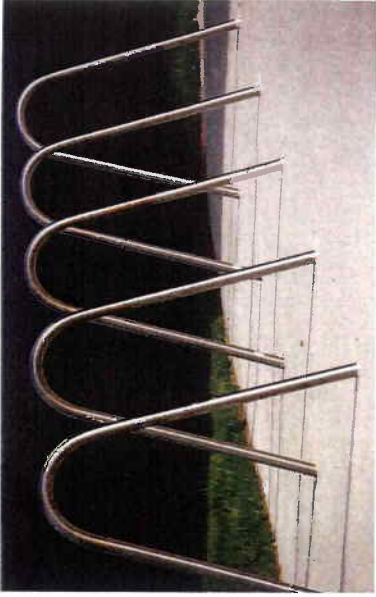
Standard Palette

Road surface : Bituminous Concrete

Sidewalks: Portland Cement Concrete

Crosswalks: Colored Concrete or Painted Asphalt (reflective paint)

Enhanced paving: clay brick



BIKE RACKS

Bicycle racks encourage ridership and foster a bike friendly environment. Given Beaverdale's proximity to trails and popularity amongst bicyclists, bike racks should be included throughout streetscape improvements. When possible, rack locations should be integrated with outdoor seating areas and/or bike parking zones.

Landscape Forms / Bola / Color
Color: Black



PLANTERS

Above grade planters provide opportunity for the addition of rich color and texture throughout the seasons in lowa. In order to be successful year-round, plantings should be replaced 2-3 times annually. In order to address cost sharing for maintenance and planting rotation, a partnership between the business community, neighborhood association and the City of Des Moines is recommended.

Longshadow - Riverside Prairie Planter 43, LS 9443
 17" height / 43" diameter / 19" square base
 Material: Dry-cast Limestone

Longshadow - Highwood Planter
 18" height / 34" diameter / 17" square base
 Material: Dry-cast Limestone

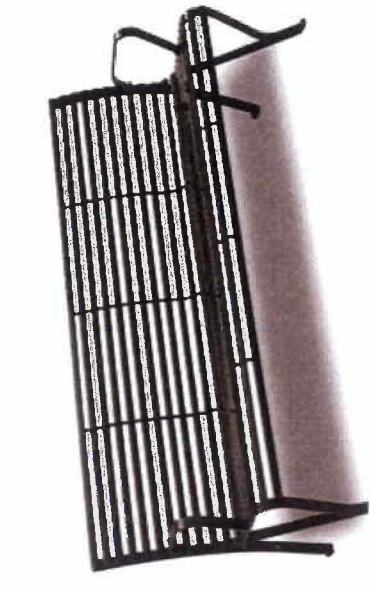


LITTER RECEPTACLES

Litter receptacles are recommended at key locations along the Beaver Avenue corridor. In particular, receptacles should be considered near seating areas and near busy intersections. In an effort to encourage recycling, the neighborhood may want to consider occasional placement of recycling centers. Recycling centers may include pedestrian scale containers.

Note: The business community has recently purchased litter receptacles for the core business district. With the addition of lids to minimize rain water from entering the receptacle, these receptacles could become the streetscape standards.

Landscape Forms / Scarborough
 Color: Black



BENCHES

A wide variety of styles and seating options exist.

The majority of input received during public meetings indicated that a traditional style bench was preferred by the Beaverdale Neighborhood. Additional commentary suggested that residents wanted Beaverdale's bench to be a custom design. In other words, unique to Beaverdale.

The use of recycled material or custom detailing within a manufactured bench could easily provide a unique character, while offering a long lasting and proven seating option.

Landscape Forms / Village Green or Parc Vue
 Color: Black

Equiparc / EP1650 / Recycled Plastic Slats

LIGHTING

Pedestrian scale and vehicular scale lighting is an important element to streetscape character and safety.

Install additional street lights where lighting is insufficient.

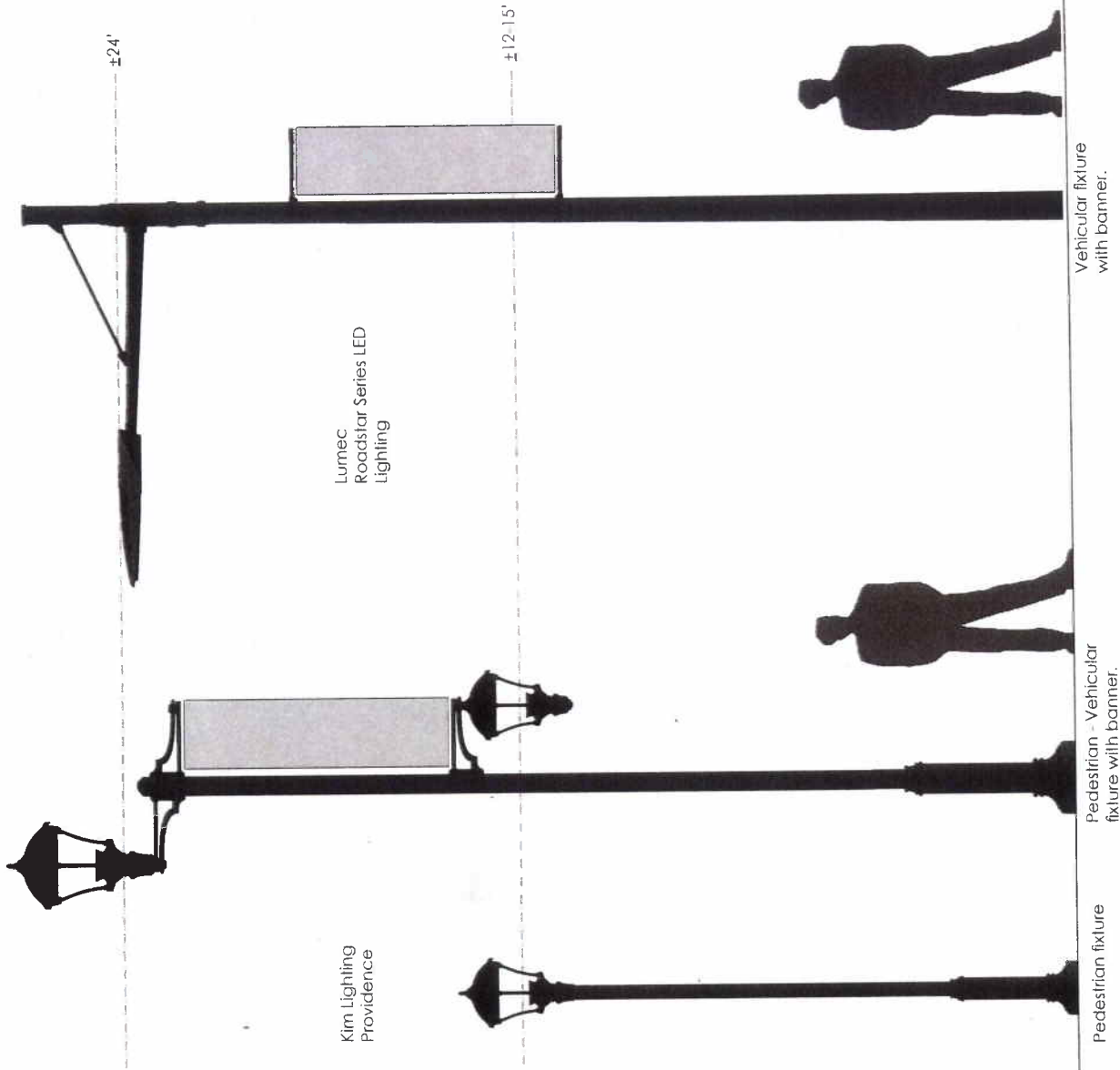
New design standards should be articulated by geometry, form, and of a character that reflects Beavertdale. Lighting of this nature will provide visual vitality to the streetscape

Consider energy conservation when selecting fixtures. LED (light emitting diodes) street and pedestrian lighting should be considered during the next phase of design work. A number of manufacturers are beginning to promote new fixtures and technologies that may make LED a viable option for the Beaver Avenue Streetscape.

Illumination levels should be differentiated by location, context and use. For example, light levels along the residential neighborhoods may be less than light levels along the commercial districts.

A variety of privately owned building mounted light sources currently enhance the sidewalk experience. Lighting of this type should be placed at a height above eight feet so that fixtures are not an impediment to sidewalk users.

Lighting installed along the Beaver Avenue Corridor must comply with MidAmerican Energy standards.



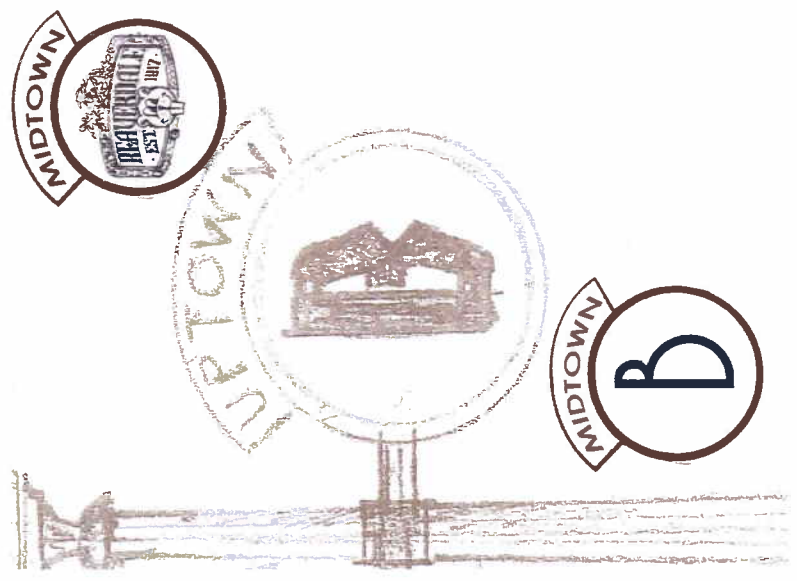
Kim Lighting Providence

Lumec Roadstar Series LED Lighting

Pedestrian fixture

Pedestrian - Vehicular fixture with banner.

Vehicular fixture with banner.



IDENTITY + ARCHITECTURAL SIGNAGE

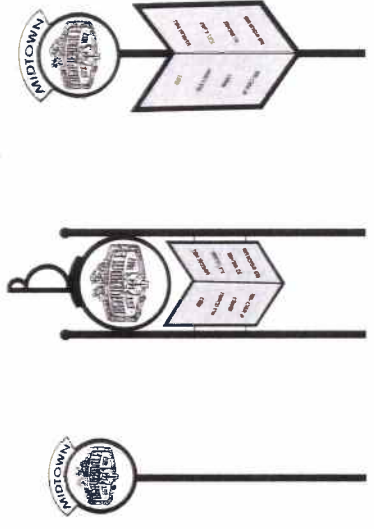
Signs that are physically attached to buildings should reflect the character of the architecture and/or image of the occupant. Where applicable, uniform dimensions throughout business districts will help create consistency. Public signage should work with the material and color palette of the streetscape.

- Identity Signage Opportunities
- Standards for Storefront Signage
- Standards for Store Signage (Automobile oriented)
- Neighborhood Gateway Markers
- Custom Street Signage
- Beaver Avenue District Designations

WAYFINDING SIGNAGE

Beaverdale could benefit from a communication systems that helps direct people as they travel through the corridor, approach businesses, or look for public parking. These signs should be located in the streetscape or parkway at key entrances or nodes in each business district so that they are easily visible to pedestrians and motorists.

- Wayfinding Signage Opportunities
- Custom Street Signs
- Parking Identification (off street)
- Kiosk Signs for Businesses
- Custom Street Signage

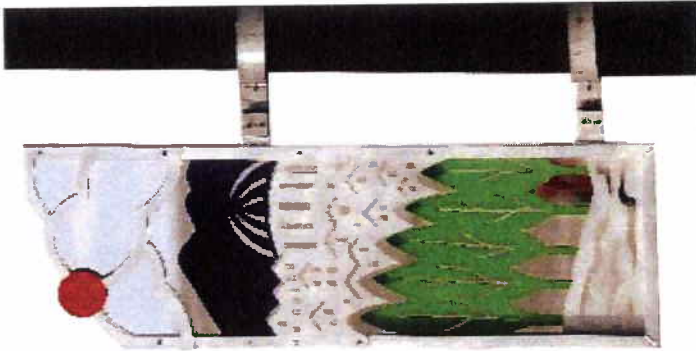


NEIGHBORHOOD BRANDING

Create a signature style and common logo and font for Beaverdale.

Branding Opportunities

- Neighborhood Gateway Markers
- Litter Receptacles / Benches
- Banners
- Neighborhood Logo - to be used consistently
- Beaver Avenue District Designations



Neighborhood District Signage
Custom Designed Street Light Armature
Artist Unknown



Public Art



Public Art Bike Rack



Water inlet into green infrastructure



Green infrastructure

GREEN STREET INFRASTRUCTURE

By transforming impervious pavement (typically asphalt or concrete) into vegetated areas, a significant amount of rain water runoff can be handled at the source, reducing water run-off that is especially problematic along Beaver Avenue. These planting areas will use soil and vegetation to absorb water. The plant roots help water soak into the ground, while the plant roots and soil bacteria help break down stormwater pollutants. Roots, insects and worms increase the space between soil particles and increase stormwater storage. When planted and maintained correctly, green infrastructure plantings can be a functional amenity.

PUBLIC ART OPPORTUNITIES

Opportunities for public art exist any place you look. While Beaverdale residents often noted traditional materials and style in their preferences, there was a strong voice for public art and unique detailing. Depending on the scale and budget of the streetscape project, there are a number of opportunities to integrate public art. Whether they are simple site installations or neighborhood specific collaborations with public artists, the creative spirit of Beaverdale should not be forgotten in forthcoming design efforts.



PEDESTRIAN SAFETY IMPROVEMENTS

Safety throughout the Beaver Avenue corridor was the number one concern expressed by residents. There are a number of improvements that should be a part of phase one improvements.

- Safety Enhancement Opportunities:
- Bulb-outs
 - Medians
 - Pedestrian median refuges
 - Painted or Paved crosswalks
 - Narrowed traffic lanes
 - Pedestrian crossing signals
 - Enhanced signage



PLANT PALETTE

While a strong tree canopy exists in residential zones, unfortunately the landscape plantings have been all but removed in the commercial districts of Beavertdale. Using trees and ground plane planting areas to create space and add life to the street environment is a high priority of the streetscape vision.

Key Considerations:
 Given Beavertdale's narrow sidewalk areas, columnar varieties will help to establish a tree canopy in restricted areas.
 Maintenance plays a key role in a healthy landscape. Maintenance requirements and strategies should be reviewed with the City of Des Moines for all plantings within the right of way along Beaver Avenue.



BRICK PLANTER PIERS



Proposed in the commercial core and at key intersections, a two - three foot high markers function as a base for planted urns, opportunity for neighborhood identity, and as a vertical expression of civic pride. Locations of markers will require additional study to ensure that no conflict exists with parked cars and intersection sight lines.

The sketch above proposes a brick pier inspired by the colors and textures of 'Beavertdale brick' and a limestone cap. The iconic 'B' appears as a void in painted steel, or as a custom ceramic inset. Plaza LT is recommended as the font type for neighborhood branding, particularly when used for the 'B' or 'Beavertdale'.





As defined by the project scope, a conceptual streetscape design is proposed from 41st Street to Douglas Avenue. Both of these design phases consider existing site conditions, current and future projects, public input, user need, maintainability, and previous planning studies.

The large corridor plan is based on typologies or specific site components. They are broken into four streetscape categories and two intersection components.

INTERSECTION IMPROVEMENTS

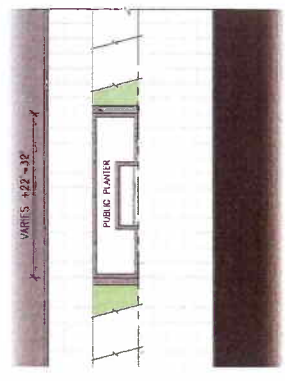
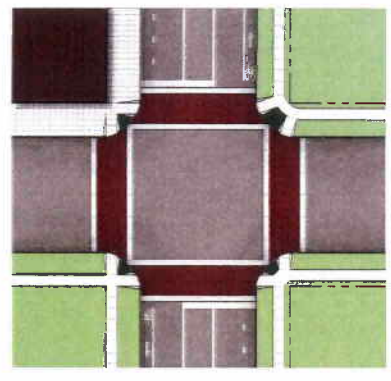
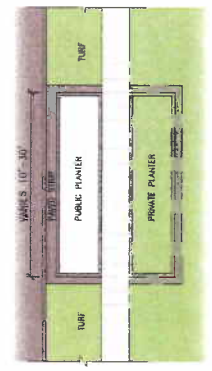
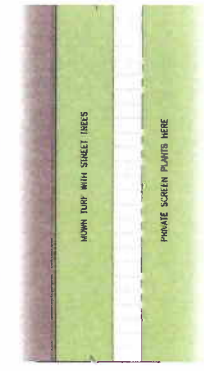
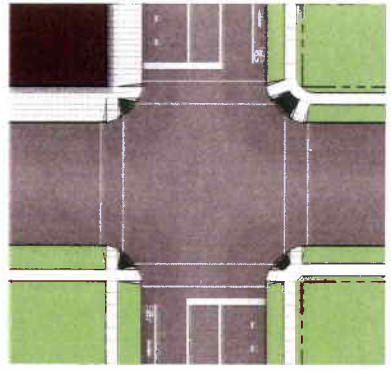
-  Primary intersections have been identified with implementation of tactile warning strips, brick or unit paver crosswalks, and improved pedestrian signals to enhance the safety.
-  Secondary intersections are identified with painted walks and tactile warning strips.

STREETSCAPE TYPOLOGIES

-  **PARKWAY IMPROVEMENTS**
Street tree plantings in parkway, shrubs for screening, new p.c.c. walk, Parking lot screening
-  **LINEAR PLANTER**
Curbed planters within the right-of-way
Private Planters as property owners allow
-  **SPECIAL USE**
Curbed planter with bench integrated, separation of private / public use, outdoor restaurant seating, shopping district
-  **URBAN CORE**
Street trees, new p.c.c. walk, bicycle racks, seating, curbed year round planters, seasonal urns, pedestrian lighting with pole banners

Corridor design using these typologies is conceptual and will require further refinement based on location prior to implementation. The corridor plan also showcases north and south gateway marker locations, and relocation of curb cuts, or drive access points along Beaver Avenue.

This key of intersection improvements and streetscape typologies applies to the North / South Conceptual plans on pages 3.3, 3.4, 3.7, 3.8.





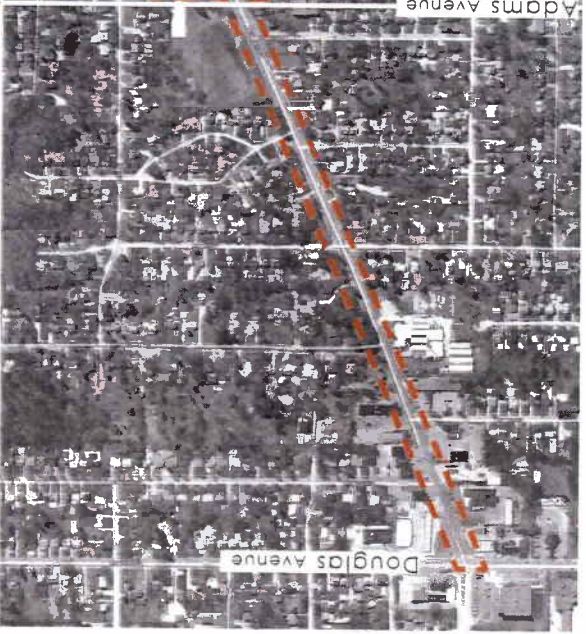
South District
Conceptual Design Area



Central Core
Preliminary Design Area



North District
Conceptual Design Area



Existing conditions- South District



Existing conditions- Central Core



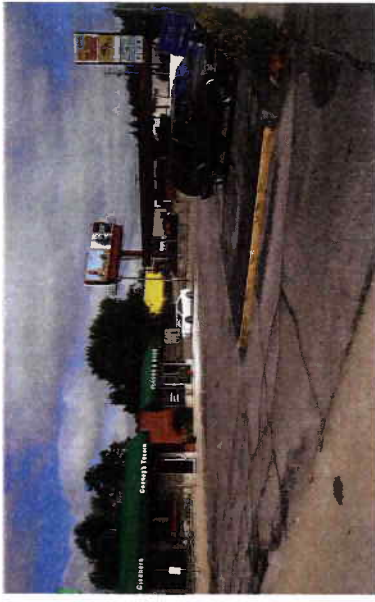
Existing conditions North District



NORTH CORRIDOR CONCEPTUAL PLAN



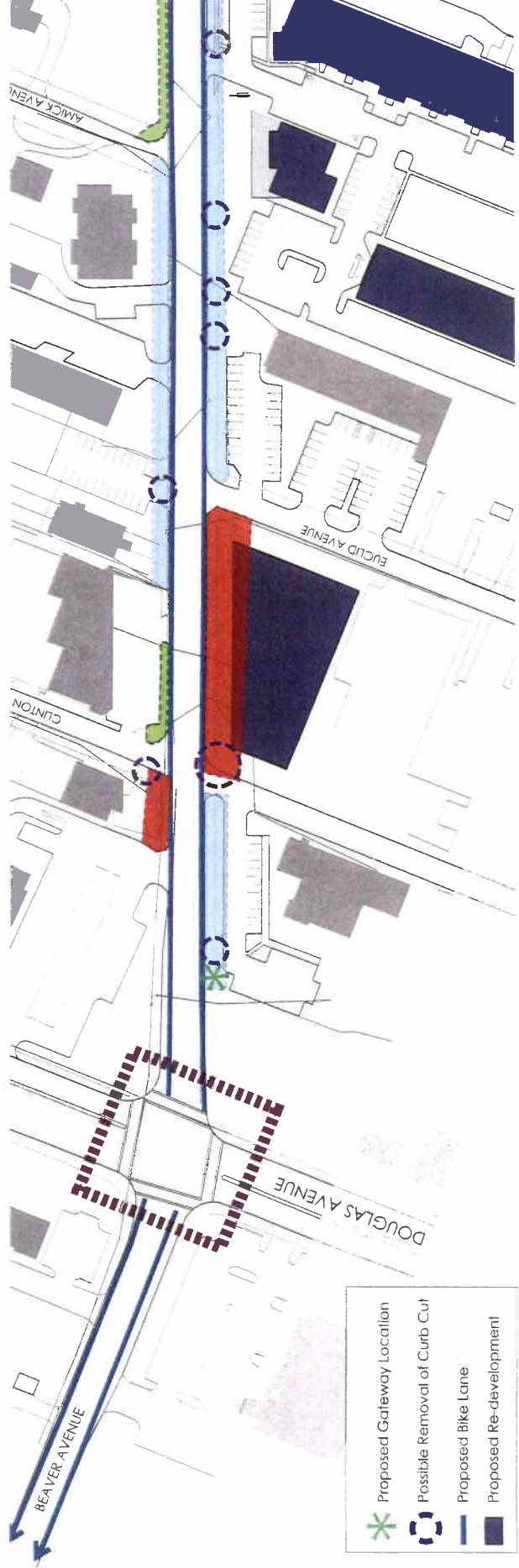
Existing condition: Douglas & Beaver Intersection



Existing conditions: Parking: Dominates street view



Existing conditions: view north on Beaver Avenue



- Proposed Gateway Location
- Possible Removal of Curb Cut
- Proposed Bike Lane
- Proposed Re-development

- Primary intersection. Existing unit paver crosswalk. Little improvement needed.

- Possible removal of curb cut near Clinton Avenue. Screen or relocate parking, net loss 4 spaces.
- Possible removal of curb cut and relocation to south, reconfiguration of parking net loss 2 spaces.
- Gateway location storm water pilot project.
- Green infrastructure
- Possible removal of curb cut and relocation to south creating new development and Hy-Vee TBD.

- Removal of curb cuts and redevelopment of Boesen the Florist.
- Possible removal of curb cut.
- Additional green space available.
- Net loss 2-3 parking spaces.
- Modification of V.F.W. curb cut to one access, reconfiguration of parking, no net loss.



Existing conditions - Back Street Annick Avenue



Existing conditions - View north towards Douglas - Narrow Sidewalks



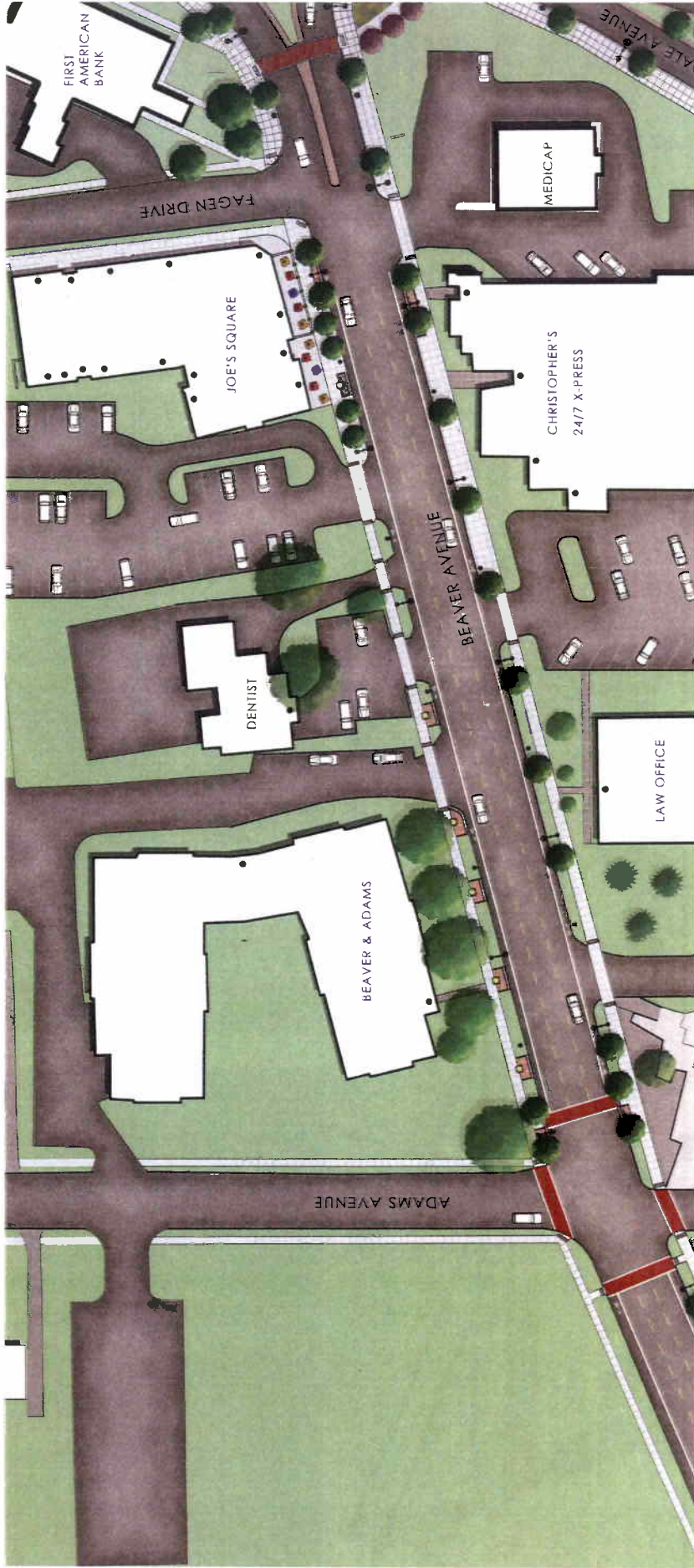
Existing conditions - View North Near Boesen the Florist



- Designated bike lane and center turn lane.
- Vegetative street improvements.
- New street trees and sidewalk planters.
- New p.c.c. - walk 5' minimum width.

- Designated bike lane and center turn lane.
- Vegetative street improvements.
- New street trees and sidewalk planters.
- New p.c.c. - walk 5' minimum width.

- Two way road / continue designated bike lane at Adams Avenue
- Residential vegetative improvements.
- New street trees / sidewalk plantings.
- New p.c.c. - walk 5' minimum.
- Modified secondary crosswalk.
- Plantings within bulb-outs.

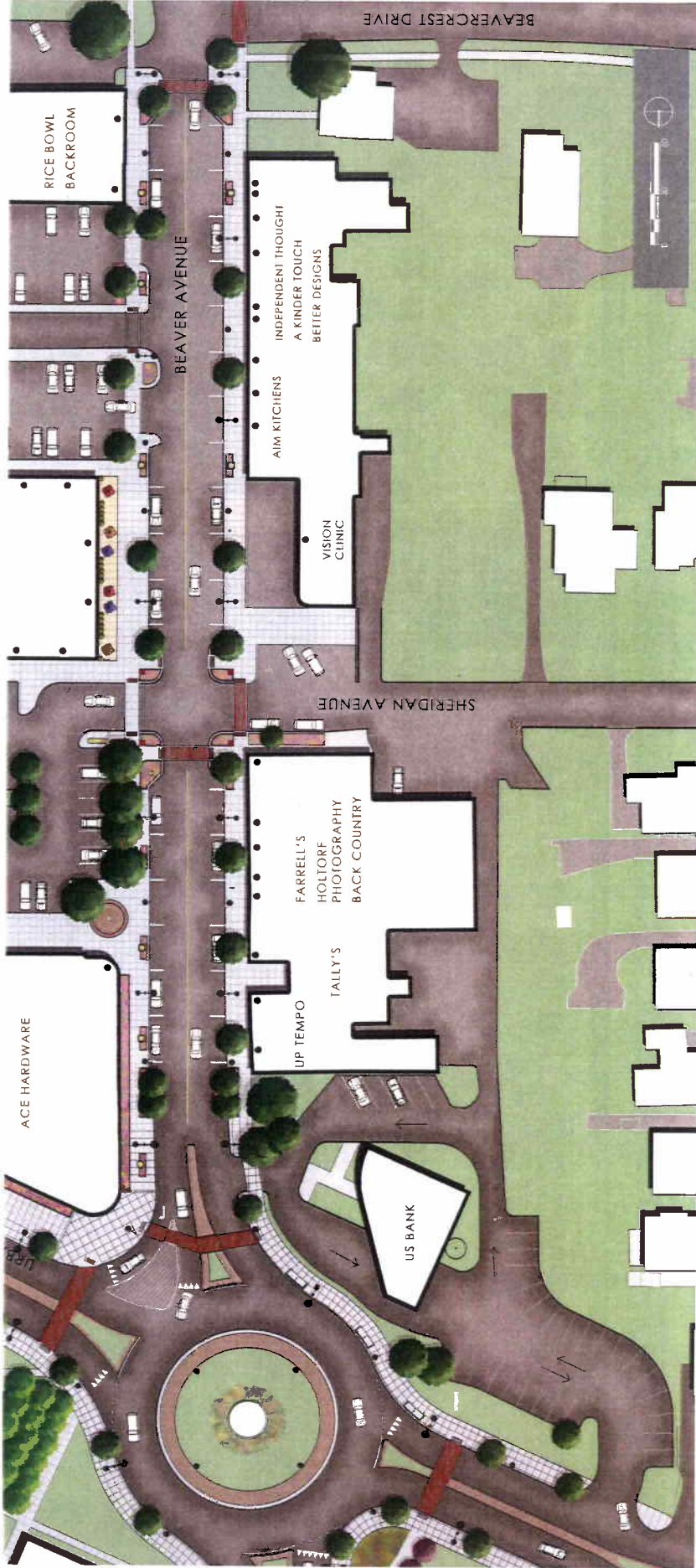


ADAMS AVENUE TO MID-BLOCK

- Install colored-concrete crosswalks at intersection of Beaver Avenue and Adams Avenue.
- Begin use of new light fixtures and consolidation of utility poles and overhead lines.
- Existing three lanes of traffic are maintained. No additional parking is added in this concept. Begin designated bike lane.
- Replace existing curb and gutter and install new concrete sidewalk.
- Street trees, planters, and planting urns are introduced to enhance visual appearance. Conifer trees and shrubs help to screen the Holy Trinity parking lot.
- Green infrastructure, including rain gardens, are proposed to improve storm water drainage.

MID-BLOCK TO FAGEN DRIVE

- Continue utilization of new light fixtures and consolidation of utility poles and overhead lines.
- Existing three lanes of traffic are maintained and no on street parking is added in this concept. Creation of a designated bike lane.
- Minimize curb cut at 24/7 Express; replace driveway with sidewalk and vegetated planters.
- Replace existing curb and gutter and install new concrete sidewalk.
- Existing street trees to remain in front of Joe's Square.
- Street trees, planters, and planting urns enhance the visual appearance of the corridor.
- Green infrastructure, including rain gardens, are proposed to improve storm water drainage.



FACET DRIVE TO URBANDALE AVENUE

- Reconfigure intersection of Beaver Avenue and Urbandale and install single-lane roundabout to ease traffic flow. Existing traffic signals are removed.
- Possible civic feature & landscaping at the center of the roundabout.
- Colored concrete crosswalks designate pedestrian crossings. Existing medians along Urbandale Avenue are narrowed to create a safe haven for pedestrians.
- Open space is created in the area south of First American Bank.
- Continue utilization of new light fixtures and consolidation of utility poles and overhead lines.
- Replace existing curb and gutter and install new concrete sidewalk.
- Street trees, planters, and planting urns enhance the visual appearance of the corridor.

URBANDALE AVENUE TO SHERIDAN AVENUE

- Continue utilization of new light fixtures and consolidation of utility poles and overhead lines.
- Existing on-street parking remains (13 existing / 12 proposed).
- Enhance the mural alongside UpTempo Music.
- Existing drive access to the Beaverdale Place parking lot is relocated to the south to create an intersection at Sheridan Avenue. Relocation of the Beaverdale Place signage is proposed.
- Colored concrete crosswalks delineate pedestrian crossings, and ADA warning pavers are installed. A pedestrian signal will remain at the Sheridan intersection.
- Plantings and green infrastructure are added along Ace Hardware, and also to screen the parking lot between Ace Hardware and Beaverdale Place. Street trees, planters, and planting urns further enhance the visual appearance of the corridor.

SHERIDAN AVENUE TO BEAVERCREST DRIVE

- Continue utilization of new light fixtures and consolidation of utility poles and overhead lines.
- Existing on-street parking remains (19 existing / 17 proposed).
- Outdoor seating and patio garden support Beaverdale Place businesses.
- Colored-concrete crosswalks delineate pedestrian crossings at the intersection of Beaver Avenue and Beavercrest Drive.
- Street trees, planters, and planting urns enhance the visual appearance of the corridor.
- Green infrastructure, including rain gardens, are proposed to improve storm water drainage.

SOUTH CORRIDOR CONCEPTUAL PLAN



Existing Conditions - South of Beavercrest Drive



Existing Conditions - North of Hickman Road



Existing Conditions - New South Hickman Road and Beaver Avenue



- Proposed Gateway Location
- Possible removal of Curb cut
- Proposed Bike Lane
- Proposed Shared Bike Lane

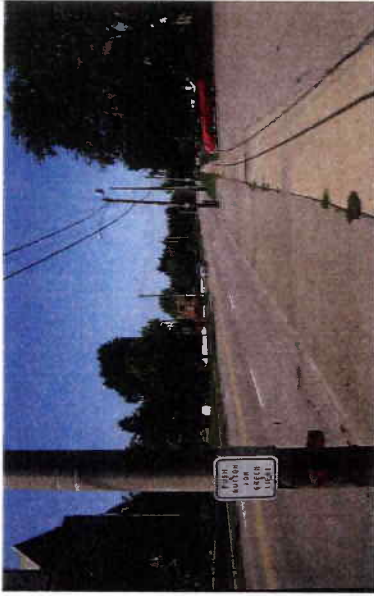
- Two way road / shared bike lane beginning at Beavercrest
- Residential vegetative improvements.
- New street trees / sidewalk plantings.
- New p.c.c. walk 5' minimum.
- Modified secondary crosswalk.
- Plantings within bulb-outs.
- Green infrastructure
- Bulb-outs on north side of intersection

- Reduce roadway width to improve pedestrian safety.
- Designated bike lanes 4-5' width with 2-3 lanes of traffic
- 10-11' width.
- Residential vegetative improvements.
- New street trees / sidewalk plantings.
- New p.c.c. walk minimum 5' width.
- Minimal curb depth. Mill and resurface.

- Primary crosswalk
- Unify ADA warning strips on all four corners
- Brick unit paver crosswalks
- Increase signaled pedestrian cross time.
- Possible relocation of curb cut, and/or width reduction of drop-off lane, new curb cut on Hickman.
- New street trees.
- Designated bike lanes



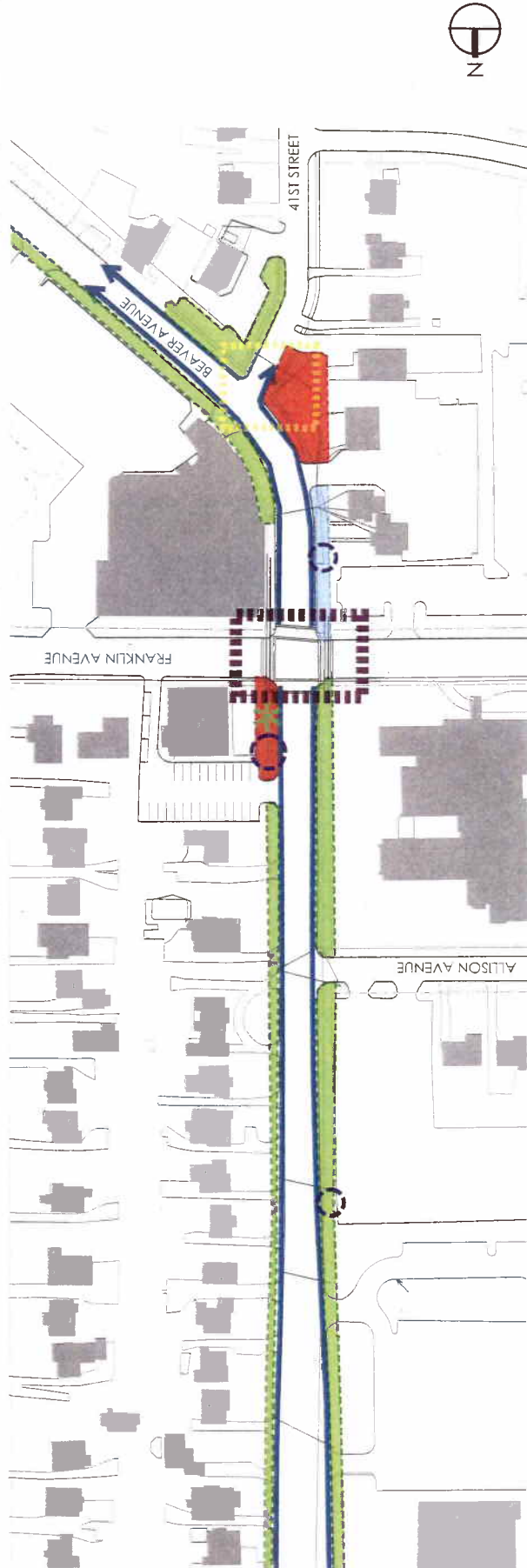
Existing Conditions: View North of Allison Avenue on Beaver



Existing Conditions: View North of Franklin Avenue on Beaver



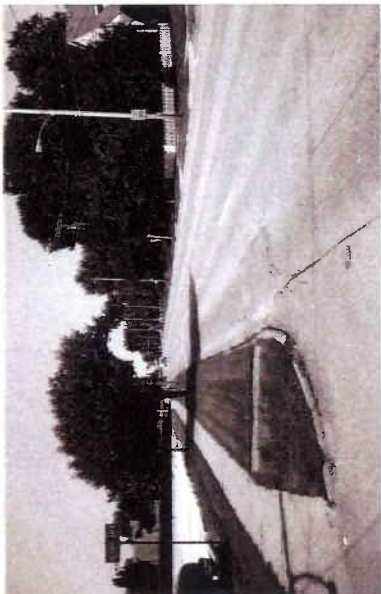
Existing Conditions: Beaver Avenue & 41st Street



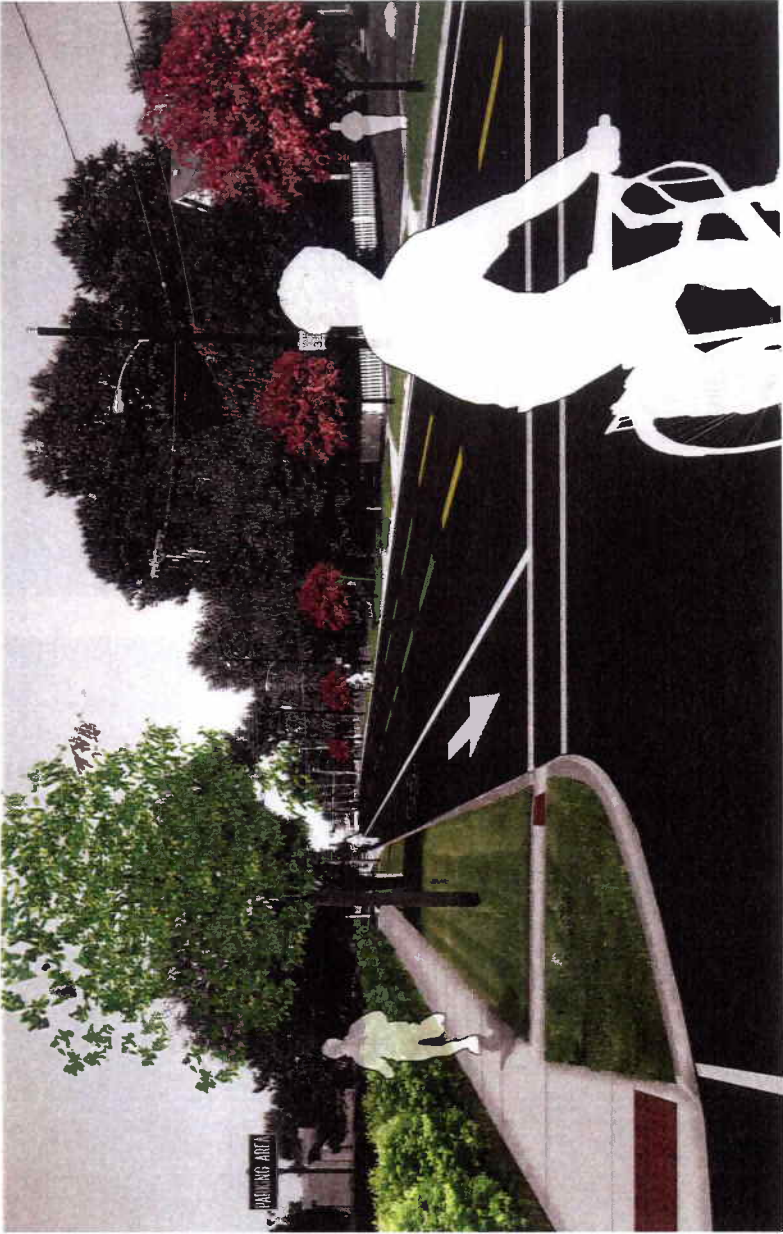
- Possible removal of curb cut.
- Reconfiguration of parking: one way angled.
- Implementation of vegetation or other means to screen large parking lots from roadway.
- Residential Vegetative Improvements.
- Implementation of new street trees.
- Series of vegetative planters along walk.
- New concrete walk [5' minimum width].

- South Gateway location.
- Removal of curb cut and reconfiguration / removal of parking to a one way drive.
- Outdoor cafe / seating area.
- Stormwater pilot project.
- New street trees.
- Brick Paving / ADA warning strip at intersections
- Designated bike lanes

- Secondary Crosswalk
- Reconfiguration of 41st and Beaver intersection.
- Painted Pedestrian crosswalk / ADA warning strip
- New Civic Space.
- Removal of curb cut and reconfiguration of parking.
- New street trees.
- Designated bike lanes creating connections to Drake neighborhood and downtown.



Existing conditions - new main



BEAVER AVENUE NORTH OF FRANKLIN AVENUE

Parking Lot Screening

Sidewalk Replacement (5-6' width)

Street Profile Improvements
 mill and overlay asphalt
 replace curbs
 enhance crosswalks to meet ADA standards

Street Plantings in Parkway
 overstory shade trees
 understorey ornamental trees
 plantings to be coordinated with utilities

Bicycle + Drive Lanes
 designated bike lanes Beavercrest to Franklin
 Avenue
 three lane vehicular use
 center turn lane



BEAVER AVENUE SOUTH OF DOUGLAS AVENUE

Bicycle and Drive Lanes
 keep existing bicycle lanes
 continue south to Adams Avenue

Street Plantings
 curbed perennial planters
 urns with annual plants
 street trees

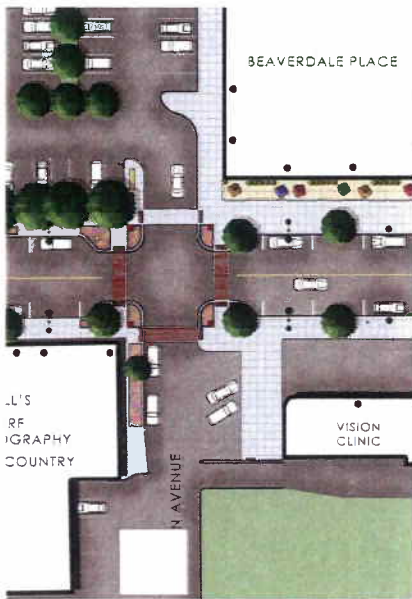
Street Lighting
 pedestrian scale lighting
 roadway scale lighting
 banners

Site Furnishings
 timeless character
 minor customization optional
 recycled materials

Vegetative Screening
 screening per city code
 unify corridor aesthetics



Existing conditions - view south



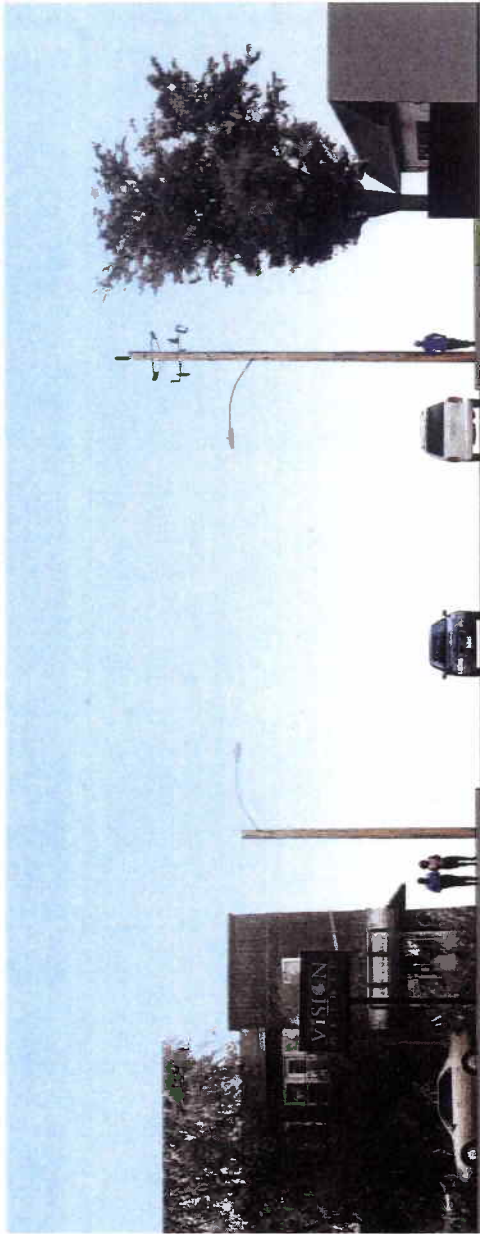
SECTION AT SHERIDAN VIEW NORTH

EXISTING CONDITIONS

- Parking in front of building
- Overhead utilities crossing Beaver Avenue
- Absence of vegetation
- Pedestrian walks with little vegetative interest
- On-street parking

PROPOSED CONDITIONS

- Reconfiguration of parking, relocation of curb cuts and additional on street parking added.
- Bulb-outs to extend planting areas and improve traffic transition.
- Green infrastructure - planters
- Narrowed roadway width, traffic calming
- New p.c.c. walk
- Outdoor seating / cafe opportunities
- On-street parking
- Removal of west side overhead utilities
- Removal of overhead utilities crossing Beaver Avenue
- Opportunities for public art
- New pedestrian and roadway lighting
- New street trees



EXIST. CONDITIONS

BEAVER AVENUE



PROPOSED

BEAVER AVENUE



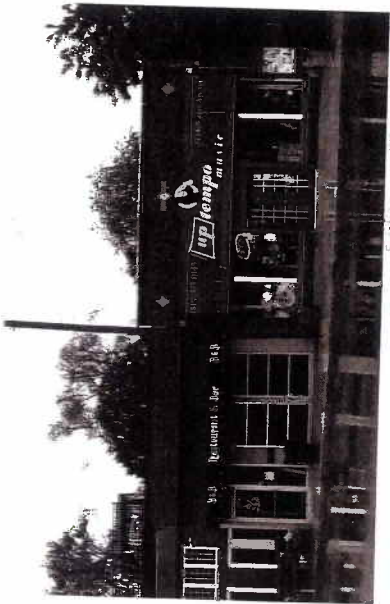
Existing 2000ft Hens - new health



SHERIDAN AND BEAVER LOOKING NORTH

- Street Tree Plantings
 - columnar overstory
 - coordinated with utilities
- Stormwater Pilot Project
 - infiltration and cleansing
 - pre-treatment
- Improved Pedestrian Safety
 - shorter crosswalks
 - tactile warning strips
 - signalized pedestrian light to remain

- Curb Bulb-outs
 - traffic calming
 - public art opportunities
 - street plantings
- Ornamental Lighting
 - pedestrian scale lighting
 - roadway lighting
 - banners
- Seating and Civic Areas
 - lighting
 - gathering spaces
 - public events



EXISTING CONDITIONS - FEBRUARY 2015



BEAVER AVENUE SOUTH OF URBANDALE AVENUE

On Street Parking
parking maintained
spaces delineated

Site Furnishings
timeless character
optional minor customizations
recycled materials

Bicycle and Drive Lanes
shared lanes from Urbandale Avenue to Beavercrest
reduced vehicular speed

Ornamental Lighting
pedestrian scale lighting
roadway lighting
banners



Existing Conditions



BEAVER AVENUE SOUTH OF URBANDALE AVENUE

Improved Business Signage
develop standards
utilize available space

Civic Space
incorporate public art
gathering / meeting area
seating

Green Infrastructure
infiltration areas within bulb-outs

Plantings at Building Face
soften pedestrian experience along walk

Advertising
opportunities to showcase products/events on wall
space adjacent to streetscape

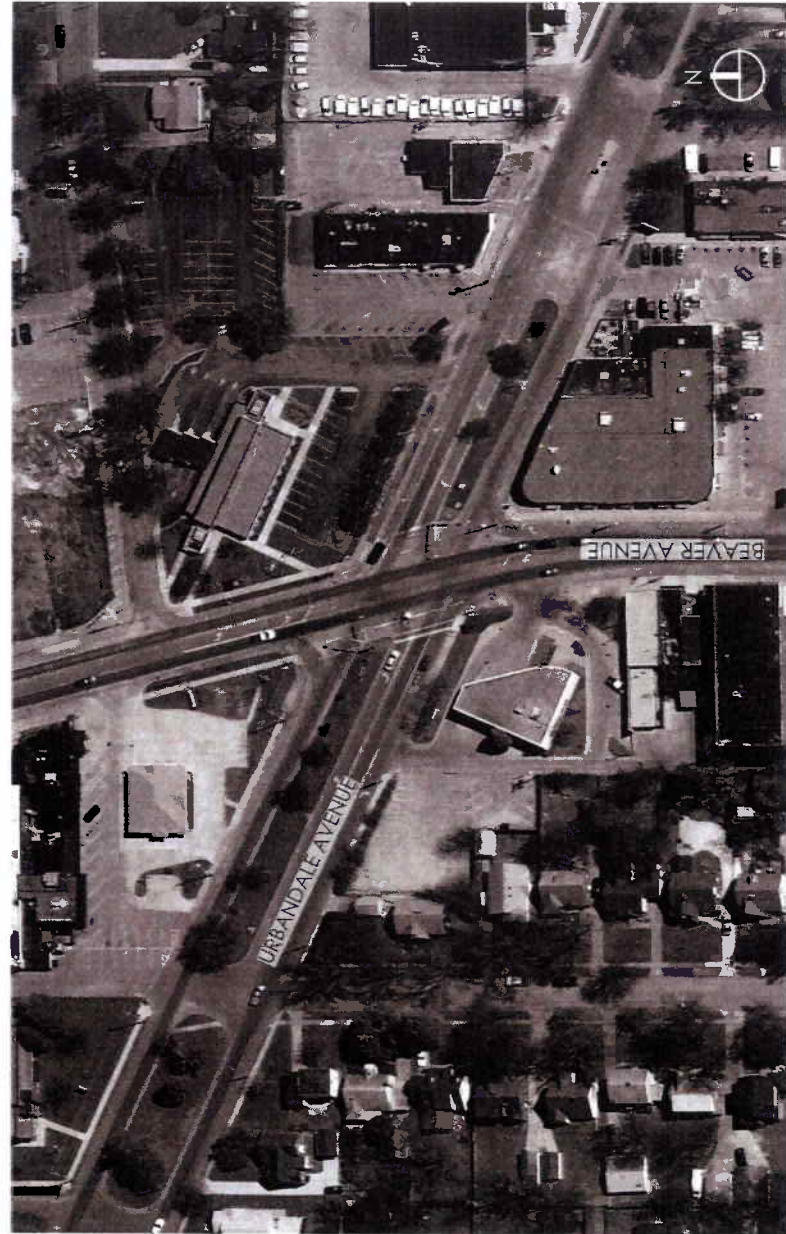


Existing conditions. Intersection of Urbandale and Beaver Avenue

**BEAVER AVENUE + URBANDALE AVENUE
INTERSECTION CONCEPTS**

EXISTING CONDITIONS

- Long Crosswalks 115' - 130'
- Curb cuts near intersection
- View of parking lots dominate
- Automobile oriented
- Lacks a sense of place
- Challenging left turn lanes
- Wide medians
- Short pedestrian crossing time



Existing conditions. Plan plan



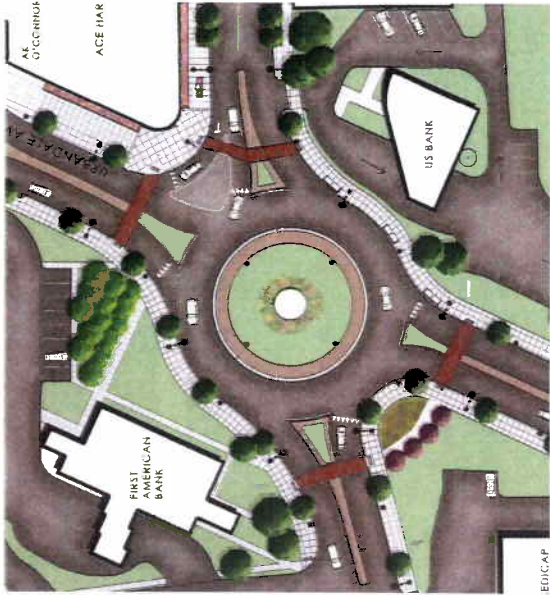
Two concepts were developed to improve pedestrian safety and vehicular circulation in the heart of Beaverdale. Each concept was presented to the public at multiple meetings. Concerns were expressed about both concepts; however, public input showed that the roundabout concept was preferred to the narrowed concept or no change at all.



NARROWED CONCEPT

- Dedicated Bike lane
- Shorter crosswalks + 65"
- Reduction of surface parking at the northeast corner near First American Bank

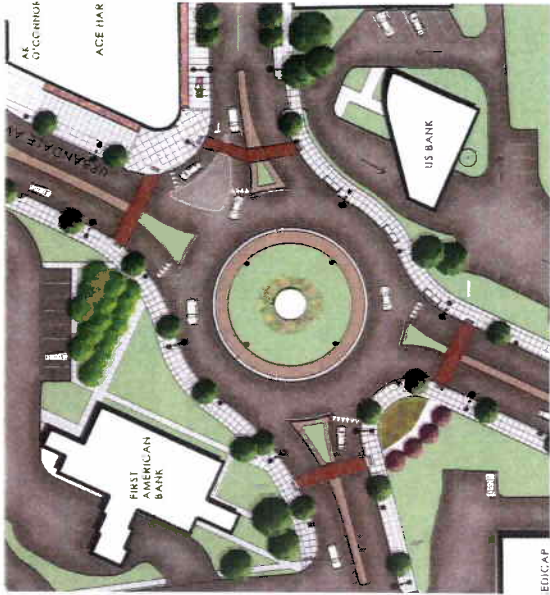
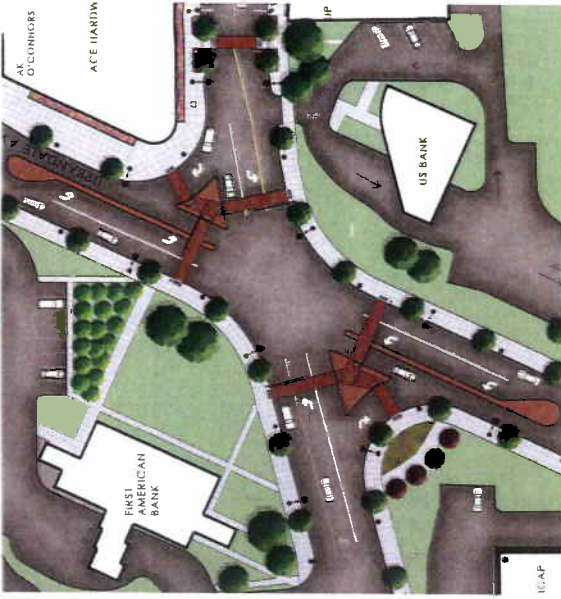
- New Traffic Signals
- Concrete walk and curb enhancements
- Street trees + perennial plantings
- Traffic movement similar to existing conditions with improved visibility for left turns.

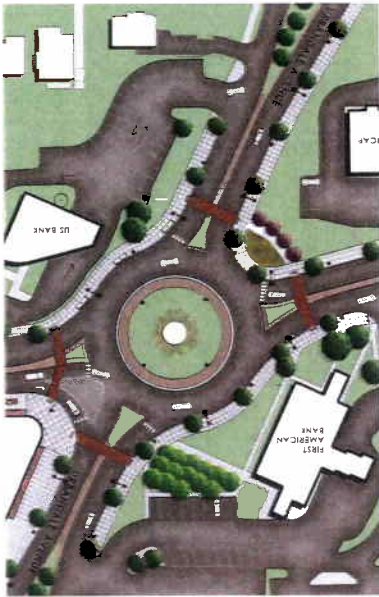


ROUNDABOUT CONCEPT | RECOMMENDED |

- One Lane Roundabout
- 150' Diameter
- Requires approximately 5,500 SF Right-of-Way Acquisition
- Eliminates traffic signals
- Crosswalks should not exceed 30'
- Reduction of surface parking near First American Bank

- Accommodates bikes, experienced bikers will navigate roundabout others will walk through the crosswalks
- Concrete walk and curb enhancements
- Street trees + perennial plantings





PROPOSED CONDITIONS

- One lane roundabout
- Reduced traffic speeds
- New civic space and urban trail hub (NE Corner)
- Re-configured parking
- Parking lot screening
- Civic feature in center of roundabout
- New street plantings
- Reduced crosswalk distances 25-48'
- New Street trees
- Removal of signals
- Safer pedestrian crossings
- New opportunities for civic space adjacent to roundabout



Existing Conditions - View South



Proposed - View South



Seasonal Simulation



Roundabout Presented Imagery

PROPOSED ROUNDABOUT

Roundabouts, used in-place of stop signs and traffic signals, are a type of circular intersection that can significantly improve traffic flow and safety. Where roundabouts have been installed, motor vehicle crashes have declined by about 40 percent, and those involving injuries have been reduced by about 80 percent. Crash reductions are accompanied by significant improvements in traffic flow, thus reducing vehicle delays, fuel consumption, and air pollution.

Source: Insurance Institute for Highway Safety

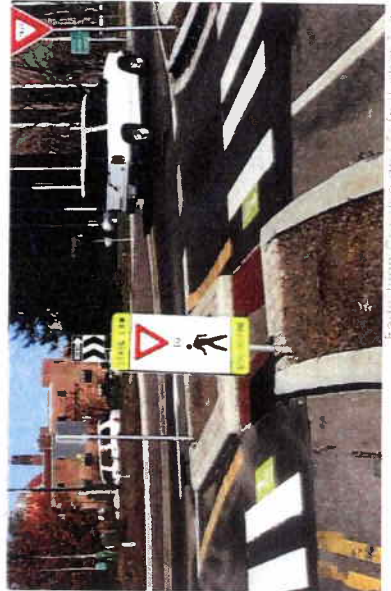
POTENTIAL GRANT OPPORTUNITIES

Iowa Clean Air Attainment Program (ICAAP)

Traffic Safety Improvement Program (TSIP)

Highway Safety Improvement Program (HSIP)

Potential Grant opportunities: \$600,000 - 1.3 Million



Roundabout showing that safe through lane



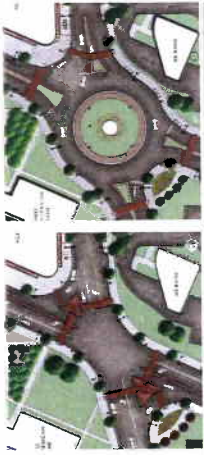
Safe Pedestrian Crossing



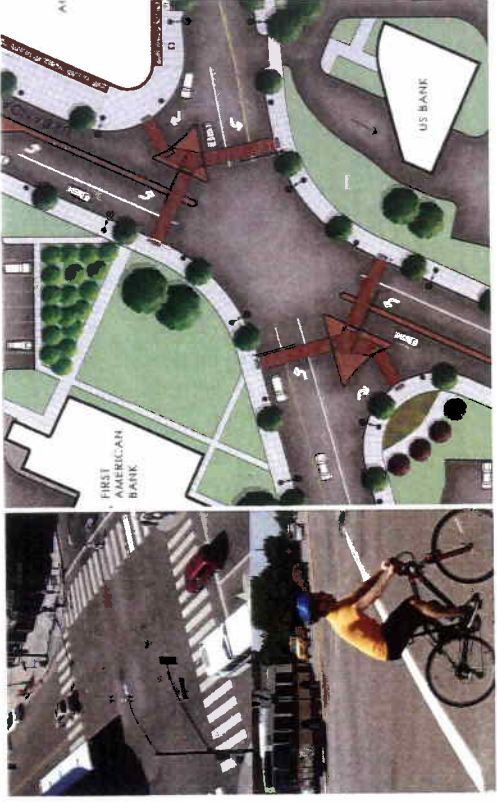
Example of a roundabout - ITRIP Roundabout



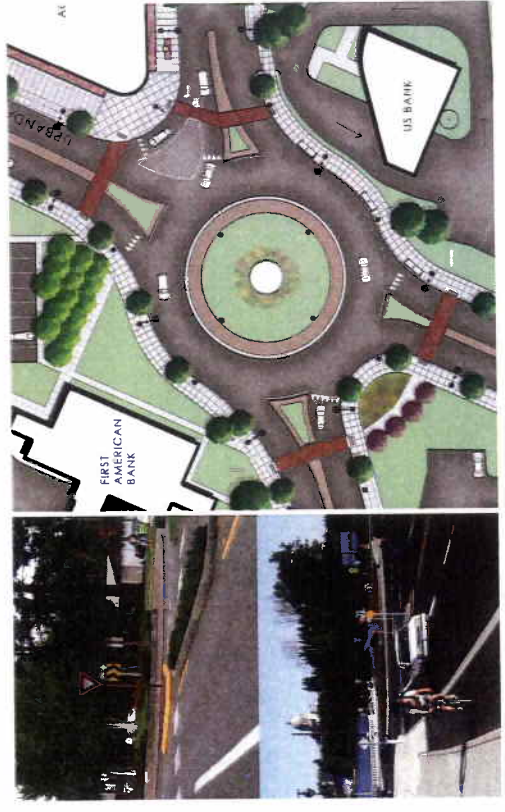
INTERSECTION COMPARISON

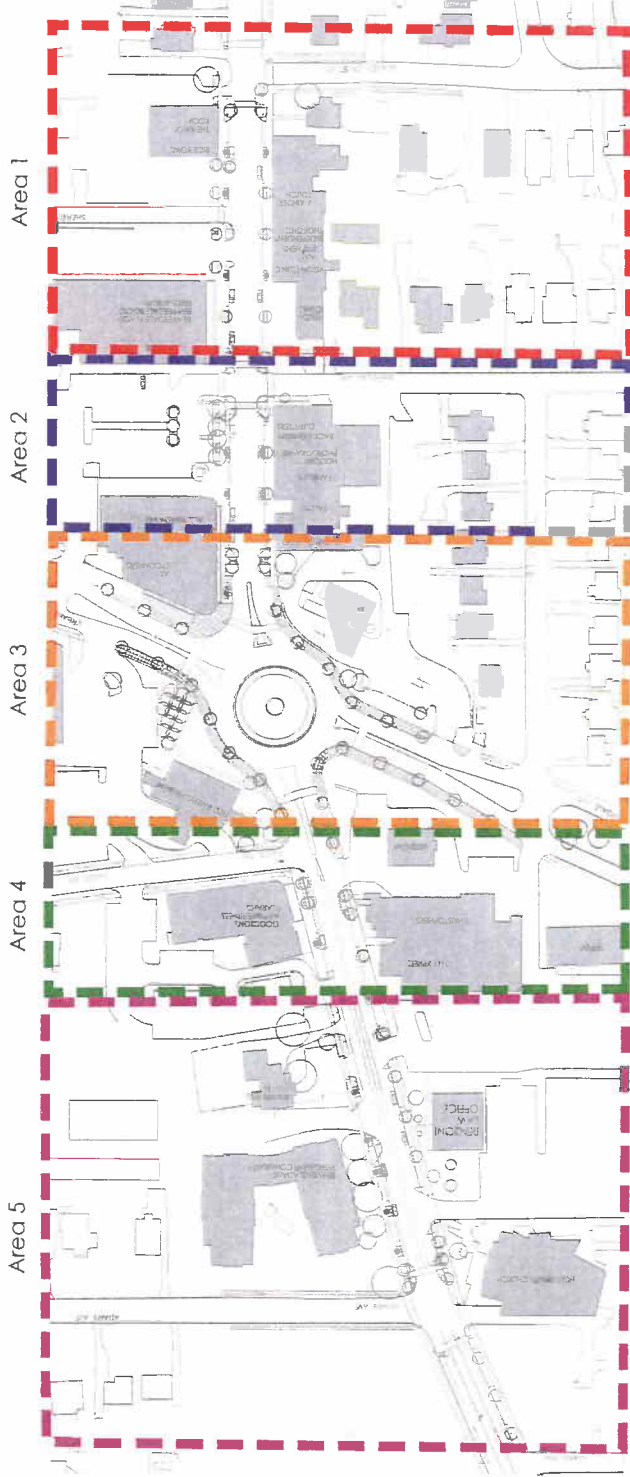


	Narrowed	Roundabout
Pedestrian Crossing Width	65' approx.	26'-48' approx.
Public Art Opportunity	Yes	Yes
Pavement Square Feet	19,000	16,000
Typical Vehicle Accident Reduction	5-10%	65-75%
Improves Emissions (clean air)	No	Yes
Land Acquisition Required	No	Yes
Traffic Signalization Required	Yes	No
Safety Enhancement	Yes	Yes
Anticipated Construction Cost	\$1,375,000	\$1,260,000
Potential Grant Opportunities	\$75,000 - 100,000	\$700,000-\$1.3 Million
Traffic Movement	Similar to Existing	Continuous Flow
Bicycle Friendly	Yes	Yes
Accommodates Buses & Fire Trucks	Yes	Yes
Accommodates Snow Plow	Yes	Yes
Works for Beaverfest Parade	Absolutely	Absolutely



ROUNDABOUT OR NARROWED





Accuracy

The level of detail and accuracy of pricing in this opinion of probable cost are consistent with the nature of the study for estimating purposes. The document used to prepare this estimate includes the Concept Plan components shown within this document and information provided by the Consultant Team members. Additional information was obtained through discussion with the Client, Stakeholders and industry contacts.

Bid Conditions

This project has been estimated as a complete project with separate subtotals for individual portions of work for informational purposes only. If the project is bid in phases, the total estimated costs will be higher.

Items Affecting the Cost Opinion

Items which may change the probable construction costs include, but are not limited to:

Modifications to the scope of work included in this estimate.
Restrictive technical specification or excessive contract conditions.
Any specified item of equipment, material, or product that cannot be obtained from at least three different sources.

Escalation

Unit costs included herein are reflective of current costs with no escalation included. A labor and material escalation factor will need to be added once a construction period has been determined.

Exclusions

Items excluded from this estimate include professional design, engineering and permitting fees.

Probable Cost Opinion Objective

This cost opinion is intended to be used as a tool for decision-making and managing construction costs during the next phases of the project. It is prepared using industry contacts, experience, and the best judgment of the professional consultants. This estimate is intended to reflect an amount close to what would be the low bid of the project with respect to the present level of design and documentation with consideration given to the current market conditions. The consultant has no control over market conditions, wage rates, or any contractor's method of determining prices or quantities. Therefore, the consultant cannot and does not guarantee this cost opinion will not vary from the actual cost of construction.

Self-Supported Municipal Improvement District (SSMID)

Iowa Code Chapter 386, a levy rate is established and assessed to a property located in the district boundaries and applied to commercially zoned property. Residential property is exempt. SSMID is required for all Des Moines streetscape projects. SSMID revenue is utilized for on-going maintenance of the streetscape improvements

SSMID is implemented by first establishing a levy rate (in previous streetscape projects the rate has been \$1.75 per \$1,000 of taxable value). A petition containing signatures of 25% of property owners would be obtained representing 25% or more of the assessed value of all the property in the district. SSMID would need to be in place prior to construction

A board of property owners and business owners is formed to manage resources. The board controls the annual review of SSMID revenue and expenditures, and makes recommendations to the city council regarding the budget.

SSMID includes properties that adjoin the streetscape improvements and potential future phases. Example: Back Country Outfitters - \$218,000 assessed value x \$1.75/\$1,000 taxable value = \$381.50/year



Beaver Avenue Streetscape				
CONCEPT DESIGN - AREA 1 - 4				
June 2010				
[ROUNDABOUT WITH THREE LANES]				
DESCRIPTION	TOTAL ESTIMATED COST	CITY FUNDS		PRIVATE FUNDS
		STREET / SIDEWALK IMPROVEMENTS (SI)	SEWER / SIGNAL IMPROVEMENTS (UI)	STREETSCAPE ELEMENTS (SE)
				(PR)
Demolition	200,250.00			
Erosion Control	16,750.00			
Utility Improvements	231,030.00		231,030.00	
Water Main Improvements	42,750.00			42,750.00
New Pavement - Standard	559,740.00			
New Pavement - Non Standard	185,580.00			\$92,790.00
Site Amenities	146,550.00			146,550.00
Landscaping	57,395.00			\$57,395.00
Overhead Power Rework	46,300.00			
Overhead to Underground Rework	69,870.00			\$23,150.00
Overhead Street Lighting	48,400.00			
Pedestrian Street Lighting	38,400.00			\$24,200.00
Electrical Conduit / Distribution	26,437.50			38,400.00
Public Improvements on Private Property	180,131.50	180,131.50		\$13,218.75
Subtotal	1,849,604.00	956,891.50	231,030.00	508,323.75
Mobilization / General Conditions (5%)	\$92,480.20	\$47,844.58	\$11,551.50	\$7,667.94
Contingency (15%)	\$277,440.60	\$143,533.73	\$34,654.50	\$23,003.81
Total	2,219,524.80	1,148,269.80	277,236.00	609,988.50
Design Services / City of D.M. Fees / C.A. (20%)	\$443,904.96	\$221,952.48	\$0.00	\$221,952.48
TOTAL	2,663,429.76	1,370,222.28	277,236.00	831,940.98
			TOTAL CITY FUNDS	TOTAL PRIVATE FUNDS
			\$1,831,468.76	\$831,940.98



Beaver Avenue Streetscape CONCEPT DESIGN - AREA 5 June 2010						
[ROUNDABOUT WITH THREE LANES]						
DESCRIPTION	ESTIMATED COST	CITY FUNDS			PRIVATE FUNDS	
		STREET IMPROVEMENTS (SI)	UTILITY IMPROVEMENTS (UI)	STREETSCAPE ELEMENTS (SE)	(PR)	
Demolition	32,595.00	32,595.00				
Erosion Control	4,000.00	4,000.00				
Utility Improvements	43,920.00		43,920.00			
Water Improvements	9,500.00					9,500.00
New Pavement - Standard	148,275.00	148,275.00				
New Pavement - Non-Standard	9,520.00					
Site Amenities	20,950.00					20,950.00
Landscaping	9,971.50					9,971.50
Overhead Power Rework	18,000.00					18,000.00
Overhead to Underground Rework	22,100.00					22,100.00
Overhead Street Lighting	22,000.00					22,000.00
Pedestrian Street Lighting	12,000.00					12,000.00
Electrical Conduit / Distribution	8,700.00					8,700.00
Public Improvements on Private Property	0.00	0.00				
Subtotal	361,503.00	184,870.00	43,920.00	34,081.50	98,631.50	
Mobilization / General Conditions (5%)	\$18,075.15	\$9,243.50	\$1,704.08	\$1,704.08	\$4,931.58	
Contingency (1.5%)	\$54,225.45	\$27,730.50	\$6,588.00	\$6,588.00	\$14,794.73	
Total	433,803.60	221,844.00	52,704.00	40,897.80	118,357.80	
Design Services / City of D. M. Fees / C.A. (20%)	\$86,760.72	\$43,380.36	\$0.00	\$0.00	\$43,380.36	
TOTAL	520,564.32	265,224.36	52,704.00	40,897.80	161,738.16	
				TOTAL CITY FUNDS	TOTAL PRIVATE FUNDS	
				\$358,826.16	\$161,738.16	