



Date October 10, 2016

RESOLUTION ON REQUEST FROM QUIK TRIP CORPORATION TO AMEND THE PLANDSM: CREATING OUR TOMORROW PLAN FUTURE LAND USE DESIGNATION (1522 CAPITOL AVENUE)

WHEREAS, on April 25, 2016, by Roll Call No. 16-0717, the City Council adopted the PlanDSM: Creating Our Tomorrow Plan; and

WHEREAS, on September 26, 2016, by Roll Call No. 16-1610, the City Council received a communication from the City Plan and Zoning Commission advising that at a public hearing held September 15, 2016, its members voted 7-4 to recommend APPROVAL of a request from Quik Trip Corporation (owner), represented by Mike Talcott (officer), to amend the PlanDSM: Creating Our Tomorrow Plan to revise the current future land use designation for real property locally known as 1522 Capitol Avenue from Low Density Residential within a neighborhood node to Community Mixed Use within a neighborhood node.

NOW THEREFORE, BE IT RESOLVED, by the City Council of the City of Des Moines, Iowa, that the proposed amendment to the PlanDSM: Creating Our Tomorrow Plan, as described above, is hereby approved / denied.

MOVED by _____ to adopt and APPROVE / DENY the proposed amendment.

FORM APPROVED:

Glenna K. Frank, Assistant City Attorney

(21-2016-4.05)

NOTE: Six affirmative votes are required to approve the proposed amendment due to the Commission's failure to approve the amendment by the affirmative vote of two-thirds of the present members of the Commission. Des Moines City Code §§ 82-77 and 82-78.

Table with 5 columns: COUNCIL ACTION, YEAS, NAYS, PASS, ABSENT. Rows include COWNIE, COLEMAN, GATTO, GRAY, HENSLEY, MOORE, WESTERGAARD, and TOTAL.

MOTION CARRIED APPROVED

Mayor

CERTIFICATE

I, DIANE RAUH, City Clerk of said City hereby certify that at a meeting of the City Council of said City of Des Moines, held on the above date, among other proceedings the above was adopted.

IN WITNESS WHEREOF, I have hereunto set my hand and affixed my seal the day and year first above written.

City Clerk

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September 22, 2016

Honorable Mayor and City Council
City of Des Moines, Iowa

Members:

Communication from the City Plan and Zoning Commission advising that at their meeting held September 15, 2016, the following actions were taken regarding requests from Quik Trip Corporation (owner) represented by Michael Talcott (officer) on property located at 1501 & 1515 East Grand Avenue and 1522 Capitol Avenue.

COMMISSION RECOMMENDATION:

After public hearing, the members voted 11-0 as follows:

Commission Action:	Yes	Nays	Pass	Absent
Francis Boggus	X			
Dory Briles	X			
JoAnne Corigliano	X			
David Courard-Hauri	X			
Jacqueline Easley	X			
Jann Freed				X
John "Jack" Hilmes	X			
Carolyn Jenison	X			
Greg Jones	X			
William Page				X
Mike Simonson	X			
CJ Stephens				X
Steve Wallace	X			
Greg Wattier				X
Rocky Sposato	X			

APPROVAL of Part A) that the proposed rezoning be found **not** in conformance with the existing PlanDSM Creating Our Tomorrow future land use designation for the property known as 1522 Capitol Avenue;

and Highway-Oriented Commercial District to revise the conditions to remove the condition prohibiting the commercial site development to Capitol Avenue, subject to the following conditions:

1. The public alley within the Property shall not be truncated and shall be connected to Capitol Avenue.
2. The applicant shall work with City staff to create an urban edge for the north streetscape (Grand Avenue frontage), to the approval of the Planning Administrator. (ZON2016-00100)

The Commission also voted 7-4 as follows:

APPROVAL of Part D) a Site Plan under design guidelines for gas stations and convenience stores on property located at 1501 & 1515 East Grand and 1522 Capitol Avenue, to allow demolition of the existing convenience store and pump island canopy and to construct a 5,773-square foot convenience store with a pump island canopy having 12 fueling locations expandable to 16 fueling locations and allowing commercial site development to access Capitol Avenue, subject to the following conditions:

1. The public alley within the Property shall not be truncated and shall be connected to Capitol Avenue.
2. The applicant shall work with City staff to create an urban edge for the north streetscape (Grand Avenue frontage), to the approval of the Planning Administrator. (10-2017-7.19)

STAFF RECOMMENDATION TO THE P&Z COMMISSION

Part A) Staff recommends that the proposed rezoning be found not in conformance with the existing PlanDSM Creating Our Tomorrow future land use designation for the property known as 1522 Capitol Avenue.

Part B) Staff recommends denial of an amendment to the PlanDSM: Creating Our Tomorrow Plan to revise the future land use designation from Low Density Residential to Community Mixed Use for the property known as 1522 Capitol Avenue.

Part C) Staff recommends denial of the request to rezoning from "R1-60" District and Limited "C-2" District to a revised Limited "C-2" District.

Part D) Staff recommends denial of the submitted Site Plan.

Written Responses

0 In Favor

2 In Opposition

STAFF REPORT TO THE PLANNING COMMISSION

1. **Purpose of Request:** The subject parcels are owned in common with the parcel that contains the applicant's existing convenience store and fuel pumps. The applicant is

August 26, 2016 (20 days prior to the hearing) and September 2, 2016 (13 days prior to the hearing due to the Labor Day holiday) to the Capitol East Neighborhood Association and to the primary titleholder on file with the Polk County Assessor for each property within 250 feet of the site.

All agendas and notices are mailed to the primary contact(s) designated by the recognized neighborhood association to the City of Des Moines Neighborhood Development Division. The Capitol East Neighborhood Association notices were mailed Jack Leachman, 1921 Hubbell Avenue, Des Moines, IA 50316. The applicant held a neighborhood meeting prior the consideration of the rezoning at the July 21, 2016 meeting of the Commission.

8. Relevant Zoning History: On August 20, 2001, the City Council approved Ordinance Number 13,985 (Roll Call No. 01-2598) rezoning a portion of the applicant's site from "R1-60" District to a Limited "C-2" District. This portion of the site fronts Capitol Avenue and is comparable in width and depth to two single-family lots. The following zoning conditions are listed in Ordinance Number 13,985.

A. There shall be no direct vehicular access between the Property and Capitol Avenue. There shall be no direct vehicular access between the Property and the adjoining East/West alley. All vehicular access to the Property shall be provided through the adjoining land to East 15th Street and East Grand Avenue.

B. The following uses of land and structures shall not be permitted upon the Property:

- 1) Automobile, trailer, motorcycle, boat and farm implement establishments for display, hire, rental or sale.*
- 2) Taverns and nightclubs.*
- 3) Adult entertainment businesses.*

9. PlanDSM Creating Our Tomorrow: The subject site is designated as "Community Mixed Use", "Low-Density Residential" all within a "Neighborhood Node" on the Future Land Use Map.

10. Applicable Regulations: The Commission reviews all proposals to amend zoning boundaries or regulations within the City of Des Moines. Such amendments must be in conformance with the comprehensive plan for the City and designed to meet the criteria in 414.3 of the Iowa Code, and taking into consideration the criteria set forth in Chapter 18B of the Iowa Code. The Commission may make recommendations to the City Council on conditions to be made in addition to the existing regulations so long as the subject property owner agrees to them in writing. The recommendation of the Commission will be forwarded to the City Council.

In consideration of the criteria set forth in Chapter 18B of the Iowa Code, any Site Plan application which includes property used as a gas station or convenience store and for extension of parking shall be approved by the Plan and Zoning Commission if the proposed Site Plan conforms with the design regulations in Section 82-213 and the following additional design guidelines in Section 82-214.08 of the City Code, unless the commission determines that the construction and use of the site will have a significant detrimental impact on the use and enjoyment of adjoining residential uses. Section 82-

- *The ability to provide housing in close proximity to jobs; and*
- *Access via transit and other alternative modes of transportation.*

LU10: Prioritize new mixed use development and redevelopment along proposed high capacity transit corridors and nodes.

LU24: Provide safe and pleasant walking and biking routes to neighborhood features including commercial areas, public spaces, recreational facilities, and schools.

LU25: Require new development and redevelopment to be compatible with the existing neighborhood character.

T11: Design safe and accessible pedestrian ways connecting major destinations throughout the City including transit corridors.

ED14: Support the development of a complete multimodal transportation network for pedestrians, bicyclists, transit, and automobiles.

CCN5: Ensure infill development is sensitive to the existing character of the neighborhood.

CCN18: Promote compact, mixed-use development to provide adequate density to support neighborhood commercial viability.

CCN22: Encourage neighborhood nodes that are accessible by pedestrians, bicyclists, and transit users, as well as motorists.

The adjoining segment of the East Grand Avenue corridor predominately consists of a traditional, walkable commercial district. Most buildings have minimal setbacks from the street, particularly those that are the most recent to be developed. The building stock is a mix of historic commercial buildings and modern infill construction.

The applicant is proposing to redevelop the site with a new convenience store building and site improvements. The building would be located slightly to the east of the center of the site. It would be separated from the adjoining Rights-Of-Way by surface parking. The fuel pump island would be located to the west of the building along East 15th Street. The submitted Site Plan shows two driveways to East Grand Avenue, one driveway to East 15th Street and one driveway to Capitol Avenue. The Site Plan proposed to remove the existing access drive that provides ingress/egress from the west end of the alley to Capitol Avenue. The portion of the alley that used to extend through the site to East 15th Street was previously vacated and incorporated into the Quik Trip site. The alley is shown as a dead end at the east perimeter of the site, closed off with bollards.

The proposed layout consists of a standard, auto-oriented design. It does not support the existing pedestrian-oriented character of the East Grand Avenue corridor, nor does it comply with the goals of the City's Comprehensive Plan as listed above. Staff understands that achieving a mixed-use project at this site that includes a convenience store with fuel sales is not practical. However, the site should still be configured in a way that the building is located along East Grand Avenue, which would better fit the

D. All development proposals should show evidence of coordination with the Site Plan as well as arrangement of buildings and planning elements of neighboring properties by:

(i) **Responding to local development patterns and the streetscape by use of consistent building setbacks, orientation and relationship of structures to the street and linkages to pedestrian facilities;**

- (ii) Seeking shared-access with adjoining commercial uses where feasible to minimize curb cuts and enhance pedestrian and vehicular circulation;
- (iii) Minimizing cross traffic conflicts within parking areas.

The adjoining segment of East Grand Avenue corridor predominately consists of a traditional, walkable commercial district. Most buildings have minimal setbacks from the street, many with a zero setback on the north side of East Grand Avenue.

The applicant is proposing to redevelop the site with a new convenience store building and site improvements. The building would be located on the eastern half of the site. It would be separated from the adjoining rights-of-way by parking. The fuel pump island would be located to the west of the building along East 15th Street. The submitted Site Plan shows two driveways to East Grand Avenue, one driveway to East 15th Street and one driveway to Capitol Avenue. The Site Plan includes removing the existing access drive that provides segregated egress from the west end of the alley to Capitol Avenue. The portion of the alley that used to extend through the site to East 15th Street was previously vacated and incorporated in the Quik Trip site. The submitted alley is shown as a dead end at the east perimeter of the site.

The proposed layout consists of a standard, auto-oriented design. It does not support the existing pedestrian-oriented character of the East Grand Avenue corridor, nor does it comply with the goals of the City's Comprehensive Plan as listed above. Staff understands that achieving a mixed-use project at this site that includes a convenience store with fuel sales is not practical. However, the site should be configured in a way that the building is located along East Grand Avenue to address the pedestrian-oriented corridor, which would better fit the existing character, support greater pedestrian connectivity, and address the goals of the PlanDSM Creating Our Tomorrow. Staff has presented this option to the applicant.

E. The Site Plan shall mitigate the negative impacts from site activities on adjoining uses as follows:

- (i) Service areas, storage areas and refuse enclosures should be oriented away from public view and screened from adjacent sites;
- (ii) Drive-through windows, menu boards and associated stacking lanes should be oriented away from residential areas or screened from public view;
- (iii) Auto repair bay openings and car-wash openings should be oriented away from residential uses;
- (iii) Lighting should be non-invasive to adjoining residential use.

or in special instances a predominant material found in the surrounding commercial area. Columns should be designed to minimize visual impact.

The three primary facades of the proposed building would be sided with brick and porcelain tile and would have aluminum cornices and entrance canopies. The rear façade would be sided with brick with no other detail. Staff recommends that porcelain elements be provided on the rear façade along with cornice design to match the other three facades. This is based on the east side of the building being within public view of East Grand Avenue.

- E. Walls, pump island canopies and other outdoor covered areas should be compatible with the building, using similar material, color and detailing.

The proposed fuel pump island canopy would be sided with metal and supported by columns wrapped in brick to match the building.

- F. To encourage visually interesting roofs, variations in the roof line and treatments such as extended eaves and parapet walls with cornice treatments are encouraged.

The proposed building design includes wall plane variation and parapet wall height on the three street facing facades.

- G. Perceived height and bulk should be reduced by dividing the building mass into smaller-scaled components. Examples of treatments that could be used to avoid excessive bulk and height include:
- (i) Low-scale planters and site walls.
 - (ii) Wainscot treatment.
 - (iii) Clearly pronounced eaves or cornices.
 - (iv) Subtle changes in material color and texture.
 - (v) Variation in roof forms.
 - (vi) Covered pedestrian frontages and recessed entries.
 - (vii) Deeply set windows with mullions.

The proposed building design includes wall plane variation, material variety and varying parapet wall heights on the three street facing facades.

- H. Canopies:

- (i) Integration of materials on canopies that are similar or compatible to those used on the building or site walls is desirable (e.g., wrap the canopy columns with brick that matches the building). Multiple canopies or canopies that express differing masses are encouraged.

The proposed fuel pump island canopy would be sided with metal and supported by columns wrapped in brick to match the building. The double array of pump apparatus would minimize the expanse of the canopy appearance from all directions.

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- E. Proper maintenance and timely replacement of plant material is required and will be enforced based on the approved Site Plan.

The submitted site plan does not comply with the City's Landscaping Standards. Minimum open space should be 20% of the site or 1,000 square feet per vehicle fueling location, whichever is greater. The greater requirement in this case is the 20% provision or 15,424 square feet. The Site Plan proposes 13,807 square feet (17.9%) of open space. The pavement and/or building footprint would need to be reduced to meet this requirement. Additional open space could also be obtained by expanding interior planting islands throughout the site.

Currently portions of the required landscaping (open space, parking perimeter, interior lot, and bufferyard) are shown within the Right-Of-Way areas which would not comply. The requirements for these landscaping elements must be satisfied within the private property.

The Site Plan needs to be revised to provide a 10-foot bufferyard where adjacent to the residential use to the east. Minimum planting requirements for bufferyards are two (2) overstory tree and six (6) evergreen trees per 100 lineal feet of property line, in addition to the 6-foot screen. There is approximately 130 lineal feet of required bufferyard along the east property line demanding this requirement. Should the alley egress be provided in this location, the bufferyard should be between the alley and the existing residences.

Perimeter lot plantings are required within the minimum 10-foot paving setback along all street side property lines of parking lots. Currently the setbacks are shown at 5.5 feet along some portions of Capitol Avenue and 7.93 feet along portions of East Grand Avenue and do not comply. All perimeter lot setbacks shall have a minimum 10-foot setback based on the amount of paving.

The interior parking lot minimum plantings require one (1) overstory deciduous tree and three (3) shrubs for every 20 parking spaces provided. They shall be within planter beds of no less than nine (9) by seventeen (17) or 153 square feet. The plans identify the islands with trees but no shrubs have been provided.

To meet the intent of PlanDSM by tying into the existing pedestrian corridor, staff recommends providing parkway planting requirements. This would call for one overstory tree per 30 lineal feet within the Right-Of-Way in 5-foot by 15-foot planters, 1 foot back of curb within the sidewalk along East Grand Avenue, with a 6-foot dedicated walk width.

To meet the design guideline for enhancing the overall appearance, staff recommends providing a minimum 42-inch tall decorative black metal fence around the perimeter of the site along East Grand Avenue and East 15th Street. This would not have to be continuous but could be broken up with other landscape features. Shrub landscaping that is indicated on the landscape plan is located on the outside, or street facing side of the fence.

of the total sign height. Monument signs are sometimes referred to in this chapter as monumental signs.

4). Lighting.

- A. Lighting of gas stations and convenience stores should enhance safety and provide light levels appropriate to the visual task with minimal glare, light trespass and excess site brightness. Lighting should not be a nuisance or a hazard.
- B. Direct light trespass beyond property lines is prohibited. The maximum horizontal illuminance at grade and the maximum vertical illuminance at five feet above grade measured at the property line should not exceed Illuminating Engineering Society of North America (IESNA) recommended practices for light trespass. (0.5 footcandles for residential, 2.0 footcandles for commercial). The Site Plan must contain illuminance models showing light levels throughout the site.

The site is surrounded by commercial and civic uses except residential property to the east. The submitted photometric plan indicates compliance with these requirements except a couple locations along the east property line would slightly exceed the 0.5 footcandles that would be required for adjacency to residential use.

- C. Light fixtures mounted under canopies should be completely recessed into the canopy with flat lenses that are translucent and completely flush with the bottom surface (ceiling) of the canopy. Generally, lights shall not be mounted on the top or sides (fascia) of the canopy and internally illuminated/entirely translucent canopies should be prohibited. However, accent lighting on the sides (fascia) of the canopy may be permitted.

The submitted drawings indicate the fuel pump island canopy would have recessed lights with lenses that are flush with the bottom surface of the canopy.

Due to the proximity of the Capitol View Elementary School, as well as current neighborhood planning efforts, Traffic Engineering disagrees with this statement and believes that introducing a commercial access point on Capitol Avenue would be contrary to efforts to prioritize walkability in the vicinity of schools in Des Moines. Capitol Avenue is on the designated school walk route for Capitol View Elementary and is used by parents, students, and school buses to access the school.

A sidewalk connection from the new QT to Capitol Avenue should be provided.

Additional response from Traffic Engineering to the submitted traffic study included:

- A) The report recommended investigating the addition of traffic signal heads for the northbound approach of East 15th Street and East Grand Avenue to potentially address crash trends at the intersection. The City will investigate further.
 - B) The report included a discussion about the crashes happening along East Grand Avenue, east of East 15th Street and suggested that a center turn lane may help to address the left turn and failure to yield from driveway crashes. There were only two reported crashes in the studied time period that may have involved motorists exiting the QT site on East Grand Avenue, therefore no significant trend currently exists that would necessitate immediate changes in association with the site redevelopment. The City will investigate the possibility of a center left turn lane on East Grand Avenue in conjunction with the results of the Walkability Study later in 2017.
5. **Parking:** A minimum of 20 off-street parking spaces are required for the proposed 5,773-square foot building (1 per 300 square feet). The applicant is proposing 62 parking spaces. The PlanDSM seeks to minimize off-street parking area. In this instance the submitted plan provided over three times the required minimum. Based on this staff believes that reducing the number of parking spaces to revise the building location could be accomplished without compromising necessary parking.
 6. **Drainage/Grading:** The submitted Site Plan indicates storm water management being handled by a surface detention basin at the eastern end of the site. This may need to be redesigned depending on the solution provided for egress of the east/west alley.
 7. **Beer and Wine Permit/Liquor License:** The existing store sells beer and wine only. For beer and wine sales, the site would be required to maintain a 150 foot separation from a church, park, school, or licensed day care use. This proposed site would not currently meet those requirements with only 80 feet of separation from Capitol View Elementary School. However, if the applicant does not surrender a valid beer and wine permit for the property and keeps it valid during the development of a new convenience store, there would not be a loss of legal non-conforming rights to the existing separations. Convenience stores do not require a Conditional Use Permit for the sale of beer and wine only.
 8. **Staff Rationale:** Based on review of the proposed rezoning, which must be found in accordance with PlanDSM: Creating Our Tomorrow, staff does not believe the development concept reinforced with the submitted Site Plan meets the intent of the City's Comprehensive Plan. Therefore, staff recommends denial of the requested

SAA

Mike Talcott with QuikTrip Corp., 5725 Fox Ridge Dr., Mission, KS, stated when they originally modeled the current store they bought the property, moved the alley, and gave the city the easement. On this plan they modeled it similar initially, but after consulting with city representatives and the Real Estate Department, they learned that all homes on Capitol Avenue have access to their driveways from the street and the alley is not necessary for them to gain access to their property driveways. The consensus was that the residents did not need this alley, but they were certainly open to a solutions.

CHAIRPERSON OPENED THE PUBLIC HEARING

Jean Schooley 1554 Capitol Avenue, member of Capitol East Neighborhood Association stated that the alley is needed as people use this alley to take and pick up children from the school located on the other side of Capitol Avenue. It is necessary as 15th Street is one-way in front of school.

Mike Simonson inquired if the neighborhood association had taken a vote.

Jean Schooley stated they did not.

Mike Talcott stated that he had attended the neighborhood meeting on September 14, and felt that the neighborhood supported the new store and were excited to have it in their area. He also noted their long history in this neighborhood and how they had initially come in and purchased a dilapidated building and cleaned up the street. They have been successful in serving this neighborhood and wish to improve and continue on in this area.

Mike Simonson wanted an explanation on zoning from staff as to how this compares to Dahl's on Ingersoll where they were required to bring their building up to the street, and then the new QuikTrip store on Keo Way. What was that zoning and why was that required, and how is this site different or the same? What is the mechanism triggering the comment for the requirement to pull this building forward?

Mike Ludwig explained at the time Dahl's C-Store on Ingersoll was part of a rezoning to PUD and was part of the PUD negotiation. The store on Keo did not get pulled up to the corner, rather it was approved by City Council despite the recommendation of staff and the Planning Commission. It was also considered prior to the adoption of Plan DSM. In light of the new Plan DSM Comprehensive Plan being adopted, staff is trying to do their best to implement the goals and visions of that plan with the existing codes. Just like other Comprehensive Plans adopted in the past, such as the Community Character 2020 Plan, we implemented those plans with the existing code that was originally adopted in 1965. We have probably had three different Comprehensive Plans that we have tried to implement with the existing code, so implementing Plan DSM with existing code is really nothing new. We are trying to write a new code, which may mandate building placement rather than utilize design guidelines. We are making staff recommendations in light of our new comprehensive plan. We have given latitude to other projects such as SW 9th and Army Post Road, but they have a different character of the surroundings and are not currently a pedestrian corridors. The store on MLK and Hickman does not carry the same opportunity for redevelopment as the area we are currently considering. We must consider the personality or character of each area. Staff feels strongly that this area along East Grand needs to have at the least the end of the store up to the street along Grand Avenue. We have done a significant amount of work contributing to the NPC character along this

Ben Bruner stated he does not agree. This is a corporate decision and what they have works. They have considered every option and what they have is what they need. They believe they are improving it from a pedestrian stand point by adding the walkway and not going through the driveway entrance. There is a consideration of the internal site functionality and safety. This is a unique site in the sense you have the pedestrian pathway on Grand and then highway oriented on E.15th. It's tough to say it's in the middle of a neighborhood that they need to fully integrate it to the street frontage. From a practical stand point, it's somewhat a corporate impossibility to do too much with it. From a procedural stand point, Erik noted if the rezoning is approved, the request from staff is to continue the site plan. Their request is for a decision on the site plan and not a continuance at this point. This is a project that QuikTrip is ready to green light or possible park.

Mike Talcott added reasons not to anchor store to the north sidewalk. Their new design segregates gas customers from store customers. Moving the store disrupts their flow taking away store parking and forcing people to fight for parking along with gas customers when only coming to the store for merchandise. In essence, they have created a separate parking area for customers and this will set them back not to have it this way. They have improved pedestrian access, they are pre-certified ADA complainant and the site plan will reveal this much safer and functional. Corporate hopes they can consider the site plan as it has been presented as the best option for this location.

Mike Simonson believes there are other things they could do to create a street presence if they absolutely cannot move the building such as brick piers or a trellis. He is frustrated that his is not hearing anything further to benefit the streetscape.

Mike Talcott responded that this is something that they have worked with on other locations and they are willing to consider items like this to create that edge. As of now, with what he has been presented, staff recommendation is not considering anything other than moving the building.

Mike Simonson stated there is a reason staff wants the building moved, but if you cannot do it, are there other ideas that would help mitigate their concerns?

Mike Talcott stated they are absolutely willing to work on the streetscape.

Rocky Sposato asked if what QuikTrip is proposing now is compliant with the current codes or are we tying it to future codes that have not been written yet?

Mike Ludwig stated that staff recommends that the proposed site plan is not compliant with the existing code when the design criteria, which are very subjective criteria, are viewed in light of PlanDSM.

CHAIRPERSON CLOSED THE PUBLIC HEARING

Greg Jones stated that he believes we have discovered that our current ordinance for C-Stores does not match our new comprehensive plan. He is not sure if that if all C-Stores are unwilling to put their building face along a pedestrian corridor that we are ever going to mate the two. They all do the same thing wanting parking around their building. Even

Glenna Frank agreed it should be both.

COMMISSION ACTION:

Mike Simonson moved staff recommendation that the proposed rezoning be found not in conformance with the existing PlanDSM Creating Our Tomorrow future land use designation for the property known as 1522 Capitol Avenue.

THE VOTE: 11-0

Mike Simonson moved approval to amend the existing PlanDSM: Creating Our Tomorrow Plan future land use designation from Low Density Residential within a Neighborhood Node to Community Mixed Use within a Neighborhood Node.

THE VOTE : 7-4 (Mike Simonson, Jaqueline Easley, Steve Wallace, JoAnne Corigliano, Carolyn Jenison, Francis Boggus, and Dory Briles voted in favor. David Courard-Hauri, John "Jack" Hilmes, Greg Jones, and Rocky Sposato voted in opposition).

****6/7 of vote is required by the council as the land use plan amendment requires affirmative vote by 8 members of P&Z (2/3 of 11 members present)***

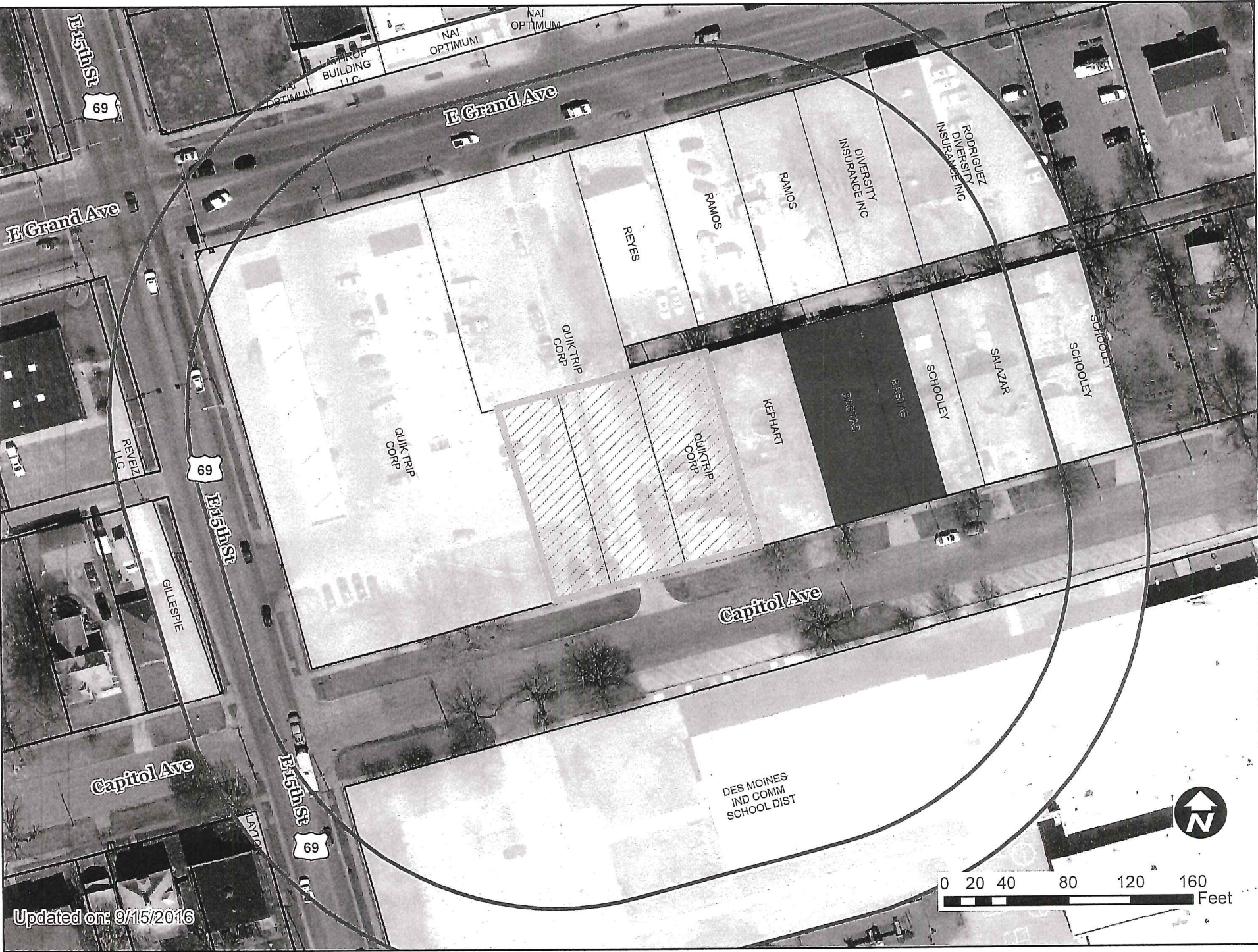
Mike Simonson moved approval of the request to rezone 1522 Capitol Avenue from "R1-60" One-Family Low-Density Residential District to "C-2" General Retail and Highway-Oriented Commercial District and rezone a portion of 1515 East Grand Avenue from Limited "C-2" General Retail and Highway-Oriented Commercial District to revise the conditions to remove the condition prohibiting the commercial site development to access Capitol Avenue, subject to the following conditions:

1. The public alley within the Property shall not be truncated and shall be connected to Capitol Avenue.
2. The applicant shall work with City staff to create an urban edge for the north streetscape (Grand Avenue frontage), to the approval of the Planning Administrator.

THE VOTE: 7-4 (Mike Simonson, Jaqueline Easley, Steve Wallace, JoAnne Corigliano, Carolyn Jenison, Francis Boggus, and Dory Briles voted in favor. David Courard-Hauri, John "Jack" Hilmes, Greg Jones, and Rocky Sposato voted in opposition).

Mike Simonson moved approval of a Site Plan under design guidelines for gas stations and convenience stores on property located at 1501 & 1515 East Grand and 1522 Capitol Avenue, to allow demolition of the existing convenience store and pump island canopy and to construct a 5,773-square foot convenience store with a pump island canopy having 12 fueling locations expandable to 16 fueling locations and allowing commercial site development to access Capitol Avenue, subject to the following conditions:

1. The public alley within the Property shall not be truncated and shall be connected to Capitol Avenue.
2. The applicant shall work with City staff to create an urban edge for the north streetscape (Grand Avenue frontage), to the approval of the Planning Administrator.



501X

SITE PLANS FOR QUIKTRIP STORE #0535 1501 EAST GRAND AVENUE CITY OF DES MOINES, COUNTY OF POLK, STATE OF IOWA

GENERAL NOTES:

- A. IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, THE CONTRACTOR SHALL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS OF THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK. THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS. ANY CONSTRUCTION OBSERVATION BY THE ENGINEER OF THE CONTRACTOR'S PERFORMANCE IS NOT INTENDED TO INCLUDE REVIEW OF THE ADEQUACY OF THE CONTRACTOR'S SAFETY MEASURES, IN, ON OR NEAR THE CONSTRUCTION SITE.
- B. CONTRACTOR SHALL BE RESPONSIBLE FOR PAVING AND REMOVAL OF THE EXISTING STRUCTURES, RELATED UTILITIES, AND ANY OTHER EXISTING IMPROVEMENTS AS NOTED. REF. SITE WORK SPECIFICATIONS.
- C. CONTRACTOR IS TO REMOVE AND DISPOSE OF ALL DEBRIS, RUBBISH AND OTHER MATERIALS RESULTING FROM PREVIOUS AND CURRENT DEMOLITION OPERATIONS. DISPOSAL WILL BE IN ACCORDANCE WITH ALL LOCAL, STATE AND/OR FEDERAL REGULATIONS GOVERNING SUCH OPERATIONS.
- D. THE GENERAL CONTRACTOR WILL BE HELD SOLELY RESPONSIBLE FOR AND SHALL TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES DURING THE CONSTRUCTION PHASES OF THIS PROJECT.
- E. ALL WORK IN ROW SHOULD BE IN ACCORDANCE WITH SUDAS AND CITY SUPPLEMENTAL SPECIFICATIONS EFFECTIVE AT THE TIME OF SITE PLAN APPROVAL.
- F. ALL SITE WORK FOR THIS PROJECT SHALL MEET OR EXCEED THE SPECIFICATIONS OF THE RELEVANT UTILITY COMPANY OR REGULATORY AUTHORITY, AND THE SPECIFICATIONS FOR THE CONSTRUCTION OF THE EXISTING IMPROVEMENTS WHICH ARE BEING ALTERED OR REPLACED. CONTRACTOR SHALL CONTACT THE ENGINEER FOR SPECIFICATION SECTIONS FOR ITEMS SUCH AS LANDSCAPING AND IRRIGATION THAT ARE AFFECTED BY THE WORK BUT NOT COMPLETELY DETAILED OR SPECIFIED ON THESE PLANS.

WETLANDS NOTICE:

ANY DEVELOPMENT, EXCAVATION, CONSTRUCTION, OR FILLING IN A U.S. CORPS OF ENGINEERS DESIGNATED WETLAND IS SUBJECT TO LOCAL, STATE AND FEDERAL APPROVALS. THE CONTRACTOR SHALL COMPLY WITH ALL PERMIT REGULATIONS AND RESTRICTIONS AND ANY VIOLATION WILL BE SUBJECT TO FEDERAL PENALTY. THE CONTRACTOR SHALL HOLD THE OWNER/DEVELOPER, THE ENGINEER AND THE LOCAL GOVERNING AGENCIES HARMLESS AGAINST SUCH VIOLATIONS.

WARRANTY / DISCLAIMER:

THE DESIGNS REPRESENTED IN THESE PLANS ARE IN ACCORDANCE WITH ESTABLISHED PRACTICES OF CIVIL ENGINEERING FOR THE DESIGN FUNCTIONS AND USES INTENDED BY THE OWNER AT THIS TIME. HOWEVER, NEITHER THE ENGINEER NOR HIS PERSONNEL CAN OR DO WARRANT THESE DESIGNS OR PLANS AS CONSTRUCTED EXCEPT IN THE SPECIFIC CASES WHERE THE ENGINEER INSPECTS AND CONTROLS THE PHYSICAL CONSTRUCTION ON A CONTINGUOUS BASIS AT THE SITE.

TRAFFIC CONTROL NOTICE:

THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING TRAFFIC CONTROL FOR ANY WORK PERFORMED WITHIN STREET RIGHT-OF-WAY. TRAFFIC CONTROL SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), CURRENT EDITION AND CITY OF DES MOINES SPECIFICATIONS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ENSURE PROPER TRAFFIC CONTROL IS IN PLACE FOR EACH PHASE OF CONSTRUCTION. THE CONTRACTOR IS ALSO RESPONSIBLE FOR PROPERLY MAINTAINING TRAFFIC CONTROL DEVICES THROUGHOUT THE DURATION OF THE WORK. THE CONTRACTOR SHALL PROVIDE TRAFFIC CONTROL PLANS TO THE CITY OF DES MOINES AS REQUIRED.

FLOOD CERTIFICATION:

ACCORDING TO THE FEDERAL EMERGENCY MANAGEMENT AGENCY FLOOD MAP SERVICE THE SUBJECT PROPERTY IS LOCATED IN FEMA FLOOD PANELS NO. 190227050 & 190227070. SUBJECT PROPERTY IS LOCATED IN ZONE X, WHICH IS DEFINED AS AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN. BOTH MAPS HAVE AN EFFECTIVE DATE OF SEPTEMBER 18, 1987.

HORIZONTAL DATUM:

NAD83, IOWA STATE PLANE, SOUTH ZONE, US FOOT

VERTICAL DATUM:

BM-5695 (CITY OF DES MOINES) CDM BRASS CAP ON THE NORTHWEST CORNER OF THE TRAFFIC SIGNAL BASE LOCATED AT THE SOUTHWEST CORNER OF EAST GRAND AVENUE AND EAST 15TH STREET. ELEVATION = 82.408

BM-1055 (CITY OF DES MOINES) TRINGLE CUT IN THE SOUTH CORNER OF THE TRAFFIC SIGNAL BASE LOCATED AT THE NORTHWEST CORNER OF WALSH AVENUE AND EAST 15TH STREET. ELEVATION = 91.402

NAVD83 CONVERSION = CITY DATUM + 773.94'

PROPERTY DESCRIPTION

1502 CAPITAL AVENUE, DES MOINES, IOWA
LOT 6 IN BLOCK 45 IN STEWART'S ADDITION, AN OFFICIAL PLAT, ALL NOW INCLUDED IN AND FORMING A PART OF THE CITY OF DES MOINES, POLK COUNTY, IOWA.

1515 E. GRAND AVENUE, DES MOINES, IOWA
LOT 5 AND LOTS 24 AND 25 AND ALL THE PART OF THE 18.0 FOOT EAST/WEST ALLEY RIGHT-OF-WAY LYING NORTH OF AND ADJOINING LOTS 4 AND 5 ALL IN BLOCK 43 IN STEWART'S ADDITION, AN OFFICIAL PLAT, ALL NOW INCLUDED IN AND FORMING A PART OF THE CITY OF DES MOINES, POLK COUNTY, IOWA.

1501 E. GRAND AVENUE, DES MOINES, IOWA
LOTS 1, 2, 3, AND 4 AND LOTS 26 AND 27 AND LOT 28 (EXCEPT THE EAST 3 FEET OF THE WEST 12 FEET OF THE NORTH 3 FEET AND A TRIANGULAR PARCEL IN THE NORTHWEST CORNER MEASURING 5 FEET ON THE NORTH LINE AND 5 FEET ON THE WEST LINE) AND THAT PART OF THE VACATED EAST/WEST ALLEY RIGHT-OF-WAY LYING NORTH OF AND ADJOINING LOTS 1, 2, AND 3 ALL IN BLOCK 43 IN STEWART'S ADDITION, AN OFFICIAL PLAT, NOW INCLUDED IN AND FORMING A PART OF THE CITY OF DES MOINES, POLK COUNTY, IOWA.

MUNICIPAL AND PRIVATE UTILITY CONTACT LIST:

- | | | |
|--|--|---|
| <p>COMMUNITY DEVELOPMENT DEPARTMENT
PERMIT & DEVELOPMENT CENTER
PLANNING DIVISION
902 ROBERT D RAY DRIVE
DES MOINES, IOWA 50309
TEL: (515) 283-4981
FAX: (515) 283-4270
CONTACT: JILLIAN SOMMER</p> | <p>POTABLE WATER
DES MOINES WATER WORKS
2201 GEORGE FLAGG PARKWAY
DES MOINES, IOWA 50321
TEL: (515) 283-8720
FAX: (515) 283-2510
CONTACT: JANA HODGES</p> | <p>NATURAL GAS
MIDAMERICAN ENERGY
TEL: (515) 282-8832
CONTACT: CRAIG RANFELD</p> |
| <p>COMMUNITY DEVELOPMENT DEPARTMENT
PERMIT & DEVELOPMENT CENTER
ENGINEERING DIVISION
902 ROBERT D RAY DRIVE
DES MOINES, IOWA 50309
TEL: (515) 283-4995
FAX: (515) 283-4270
CONTACT: CHARLES LEPAK</p> | <p>SANITARY SEWER
ENGINEERING DIVISION
902 ROBERT D RAY DRIVE
DES MOINES, IOWA 50309
TEL: (515) 283-4995
FAX: (515) 283-4270
CONTACT: CHARLES LEPAK</p> | <p>TELECOMMUNICATIONS
CENTURYLINK
TEL: (503) 694-8080
CONTACT: TOM STURMER</p> |
| <p>ENGINEERING DEPARTMENT
TRAFFIC & TRANSPORTATION DIVISION
902 ROBERT D RAY DRIVE
DES MOINES, IOWA 50309
TEL: (515) 283-4126
CONTACT: JENNIFER DAKOVICH</p> | <p>STORM SEWER
ENGINEERING DIVISION
902 ROBERT D RAY DRIVE
DES MOINES, IOWA 50309
TEL: (515) 283-4270
CONTACT: CHARLES LEPAK</p> | <p>TELECOMMUNICATIONS
IOWA COMMUNICATIONS NETWORK
TEL: (515) 725-4742
CONTACT: JONAS BRILSHESHER</p> |
| <p>FREE DEPARTMENT
2715 DEAN AVENUE
DES MOINES, IOWA 50309
TEL: (515) 237-1316
FAX: (515) 283-4907
CONTACT: PATRICK PHELAN</p> | <p>ELECTRICAL POWER
MIDAMERICAN ENERGY
TEL: (515) 282-8832
CONTACT: CRAIG RANFELD</p> | <p>TELECOMMUNICATIONS
IOWA NETWORK SERVICES
TEL: (515) 630-6448
CONTACT: JEFF KLOCKO</p> |
| <p>ENGINEER OF RECORD
MCCLURE ENGINEERING
1360 NORTHWEST 121ST STREET
CLIVE, IOWA 50325
TEL: (515) 964-1229
FAX: (515) 964-2370</p> | <p>DI. REAL ESTATE PROJECT MANAGER
QUIKTRIP CORPORATION
5725 FORBES DRIVE
MISSION, KANSAS 66202
TEL: (913) 905-2088
FAX: (913) 905-2080</p> | <p>TELECOMMUNICATIONS
METADOM COMMUNICATIONS
TEL: (515) 246-2252
CONTACT: PAUL MAY</p> |

PROJECT CONTACT LIST:

- | | |
|--|---|
| <p>ENGINEER OF RECORD
MCCLURE ENGINEERING
1360 NORTHWEST 121ST STREET
CLIVE, IOWA 50325
TEL: (515) 964-1229
FAX: (515) 964-2370</p> | <p>DI. CIVIL PROJECT MANAGER
QUIKTRIP CORPORATION
4205 SOUTH 125TH EAST AVENUE
TULSA, OKLAHOMA 74134
TEL: (918) 615-7942</p> |
|--|---|



SHEET INDEX	
SHEET NO.	SHEET TITLE
1	COVER
2	DEMOLITION PLAN
3	SITE PLAN
4	GRADING PLAN
5-8	DETAILED GRADING PLAN
7	STORM SEWER PLAN
8	STORM WATER MANAGEMENT FACILITIES PLAN
9	EROSION CONTROL PLAN
10	UTILITY PLAN
11	TREE MITIGATION PLAN
12	LANDSCAPE PLAN
13-16	DETAILS

DEVELOPMENT SUMMARY	
GROSS PROPERTY AREA	77,117 S.F. ± (1.77 ACRES ±)
BUILDING:	
TYPE OF USE	CONVENIENCE STORE WITH FUEL
HEIGHT	22'-0"
GROSS FLOOR AREA	5,773 S.F.
CANOPY:	
DISPENSER ARRANGEMENT	5 DISPENSERS (DOUBLE) WITH 2 FUTURE DISPENSERS
GROSS CANOPY AREA	5,418 S.F.
HEIGHT	18'-0" MIN. / 28'-0" MAX.
OPEN SPACE:	
REQUIRED	15,424 S.F. (20.0%)
PROVIDED	13,807 S.F. (17.8%)
IMPERVIOUS AREA:	
EXISTING	54,559 S.F. (70.7%)
PROPOSED	63,219 S.F. (82.1%)
REQUIRED PARKING	
STANDARD	19
ADA	1
TOTAL	20
STANDARD	59
ADA	3
TOTAL	62

ZONING SUMMARY	
ZONING:	C-2 (GENERAL RETAIL AND HIGHWAY ORIENTED COMMERCIAL DISTRICT)
MINIMUM LOT AREA (SQUARE FEET)	NONE
MINIMUM LOT WIDTH (FEET)	NONE
SITE AREA PER ACR (SQUARE FEET)	NONE
BUILDING:	
FRONT YARD SETBACK (FEET)	25
SIDE YARD SETBACK (FEET)	N/A
REAR YARD SETBACK (FEET)	100
REQUIRED OPEN SPACE	20%
MAXIMUM HEIGHT (FEET)	45

I HEREBY CERTIFY THAT THE PORTION OF THIS TECHNICAL SUBMISSION DESCRIBED BELOW WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND RESPONSIBLE CHARGE. I AM A LICENSED PROFESSIONAL ENGINEER IN THE STATE OF IOWA.

PRELIMINARY

DATE: _____
SHEET 11-12

I HEREBY CERTIFY THAT THIS ENGINEERING DOCUMENT WAS PREPARED BY ME OR UNDER MY DIRECT PERSONAL SUPERVISION AND THAT I AM A LICENSED PROFESSIONAL ENGINEER IN THE STATE OF IOWA.

PRELIMINARY

DATE: _____
SHEETS 1-10

SITE PLAN

APPROVED APPROVED WITH CONDITIONS
See Exhibit "A" attached hereto.

IN ACCORDANCE WITH SECTION 88-207 (a) 2000 Des Moines MUNICIPAL CODE AS AMENDED.

NO CHANGES TO THIS PLAN UNLESS APPROVED IN WRITING FROM THE PLANNING DIRECTOR OR NEW AMENDED DATED PLAN.

DATE: _____ PLANNING DIRECTOR



PROJECT NO. 071461

QuikTrip No. 0535

1501 EAST GRAND AVENUE
DES MOINES, IA

DATE: _____

DESIGNER: _____

ENGINEER: _____

PROJECT TITLE: _____

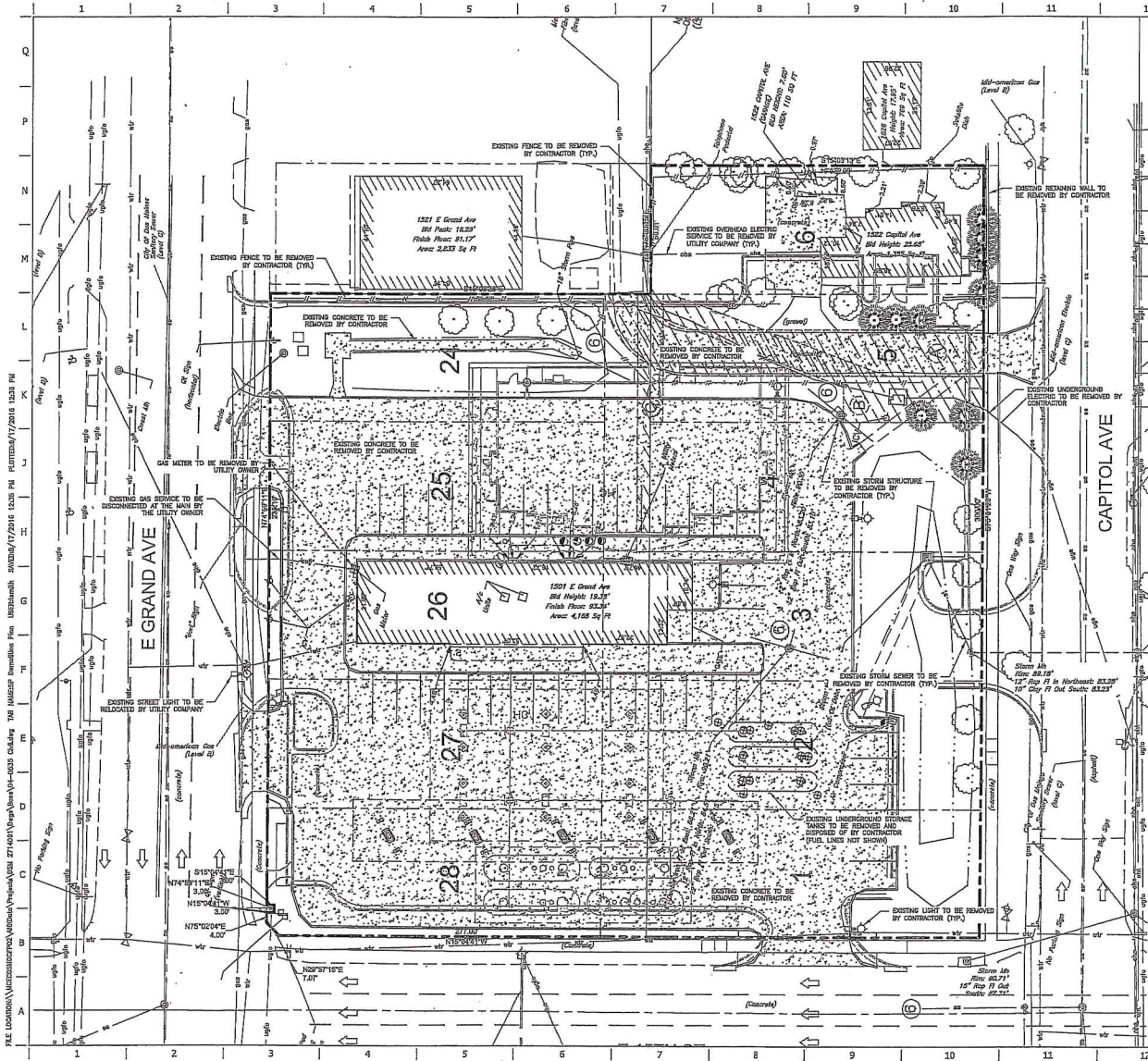
COVER

SHEET NUMBER: _____

1

ORIGINAL ISSUE DATE: 02-23-16

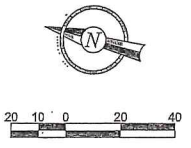
ITEM #1



- ### GENERAL NOTES
1. THE DEMOLITION CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING AND PAYING FOR ALL CITY, COUNTY, STATE AND FEDERAL PERMITS REQUIRED TO COMPLETE THE DEMOLITION WORK.
 2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING AND REMOVAL OF THE EXISTING STRUCTURES (INCLUDING FOUNDATIONS & TRUNKING) RELATED UTILITIES, PAVING, AND ANY OTHER EXISTING IMPROVEMENTS AS NOTED AND/OR NECESSARY FOR THE CONSTRUCTION OF THE PROPOSED IMPROVEMENTS, UP TO HIS WORK SPECIFICATIONS.
 3. CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT ANY REMOVED OR BAZED ITEM OR FACILITY SHALL BE, BUT NOT LIMITED TO, UNDERGROUND STORAGE TANKS, BAZED HOMES, FOUNDATIONS, AND BASEMENTS HAVE BEEN OR WILL BE REPLACED WITH APPROPRIATE MATERIAL AND CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROFESSIONAL REPORT AND PROJECT SPECIFICATIONS. THE THIRD PARTY TESTING FIRM MUST CONTROL DEMOLITION OPERATIONS AND COMPLIANCE WITH STATE AND FEDERAL. THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING THESE TESTING REPORTS AT HIS EXPENSE.
 4. THE CONTRACTOR IS TO REMOVE AND DISPOSE OF (OFF-SITE) ALL DEBRIS, RUBBISH AND OTHER MATERIALS RESULTING FROM PROVIDED AND SUSPENDED DEMOLITION OPERATIONS. DISPOSAL WILL BE IN ACCORDANCE WITH ALL LOCAL, STATE AND/OR FEDERAL REGULATIONS GOVERNING SUCH OPERATIONS. ANY MATERIALS UNWISELY SHALL BE DISPOSED OF IN ACCORDANCE WITH APPLICABLE ENVIRONMENTAL LAWS. CONTRACTOR TO CONTACT QUANTRO ENVIRONMENTAL COMPLIANCE ENGINEER WITH DISPOSAL PLAN.
 5. THE CONTRACTOR SHALL PROVIDE PROPER PROTECTION OF EXISTING FEATURES THAT ARE TO REMAIN TO THE SATISFACTION OF THE OWNER AND GOVERNING AGENCIES.
 6. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE TO DETERMINE THE FULL EXTENT OF DEMOLITION REQUIRED.
 7. THE CONTRACTOR SHALL PROVIDE A FULL DEPTH SAW CUT WHERE PAVEMENT DEMOLITION ARE PAIDMENT THAT REMAINS IN PLACE. WHERE NEW PAVEMENT IS TO BE PLACED ADJACENT TO THIS SAW CUT, THE SAW CUT EDGE IS TO BE PROTECTED UNTIL NEW PAVEMENT IS PLACED. IN THE EVENT THE SAW CUT BECOMES DAMAGED THE CONTRACTOR SHALL PROVIDE A NEW SAW CUT TO ESTABLISH A "CLEAN" EDGE FOR THE NEW PAVEMENT TO ADJ.
 8. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING ALL EXISTING UTILITY SERVICE DISCONNECTIONS WITH THE GOVERNING AGENCIES.
 9. REFER TO EROSION CONTROL PLANS FOR EROSION CONTROL MEASURES THAT MUST BE IN PLACE PRIOR TO PERFORMING DEMOLITION OPERATIONS.
 10. THE CONTRACTOR WILL BE RESPONSIBLE FOR PROVIDING A TRAFFIC CONTROL PLAN IF A LANE CLOSURE IS REQUIRED FROM WORK ADJACENT TO PUBLIC STREET.
 11. THE DEMOLITION CONTRACTOR IS RESPONSIBLE FOR RETURNING ALL EXISTING UTILITY METERS, EQUIPMENT, ETC AS REQUIRED BY THE UTILITY COMPANIES AND RETURNING PRODUCE GOODS TO THE OWNER WITHIN 48 HOURS.
 12. THE CONTRACTOR SHALL FULL DEPTH SAW CUT, REMOVE AND DISPOSE OF ALL NECESSARY SIDEWALKS TO PUBLIC STREET TO ACHIEVE THE CONSTRUCTION OF THE PROPOSED DRIVEWAY ENTRANCES.
 13. DEMOLITION NOTES AS SHOWN ON THE PLAN ARE NOT ALL INCLUSIVE. THE CONTRACTOR TO REMAIN IN PLACE FOR REMOVAL AND DISPOSAL OF ALL EXISTING SITE IMPROVEMENTS ABOVE AND BELOW GROUND TO COMPLY WITH THE GENERAL INTENT OF THIS DOCUMENT. CONTRACTOR SHALL COORDINATE ALL DEMOLITION ACTIVITIES WITH APPLICABLE CLIENT PERSONNEL AND APPLICABLE UTILITY PROVIDERS.

GENERAL LEGEND

ohs	OVERHEAD ELECTRIC
ohfo	OVERHEAD FIBER OPTIC
ufo	UNDERGROUND FIBER OPTIC
gas	GAS LINE (PLUS SIZE/TYP ON PLANS)
sw	SEWER LINE (PLUS SIZE/TYP ON PLANS)
st	STORM SEWER LINE (<17" (PLUS SIZE/TYP ON PLANS)
st	STORM SEWER LINE (>17" (PLUS SIZE/TYP ON PLANS)
ut	UNDERGROUND TELEPHONE LINE
w	WATER LINE (PLUS SIZE/TYP ON PLANS)
u	UTILITY PRESTRESS/STEEL-PLUS TYPE OF UTILITY
u	UTILITY POLE-PLUS TYPE OF UTILITY
u	UTILITY POLE W/SLUT
u	UTILITY POLE W/TRANSFORMER
u	UTILITY MANHOLE (PLUS TYPE/L'S)
u	UTILITY METER (PLUS TYPE)
u	UTILITY VAULT (PLUS TYPE)
u	GUY WIRE
u	LIGHT POLE (PLUS # BEAMS/ DIRECTION SHOWN GRAPHICALLY)
u	PIKE HYDRANT
u	CURB INLET
u	DECEADUOUS TREE (PLUS SIZE/TYP - COUPLES SHOWN TO SCALE ON PLANS)
u	MONITOR WELL
u	CENTER LINE
u	PROPERTY BOUNDARY LINE (OF NEI)
u	SECTION LINE
u	CURB LINE
u	EDGE OF BITUMINOUS
u	EXISTING CONCRETE PAVEMENT / SIDEWALK REMOVAL
u	EXISTING BITUMINOUS PAVEMENT / SIDEWALK REMOVAL
u	EXISTING BUILDINGS



PROJECT NO.: 224041

QuitTrip No. 0535

1501 EAST GRAND AVENUE
DES MOINES, IA

DESIGNER: JH
DRAWN BY: JH
REVISIONS:

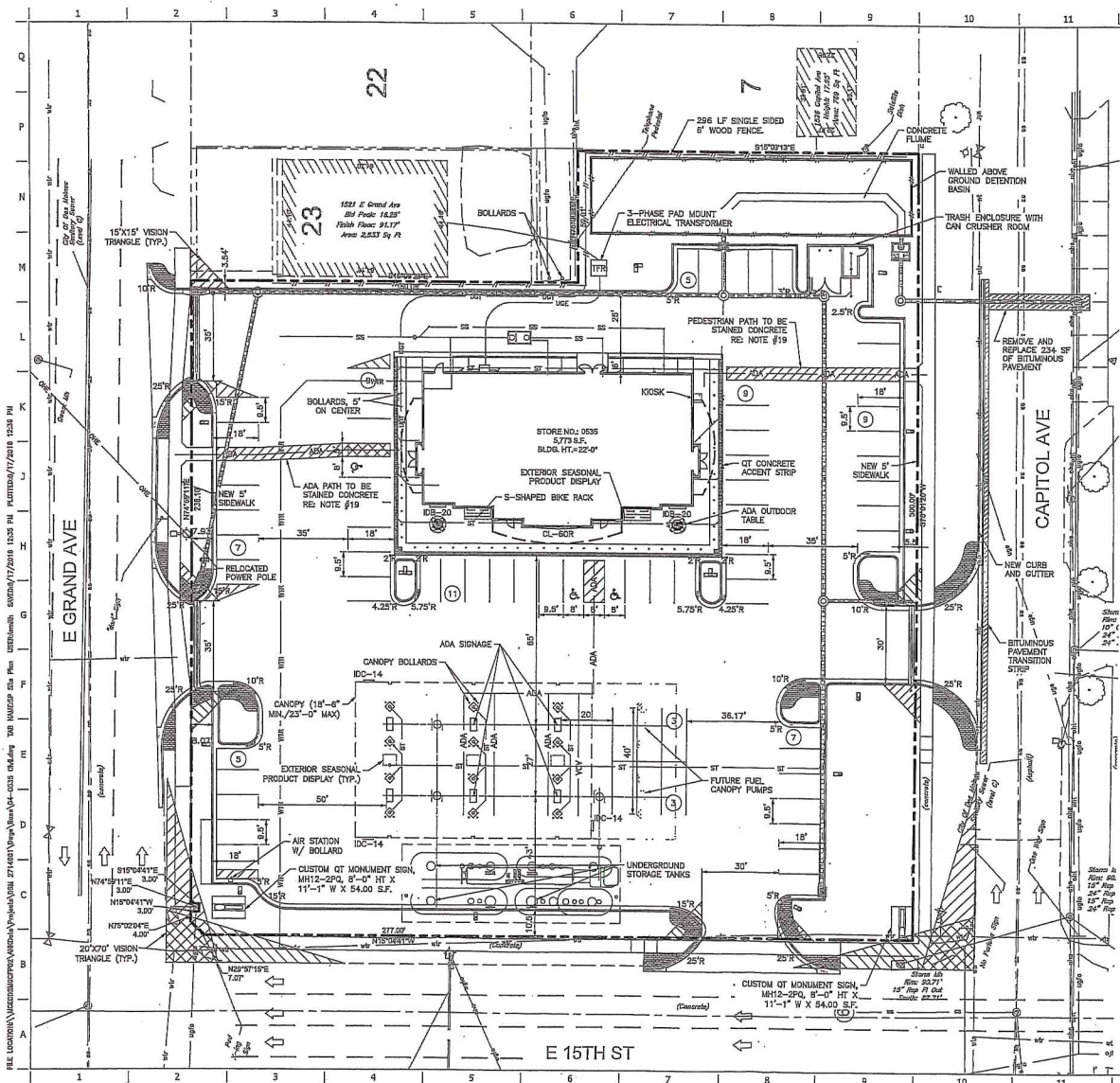
DATE: 05/20/18

DESCRIPTION:

ORIGINAL ISSUE DATE: 05-25-16

SHEET TITLE:
DEMOLITION PLAN

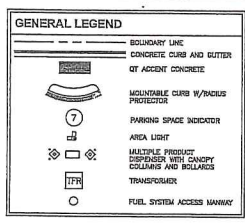
SHEET NUMBER:
2



- GENERAL NOTES**
1. ALL DIMENSIONS SHOWN ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.
 2. POLE LIGHTS ARE TO BE PLACED 6 FT CLEAR FROM FACE OF CURB TO FACE OF BASE UNLESS OTHERWISE NOTED.
 3. ALL IMPROVEMENTS WITHIN PUBLIC RIGHT-OF-WAY SHALL BE CONSTRUCTED IN ACCORDANCE WITH CORNERING AGENCY SPECIFICATIONS. THE CORNERING AGENCY IS RESPONSIBLE FOR PULLING THE SIDEWALK AND DRIVEWAY APPROVAL PERMIT AND ASSOCIATED FEES.
 4. REFER TO CONTRACT SPECIFICATIONS FOR STRIPING SPECIFICATIONS.
 5. THE MAXIMUM CROSS SLOPE ON ALL PROPOSED SIDEWALKS, INCLUDING PUBLIC AND PRIVATE SIDEWALK DRAINAGE CROSSINGS, IS 2%.
 6. REFER TO ELECTRICAL PLANS FOR SITE LIGHTING, SECURITY CAMERA, AND SIGNAGE WIRING DETAILS.
 7. DINING TABLE INSTALLATION SHALL CONFORM TO MANUFACTURER SPECIFICATIONS.
 8. REFER TO STRUCTURAL PLANS FOR ALL BOLLARD INSTALLATION DETAILS.
 9. UNDERGROUND STORAGE TANKS ARE TO BE STAKED USING THE FUEL PLANS (BY CONTRACTOR), INCLUDING THE TANK DETENTION LIMITS.
 10. THE CONTRACTOR SHALL NOTIFY ALL APPLICABLE CITY OF DES MOINES DEPARTMENTS FOR STORAGE TANK AND FUEL LINE INSPECTIONS.
 11. THE SITE SHALL BE MAINTAINED IN COMPLIANCE WITH ALL CITY CODE APPLICABLE ON THE DATE OF SITE PLAN APPROVAL.
 12. ANY NEW ROOFTOP MECHANICAL EQUIPMENT MUST BE SCREENED FROM STREET LEVEL VIEW.
 13. TRANSFORMERS, JUNCTION BOXES, AIR CONDITIONERS OVER 3 FEET IN HEIGHT, OR OTHER SUCH ITEMS MAY NOT BE LOCATED IN THE REQUIRED SEARCH AREA.
 14. ANY AMENDMENTS OR CHANGES TO THE PROPOSED SITE THAT DO NOT MEET WHAT IS SHOWN ON THE SITE PLAN NEED TO BE APPROVED WITH THE PERMIT AND DEVELOPMENT CENTER PRIOR TO INSTALLATION/CONSTRUCTION.
 15. LIGHTING MUST BE LOW GLARE CUT-OFF TYPE FIXTURES TO REDUCE THE CLARE OF LIGHT POLLUTION ON SURROUNDING PROPERTIES.
 16. THE REQUIRED LANDSCAPING, BOTH EXISTING AND PROPOSED, SHALL BE MAINTAINED FOR THE LIFE OF THE CENTRIFUGE OF OCCUPANCY.
 17. ALL DISTURBED AREAS SHOULD BE RESTORED BY SEEDING OR SOODOING.
 18. THE DUMPSTER ENCLOSURE SHALL BE CONSTRUCTED IN SUBSTANTIAL CONFORMANCE WITH THIS DIAGRAM AND THIS ENDORSEMENT PLAN DRAWING SHALL SUPERSEDE FOR CONSTRUCTION PURPOSES. ANY AND ALL DUMPSTER ENCLOSURE DIAGRAMS PROPOSED. THE ENCLOSURE SHALL BE BUILT TO MATCH THE BUILDING.
 19. BRAND AND COLOR OF STAINED CONCRETE USED FOR PEDESTRIAN PATH TO BE SCORFOLD REBVE STAIN (WESTWOOD BROWN).

PAVEMENT THICKNESS

PUBLIC SIDEWALK	4" PCC
PAVING W/IN RIGHT-OF-WAY	7" PCC
ON-SITE PARKING AND DRIVE AREAS	8" PCC



AREA SUMMARY

CROSS PROPERTY AREA	77,117 S.F.
ADDRESS	1501 EAST GRAND AVENUE

PROJECT NO. 27-4621

QuikTrip No. 0535
1501 EAST GRAND AVENUE
DES MOINES, IA

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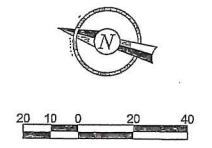
PROPOSED BY: 4-16 (05/16/15)
DESIGNED BY:
CHECKED BY:
DRAWN BY: JCA
REVIEWED BY:

DATE: 03-23-16

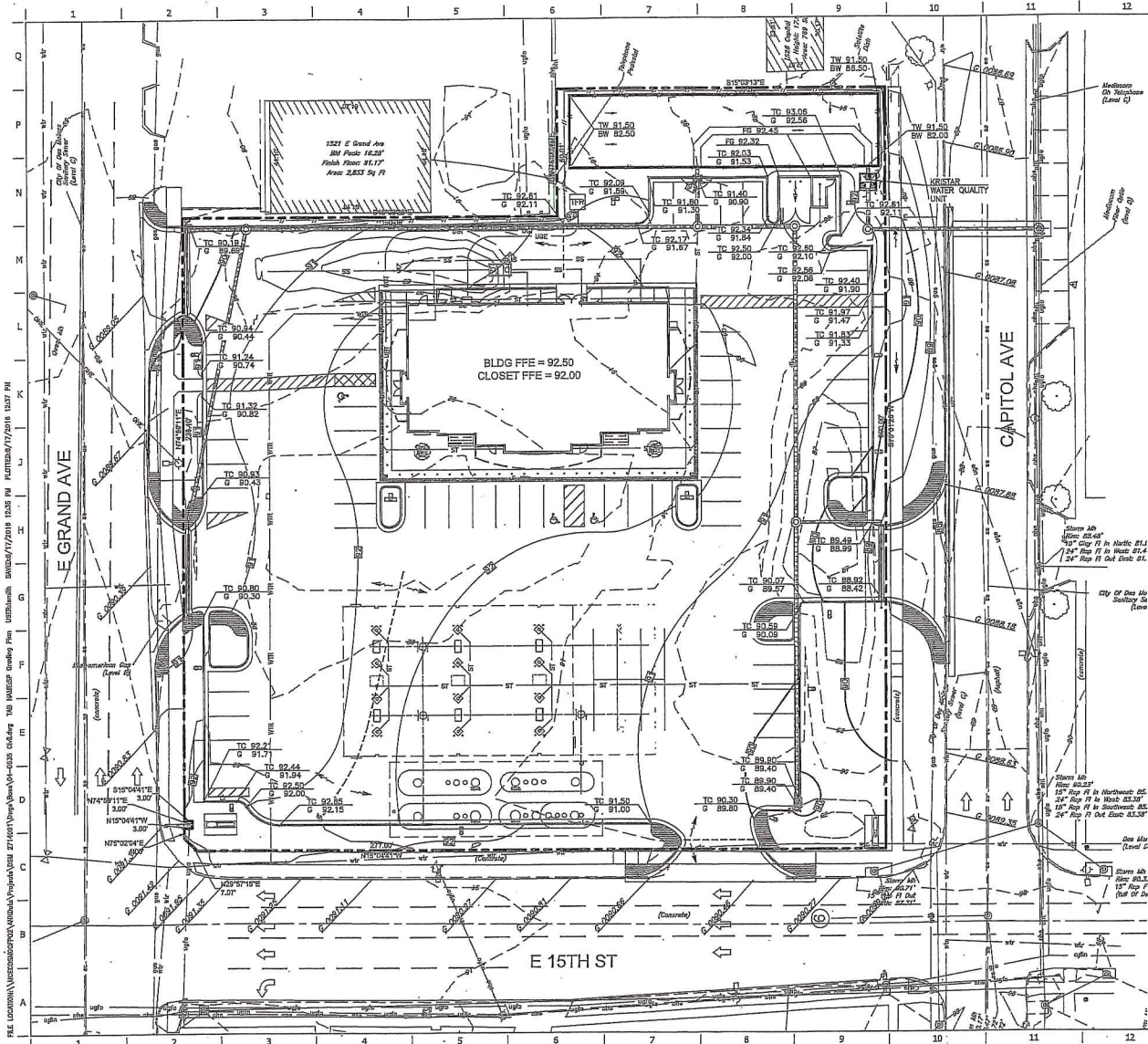
ORIGINAL ISSUE DATE: 03-23-16

SHEET TITLE:
SITE PLAN

SHEET NUMBER:
3



FILE LOCATION: \\sdc\projects\2016\0535\0535.dwg PLOT DATE: 03/23/16 10:38 AM PLOTTER: HP DesignJet T1200 PS PLOTTER: HP DesignJet T1200 PS



GENERAL NOTES

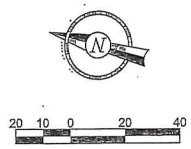
1. REFER TO GEOTECHNICAL REPORT AND PROJECT SPECIFICATIONS FOR GRADING AND SUBGRADE PREPARATION REQUIREMENTS.
2. REFER TO EROSION CONTROL PLANS FOR EROSION CONTROL THAT MUST BE IN PLACE PRIOR TO BEGINNING GRADING OPERATIONS.
3. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING PROPOSED GRADES SHOWN WILL ADEQUATELY MATCH EXISTING GRADES PRIOR TO CONSTRUCTING PAVEMENT. CONFLICTS IN GRADES MUST BE BROUGHT TO THE ATTENTION OF THE ENGINEER IMMEDIATELY.
4. DISTURBED AREAS NOT RECEIVING PAVEMENT SHALL HAVE 6 INCHES OF TOPSOIL, ACCORDING TO LOCAL SLOPE, REFER TO LANDSCAPE PLAN FOR SURFACE RESTORATION REQUIREMENTS.
5. TO AVOID ANY DEFICIT CREEPS OR SEEPS, "GRADE BREAKS" SHALL BE CONSIDERED AS A "TYPICAL" GRADE BREAK AND CONTRACTOR SHALL CONFER WITH QUALTRIP CONSTRUCTION MANAGER PRIOR TO CONSTRUCTION STAKING OR CONCRETE PLACEMENT RELATIVE TO ANY GRADE BREAKS.
6. THE MAXIMUM CROSS SLOPE ON ALL PROPOSED SIDEWALKS, INCLUDING SIDEWALK BROWNS, EXISTENCES, IS 5%.
7. WHEN PLAN GRADES DEPICT RUNOFF TO BE DIRECTED AWAY FROM THE CURB, USE DRAIN AWAY CURB ALTERNATIVE. IF QUESTIONS ARISE AS TO WHETHER OR NOT RUNOFF SHOULD BE DIRECTED AWAY FROM THE CURB, CONSULT WITH THE QUALTRIP CONSTRUCTION MANAGER.
8. GRADE ADJUSTMENTS MAY BE REQUIRED WITHIN PUBLIC RIGHT-OF-WAY TO MATCH EXISTING GRADES. REFER TO ADA COMPLIANCE REFERENCE PLAN FOR GRADING REQUIREMENTS.
9. REFER TO CANOPY PLANS FOR CANOPY COLUMN FOOTING CONSTRUCTION DETAILS.
10. CONTRACTOR TO INSTALL WATERPROOFING WHERE GRADES ADJACENT TO BUILDING ARE ABOVE FINISHED FLOOR ELEVATIONS.
11. CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT ANY REMOVED OR RAZED ITEM OR FACILITY SHALL BE, BUT NOT LIMITED TO, RAZED, FENCES, POSTINGS, FOUNDATIONS, AND BASEMENTS HAVE BEEN REPLACED WITH APPROPRIATE MATERIAL AND PROPERLY INSTALLED AND COMPANED IN ACCORDANCE WITH THE GEOTECHNICAL REPORT AND PROJECT SPECIFICATIONS. THE THIRD PARTY TESTING FIRM MUST CONFORM QUALIFICATION OPERATIONS HAVE COMPLETED AS STATED ABOVE.
12. ADJUSTMENTS TO PROPOSED ELEVATIONS (INCLUDING ADA RAMP) MAY BE REQUIRED TO MATCH EXISTING GRADES. IF NECESSARY, CONTRACTOR TO PROVIDE THE ENGINEER WITH ADDITIONAL ELEVATIONS FOR REVIEW. REFER TO ADA COMPLIANCE REFERENCE PLAN FOR GRADE LIMITATIONS.
13. CAPITOL AVENUE, EAST 15TH STREET AND EAST GRAND AVENUE DRIVEWAY ENTRANCE TO-N CURB GRADES SHALL MATCH EXISTING STREET GRADES AND CROSS SLOPES. IF ISSUES ARISE, CONSULT WITH QUALTRIP CONSTRUCTION MANAGER.
14. SEE QUALTRIP CONSTRUCTION MANAGER FOR ANY AND ALL POTHOLES RESULTS.
15. ALL EXISTING UTILITIES INCLUDING, BUT NOT LIMITED TO, MANHOLES, UTILITY VALVES, CLEANOUTS, AND INTAKES ARE TO BE ADJUSTED AND/OR REBUILT TO FINISH GRADE AS REQUIRED.
16. REFER TO PLAN SHEETS 5 & 6 FOR FINAL GRADES NEAR THE BUILDING AND DRIVE ENTRANCES.
17. REFER TO PLAN SHEET 7 FOR FLOORED AND FINAL GRADES AT STORM STRUCTURES.

GENERAL LEGEND

ST	STORM PIPE (< 10" NEW)
ST	STORM PIPE (> 12" NEW)
ST	STORM PIPE (> 12" EXISTING)
CD	BACK OF CURB DRAIN
CD	ADA ADA FINISH OF TRAVEL
XXXXXX	MAJOR CONTOUR (NEW)
XXXXXX	MINOR CONTOUR (NEW)
XXXXXX	MAJOR CONTOUR (EXISTING)
XXXXXX	MINOR CONTOUR (EXISTING)
TO XXXXX.XX	TOP OF CURB ELEVATION (NEW)
G XXXXX.XX	GUTTER ELEVATION (NEW)
FG XXXXX.XX	FINISHED GRADE ELEVATION (NEW)
ST	STORM GRATE (NEW)

SITE BENCHMARK

ELEVATIONS SHOWN ARE DERIVED FROM THE NORTH AMERICAN VERTICAL DATUM OF 1988 DATED 88.



PROJECT NO. 273481

QuikTrip No. 0535
4301 EAST GRAND AVENUE
DEERWOODS, GA

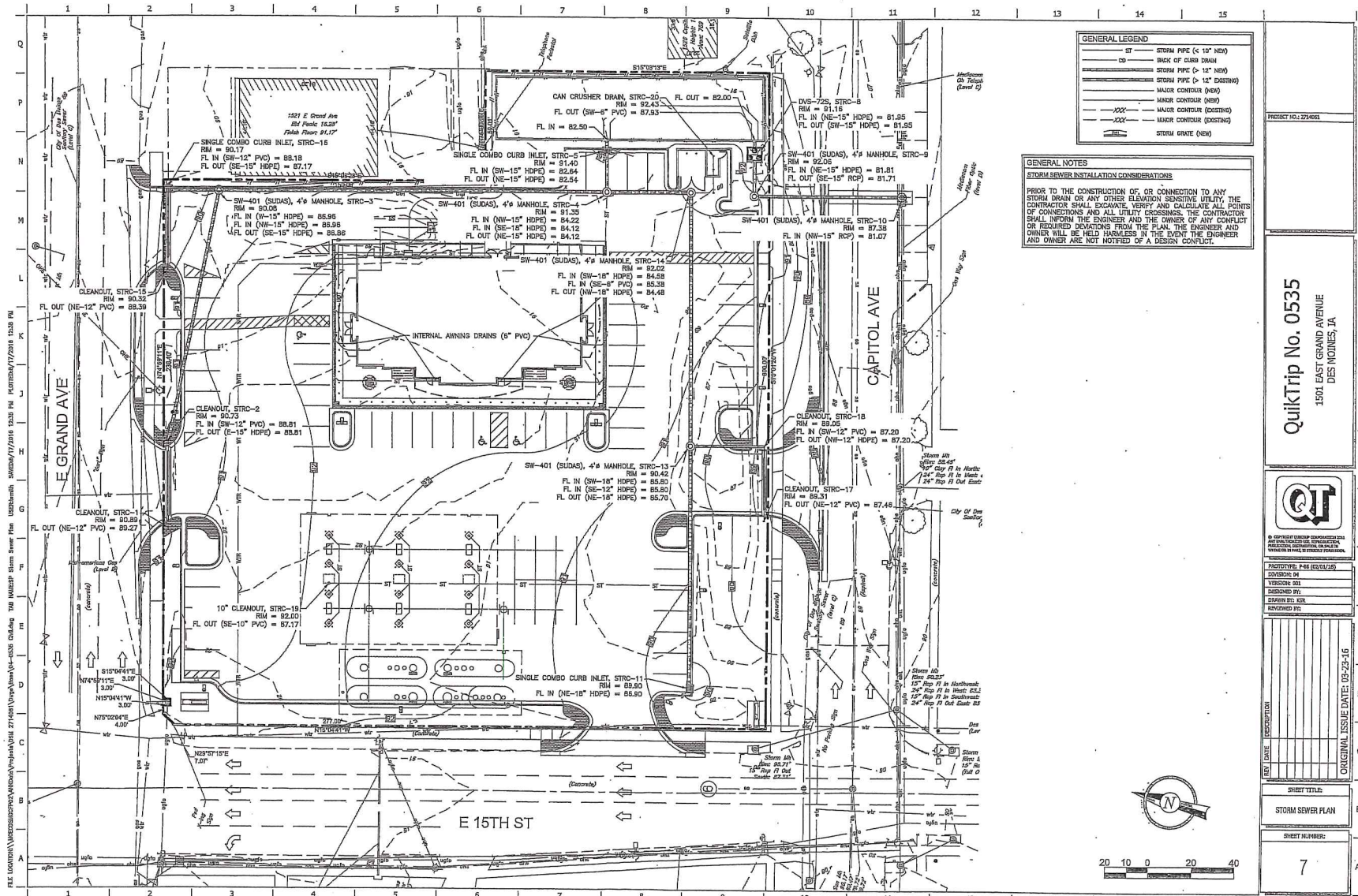


PHOTOGRAPH: PAGE 02/21/16
REVISION: 04
REVISION: 04
DRAWN BY: KSA
CHECKED BY:

DATE: 08/20/16
SHEET TITLE:
GRADING PLAN
SHEET NUMBER:
4

ORIGINAL ISSUE DATE: 03-22-16

FILE LOCATION: \\usdpc01\corporate\usdpc01\pva\pva\1521 E Grand Ave\1521 E Grand Ave.dwg 7/2/16 12:38 PM PLOTDATE: 7/2/16 12:37 PM

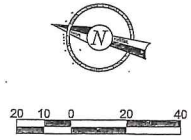


GENERAL LEGEND

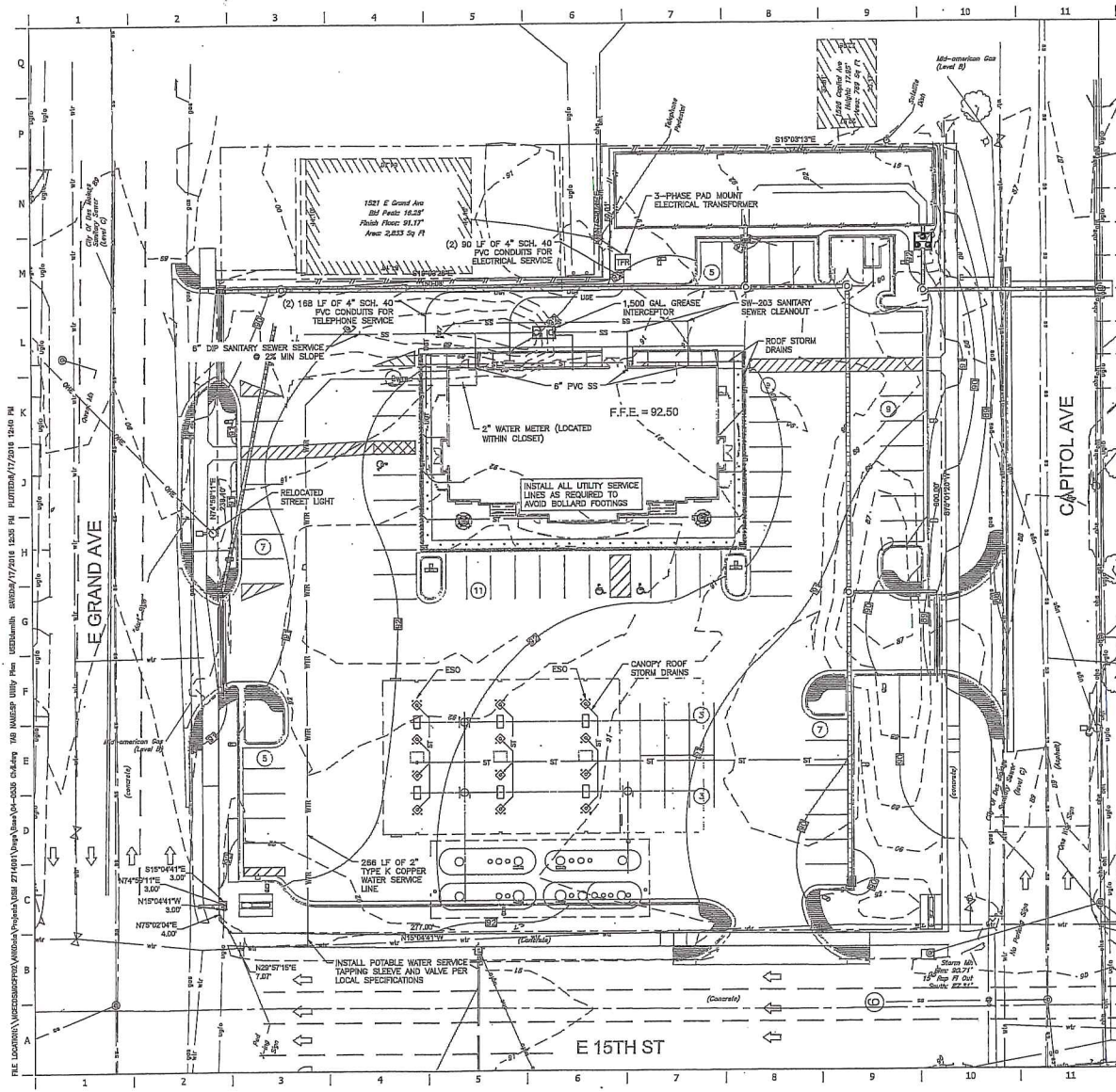
(Symbol)	STORM PIPE (< 12" NEW)
(Symbol)	BACK OF CURB DRAIN
(Symbol)	STORM PIPE (> 12" NEW)
(Symbol)	STORM PIPE (> 12" EXISTING)
(Symbol)	MAJOR CONTOUR (NEW)
(Symbol)	MAJOR CONTOUR (EXISTING)
(Symbol)	MINOR CONTOUR (NEW)
(Symbol)	MINOR CONTOUR (EXISTING)
(Symbol)	STORM GRATE (NEW)

GENERAL NOTES
STORM SEWER INSTALLATION CONSIDERATIONS
 PRIOR TO THE CONSTRUCTION OF, OR CONNECTION TO ANY STORM DRAIN OR ANY OTHER ELEVATION SENSITIVE UTILITY, THE CONTRACTOR SHALL EXCAVATE, VERIFY AND CALCULATE ALL POINTS OF CONNECTIONS AND ALL UTILITY CROSSINGS. THE CONTRACTOR SHALL INFORM THE ENGINEER AND THE OWNER OF ANY CONFLICT OR REQUIRED DEVIATIONS FROM THE PLAN. THE ENGINEER AND OWNER WILL BE HELD HARMLESS IN THE EVENT THE ENGINEER AND OWNER ARE NOT NOTIFIED OF A DESIGN CONFLICT.

PROJECT NO: 274606	
QuitTrip No. 0535	
1501 EAST GRAND AVENUE DES MOINES, IA	
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PROJECT: 274606-05	DATE: 03-23-16
DESIGNER: SA	DATE: 03-23-16
CHECKED BY: SA	DATE: 03-23-16
DRAWN BY: SA	DATE: 03-23-16
APPROVED BY: SA	DATE: 03-23-16
REV. DATE	DESCRIPTION
SHEET TITLE	
STORM SEWER PLAN	
SHEET NUMBER:	
7	



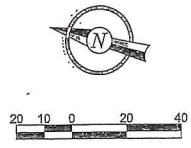
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 PLOT SHEET: 7
 PLOT TOTAL SHEETS: 10
 PLOT STATUS: OK
 PLOT MESSAGE: THE NUMBER OF SHEETS TO BE PLOTTED IS 10. THE NUMBER OF SHEETS TO BE PLOTTED IS 10. THE NUMBER OF SHEETS TO BE PLOTTED IS 10.



- GENERAL NOTES**
1. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS ARE BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THIS INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR SHALL CONTACT ALL UTILITY COMPANIES AND SHALL LOCATE ALL UNDERGROUND UTILITY LINES PRIOR TO CONSTRUCTION.
 2. ALL UTILITY SERVICE LINES SHALL BE INSTALLED IN ACCORDANCE WITH RESPECTIVE UTILITY COMPANY/GOVERNING AGENCY SPECIFICATIONS.
 3. AS POSSIBLE, ALL PROPOSED UTILITIES SHALL BE ROUTED THROUGH PERVIOUS AREAS - SPECIFICALLY AVOIDING ON-SITE PAVED AREAS DESIGNATED FOR PARKING, ACCESS AND VEHICULAR TRAFFIC FLOW.
 4. THE CONTRACTOR IS TO MAINTAIN PROPER CLEARANCE BETWEEN PROPOSED UTILITY SERVICES AND EXISTING UTILITIES DURING THE CONSTRUCTION OF THESE IMPROVEMENTS.
 5. PRIOR TO THE CONSTRUCTION OF OR CONNECTION TO ANY STREET DRAIN, SANITARY SEWER, OR ANY OTHER EXISTING EXISTING UTILITY, THE CONTRACTOR SHALL KNOW AND CALCULATE ALL POINTS OF CONNECTIONS AND ALL UTILITY DEPTHS. THE CONTRACTOR SHALL NOTIFY THE ENGINEER AND THE OWNER OF ANY CONFLICT OR REQUIRED DIMENSIONS FROM THE PLAN. THE CONTRACTOR AND OWNER WILL BE HELD HARMLESS IN THE EVENT THE ENGINEER AND OWNER ARE NOT NOTIFIED OF A DESIGN CONFLICT.
 6. THE CONTRACTOR SHALL COORDINATE ANY UTILITY SERVICE DISRUPTIONS, IF ANY, WITH ADVANTAGE PROPERTY OWNERS.
 7. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING ALL REQUIRED INSPECTIONS WITH THE UTILITY COMPANIES AND ENSURING THAT ALL FEES HAVE BEEN OBTAINED.
 8. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING ALL REQUIRED APPLICATION, PERMITS, ETC. WITH THE CITY OF DES MOINES DEPARTMENTS AND ENSURING THAT ALL APPLICATIONS AND PERMITS HAVE BEEN OBTAINED (E.G. POTABLE WATER, SANITARY SEWER, AND STORM SEWER SERVICES). ALL APPLICATION AND PERMIT FEES SHALL BE COMPLETED, SUBMITTED, AND PAID BY THE OWNER.
 9. THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING THAT ALL UTILITY BUILDING ENTRANCE LOCATIONS SHOWN UNDER THE ARCHITECTURAL PLANS, THE ENGINEER IS TO BE NOTIFIED IMMEDIATELY IF DISCREPANCIES EXIST.
 10. REFER TO ELECTRICAL PLANS FOR SITE LIGHTING, SIGNAGE, AND SECURITY CAMERA ELECTRICAL WIRING REQUIREMENTS.
 11. ELECTRICAL SERVICE CONDUIT SHALL BE 4" PVC (SCH. 40) CONDUIT WITH 42" MINIMUM COVER. SERVICE ARE TO HAVE 2" MIN. BUSH. CONDUIT IS TO BE INSTALLED BY CONTRACTOR IN ACCORDANCE WITH UTILITY COMPANY SPECIFICATIONS. THE CONDUIT END IS TO BE CLEARLY MARKED AT THE PROPERTY LINE BY SIGNAGE. THE CONTRACTOR SHALL CONTACT THE UTILITY COMPANY TO VERIFY CONDUIT SIZE AND ALIGNMENT PRIOR TO INSTALLATION.
 12. TELEPHONE SERVICE IS TO BE ENGINEER IN 4" SCHEDULE 40 PVC PIPE WITH 3" MIN. BUSH AT TURN. CONDUIT IS TO BE INSTALLED BY CONTRACTOR FROM THE BUILDING TO THE PROPERTY LINE IN ACCORDANCE WITH THE CITY OF DES MOINES STANDARD SPECIFICATIONS AND ARE SUBJECT TO FIELD VERIFICATION AND APPROVAL BY THE WATER SERVICE INSPECTOR. RADIOLOW PRESENCE ARE TO BE LOCATED INSIDE THE BUILDING. REFER TO ARCHITECTURAL PLANS. ALL WATER SERVICE LINES 2" OR LARGER SHALL BE CO-COVERED WITH 12" MINIMUM COVER. ANY FEES ASSUMED BY THE UTILITY COMPANY WILL BE PAID DIRECTLY BY THE OWNER. PLUMBING CONTRACTOR SHALL MARK THE LOCATION OF THE SERVICE WATER LINE FROM THE BUILDING WITH SIGNAGE.
 13. ALL WATER SERVICE INSTALLATIONS, INCLUDING RADIOLOW DEVICES, ARE TO BE INSTALLED IN ACCORDANCE WITH THE CITY OF DES MOINES STANDARD SPECIFICATIONS AND ARE SUBJECT TO FIELD VERIFICATION AND APPROVAL BY THE WATER SERVICE INSPECTOR. RADIOLOW PRESENCE ARE TO BE LOCATED INSIDE THE BUILDING. REFER TO ARCHITECTURAL PLANS. ALL WATER SERVICE LINES 2" OR LARGER SHALL BE CO-COVERED WITH 12" MINIMUM COVER. ANY FEES ASSUMED BY THE UTILITY COMPANY WILL BE PAID DIRECTLY BY THE OWNER. PLUMBING CONTRACTOR SHALL MARK THE LOCATION OF THE SERVICE WATER LINE FROM THE BUILDING WITH SIGNAGE.
 14. SANITARY SEWER SERVICE LINE SHALL BE DIP WITH 48" MINIMUM COVER. SANITARY SEWER SERVICE IS TO BE CONSTRUCTED IN ACCORDANCE WITH GOVERNING AGENCY REGULATIONS. 4" SANITARY SEWER CLEANERS ARE TO BE INSTALLED AT ALL POINTS OF DEFLECTION ALONG THE SERVICE AND AS NOTED. THE PLUMBING CONTRACTOR WILL BE RESPONSIBLE FOR OBTAINING THE NECESSARY PERMITS.
 15. ALL EXISTING UTILITIES INCLUDING, BUT NOT LIMITED TO, MANHOLES, UTILITY VALVES, CLEANOUTS, AND NOISES ARE TO BE AVOIDED AND/OR RESULT TO FINISH SERVICE AS REQUIRED.
 16. THE CONTRACTOR IS RESPONSIBLE FOR ADJUSTING THE GREASE INTERCEPTOR ACCESS HANDBLES TO PROPOSED FINISH GRADE. SEE PLUMBING PLANS FOR SERVICE INSPECTOR DETAILS.
 17. THE CONTRACTOR SHALL COORDINATE ALL APPLICABLE CITY OF DES MOINES APPLICATIONS AND PERMITS FOR POTABLE WATER, SANITARY SEWER, AND STORM SEWER SERVICES. ALL APPLICATION AND PERMIT FEES SHALL BE COMPLETED, SUBMITTED, AND PAID BY OWNER.
 18. ALL PUBLIC SANITARY SEWER CONSTRUCTION SHALL BE INSTALLED UNDER A SEPARATE CONTRACT.

GENERAL LEGEND

— WTL	WATER LINE
— SS	SANITARY SEWER LINE
— USE	UNDERGROUND ELECTRIC LINE
— UTL	UNDERGROUND TELEPHONE LINE
— ST	STORM PIPE (< 10')
— ST	STORM PIPE (> 12')
— MC	MAJOR CONTOUR
— CD	CONCRETE CURB AND GUTTER



PROJECT NO: 074984

QuikTrip No. 0535
1501 EAST GRAND AVENUE
DES MOINES, IA



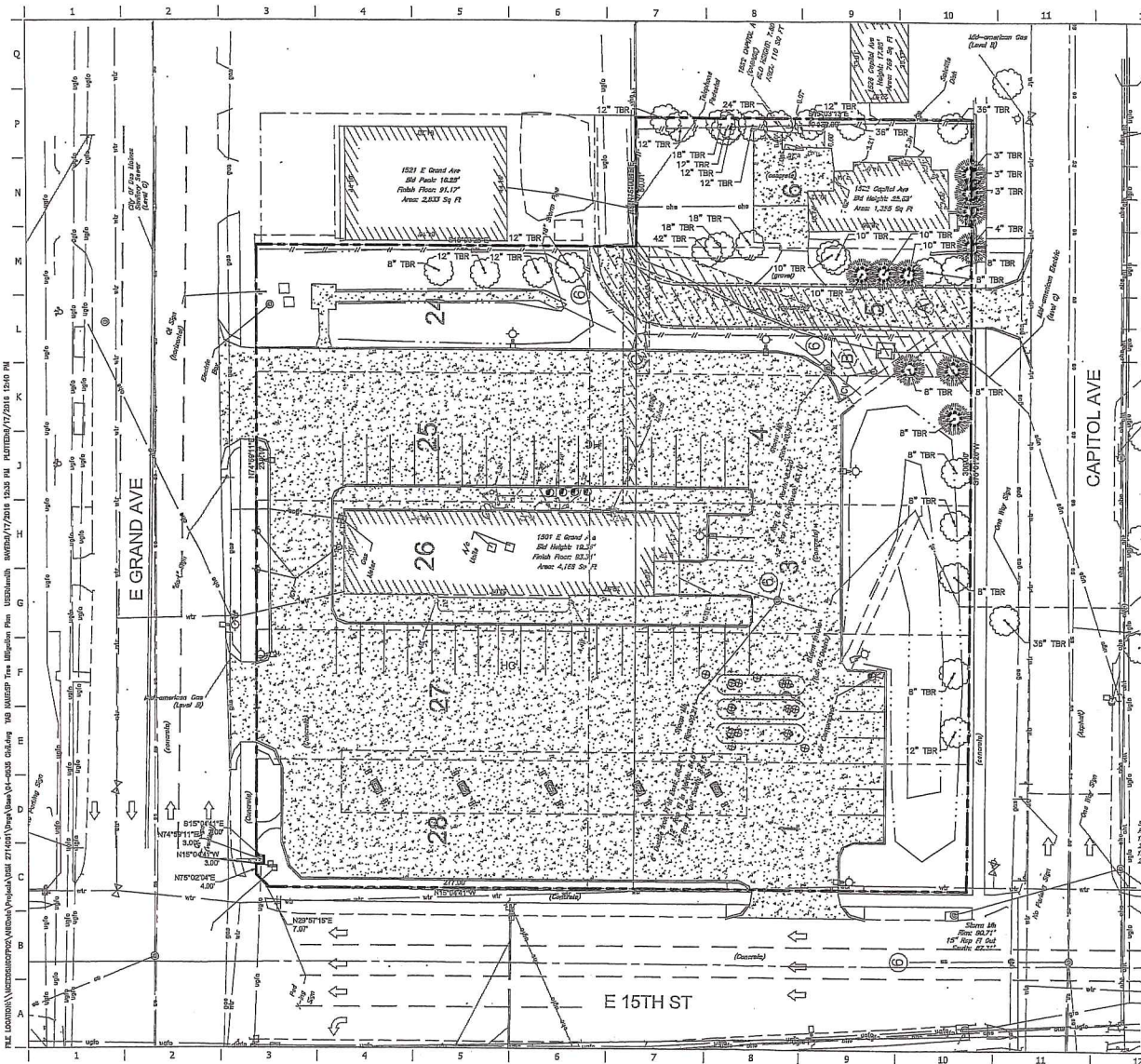
DESIGNED BY: [Signature]
CHECKED BY: [Signature]
DATE: 08-23-16

DATE	DESCRIPTION

SHEET TITLE:
UTILITY PLAN

SHEET NUMBER:
10

FILE LOCATION: \\c:\projects\0535\0535.dwg; 150 GRAND ST UTILITY PLAN; 08/23/16 12:30 PM; 150 GRAND ST UTILITY PLAN; 08/23/16 12:30 PM; 150 GRAND ST UTILITY PLAN; 08/23/16 12:30 PM

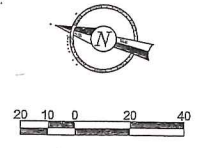


- GENERAL NOTES**
1. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOOKING ABOVE DIMENSIONS OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND, WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANY TO REVERIFY EXACT FIELD LOCATION OF UTILITIES.
 2. LOCATE ALL UTILITIES BEFORE ANY PLANTING BEGINS.
 3. THE MOST RECENT EDITION OF THE DESIGN SPECIFICATIONS (DESIGN MANUAL) AND ALL CITY ORDINANCES, IF APPLICABLE, SHALL APPLY TO ALL WORK ON THIS PROJECT.
 4. THE SIZE AND QUALITY OF PLANT MATERIAL SHALL CONFORM TO THE MOST CURRENT EDITION OF THE AMERICAN STANDARDS FOR NURSERY STOCK AND SOILS.
 5. ALL PLANT MATERIAL SHALL BE HEALTHY SPECIMENS WITHOUT DEFOLIATION, WOUND OR OPEN SPACES, WITH VIBES, BRANCHES AND ROOT SYSTEMS, TO BE HEALTHY, SHOWN AND CHARACTER OF GROWTH OF THE SPECIES OR VARIETY.
 6. CONTRACTOR TO REPAIR ALL DISTURBED AREAS OF THE SITE NOT SET ASIDE AS PLANTING BEDS OR AREAS TO BE SOILED.
 7. BACKFILL TO TOP OF CURB (MINIMUM 1.5' FOR SOIL, IF REQUIRED).
 8. WEED PREVENTION (PRE-EMERGENT) SHALL BE SPREAD OVER SOIL AFTER PLANTING AND BEFORE MULCHING IN ALL PLANTING BEDS PER MANUFACTURER'S RECOMMENDATION.
 9. 2" WOUND RIVER ROCK AND WEED BARRIERS SHALL BE LAYED IN ALL PLANTING BEDS. ALLOWING PLANTED TREES SHALL HAVE A 6" TOTAL DIAMETER (DAP) RADIUS OF TRUNKS SURROUNDED HARDWOOD MULCH RANGING APPROXIMATELY 2" TO 4" IN THICKNESS. MULCH SHALL BE 4" DEEP. MULCH SHALL BE 2" FROM TRUNK AND ROOT FLARE. MULCH PRODUCTS ARE NOT PERMITTED. THE CONTRACTOR SHALL PROVIDE A SAMPLE OF THE SUGGESTED MULCH, 2" THICK, AND ASSESS TO THE LANDSCAPE ARCHITECT AND OWNER PRIOR TO CONSTRUCTION.
 10. ALL TREES SHALL BE CONCRETE BULLET TYPE EDGING GENERALLY 4" WIDE & 2" HIGH & 2" TALL. THE CONTRACTOR SHALL SUBMIT A SAMPLE FOR REVIEW TO THE LANDSCAPE ARCHITECT AND OWNER PRIOR TO CONSTRUCTION.
 11. PLANT QUANTITIES ARE SHOWN FOR INFORMATION ONLY. THE DRAWING SHALL PREVAIL IN ANY CONFLICT AREAS.
 12. ALL TREES SPREAD IN THE PUBLIC RIGHT-OF-WAY SHALL BE FENCED BY THE CONTRACTOR AT THE END OF EACH WORK DAY.
 13. CONTRACTOR SHALL WARRANTY ALL PLANT MATERIALS FOR A PERIOD OF ONE YEAR FROM DATE OF INSTALLATION.
 14. CONTRACTOR SHALL PROVIDE IRRIGATION DESIGN TO OWNER, IF REQUESTED, FOR APPROVAL.
 15. CONTRACTOR SHALL PROVIDE DIVISION PRICES TO PLANTING IN THE PUBLIC RIGHT-OF-WAY AT \$150 PER HOUR.
 16. ALL WIRE TRENCH AND BURIAL SHALL BE REMOVED FROM THE FOOT PAWS OF STREET TREES PRIOR TO PLANTING.
 17. NO STAKING OF TREES IS ALLOWED.
 18. NO PLANTING WILL BE INSTALLED UNTIL ALL GRADING AND CONSTRUCTION HAS BEEN COMPLETED IN THE IMMEDIATE AREA.
 19. MAINTAIN ALL CUT AND FILL AREAS TO ACCOMMODATE SURFACE DRAINAGE AWAY FROM ALL STRUCTURES.
 20. THE CONTRACTOR SHALL BE RESPONSIBLE TO TAKE WHATEVER MEASURES NECESSARY TO ESTABLISH PERMANENT SOIL STABILIZATION.
 21. HOLES FOR TREES, EVAPORATORS, AND GUNNS SHALL BE 2" WIDER THAN ROOT BALL.
 22. PLANT MATERIALS SHALL BE OF APPROPRIATE SIZE AND TYPE AS SHOWN IN THE PLANTING SCHEDULE. ANY SUBSTITUTIONS TO THE PLANT SCHEDULE SHALL BE SUBMITTED IN WRITING FOR APPROVAL BY THE LANDSCAPE ARCHITECT.
 23. OVERMATURE TREES SHALL HAVE STABILIZED LEAVES AND NO "V" CRACKS. UNDERMATURE TREES SHALL HAVE NO BRANCHED LEAVES AND NO "V" CRACKS.
 24. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REMOVAL OF IDENTIFICATION TAGS, EXPANSION, AND BURLAP CORDS ON ALL PLANT MATERIAL PRIOR TO COMPLETION OF THE CONTRACT.
 25. CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING ANY TREES THAT HAVE BEEN TIED WITHIN ONE YEAR FROM DATE OF INSTALLATION.
 26. CONTRACTOR SHALL BE RESPONSIBLE FOR WEEDING ALL PLANT BEDS FOR THREE MONTHS OF FIRST GROWING SEASON.
 27. ALL WEEDS AND DISTURBED AREAS SHALL BE INSPECTED AFTER EACH RAINFALL. ALL WEEDS, GRASS, AND BILLS SHALL BE IMMEDIATELY REPAIRED AND REDUCED.
 28. CONTRACTOR SHALL REPAIR ALL DAMAGE TO PROPERTY AND EXISTING UTILITIES FROM PLANTING OPERATIONS AT NO COST TO THE OWNER.

MITIGATION SUMMARY

CLASSIFICATION	REPLACEMENT RATIO	QUANTITY REMOVED	QUANTITY REQUIRED
12"-18" CALIPER	1:1	10	10
18"-24" CALIPER	2:1	3	6
24"-30" CALIPER	3:1	1	3
30"-36" CALIPER	4:1	0	0
36"-42" CALIPER	5:1	0	0
42"-48" CALIPER	6:1	1	6
TOTAL		14	46

GENERAL LEGEND
 TBR TO BE REMOVED AND DISPOSED OF



PROJECT NO. 274403

QuikTrip No. 0535
 1501 EAST GRAND AVENUE
 DES MOINES, IA

QT

DESIGNED BY: [blank]
 CHECKED BY: [blank]
 DRAWN BY: [blank]
 REVISIONS: [blank]

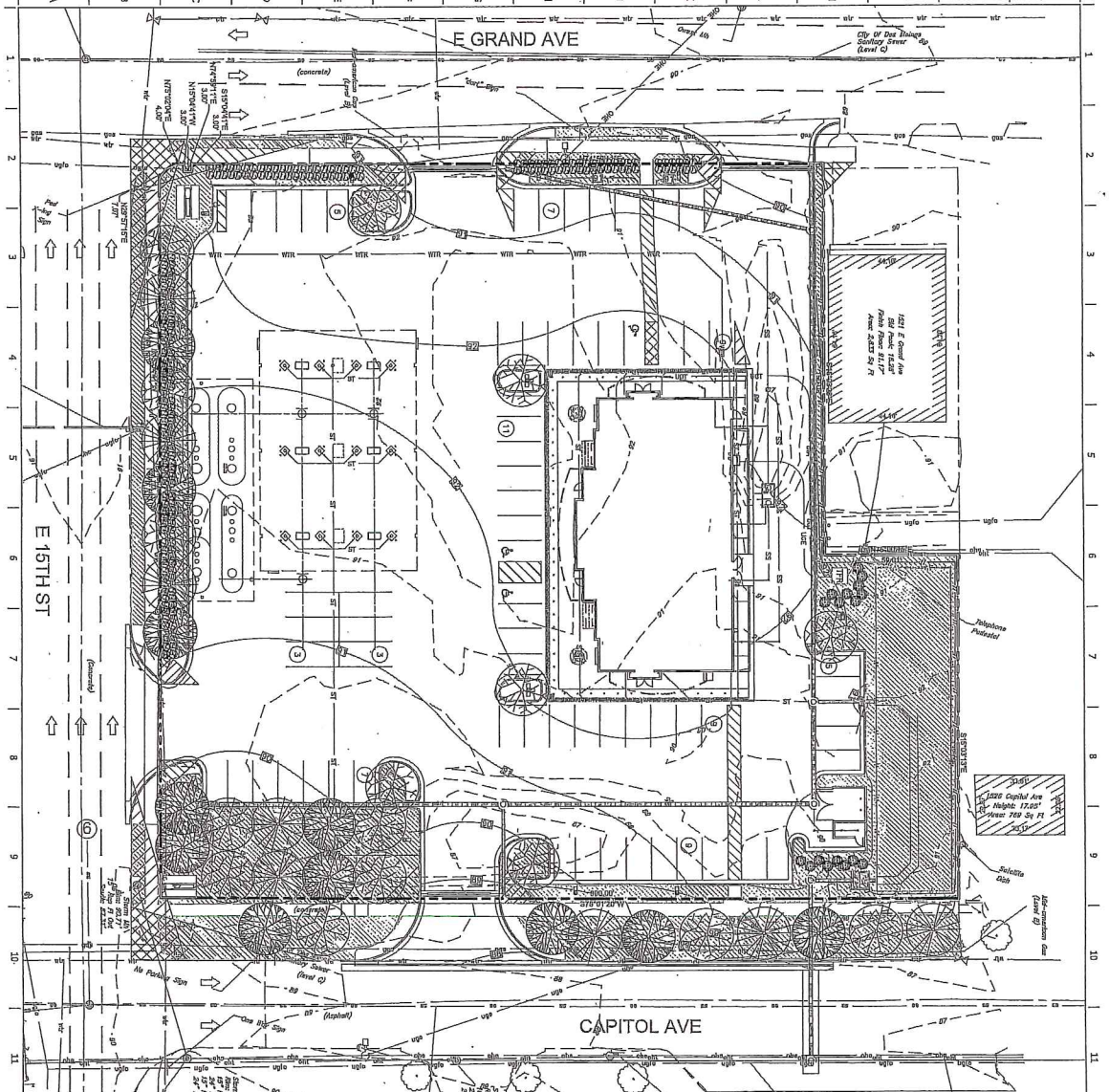
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 EXPIRES: 04
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 DESIGNED BY: [blank]
 DRAWN BY: [blank]
 REVISIONS: [blank]

DATE: [blank] DESCRIPTION: [blank]

SHEET TITLE:
 TREE MITIGATION PLAN


SHEET NUMBER:
 11

ORIGINAL ISSUE DATE: 03-23-16



ITEM	LANDSCAPE	QUANTITIES	COMMENTS
1	GRASS	1000 SQ. FT.	SEE LANDSCAPE PLAN AND ASSOCIATED NOTES FOR SPECIFICATIONS AND INSTALLATION.
2	SPRING WATER	0.00	SEE LANDSCAPE PLAN AND ASSOCIATED NOTES FOR SPECIFICATIONS AND INSTALLATION.
3	GREEN PLANTING	194 PL.	SEE LANDSCAPE PLAN AND ASSOCIATED NOTES FOR SPECIFICATIONS AND INSTALLATION.
4	WHITE SWAMP OAK	18 PL.	SEE LANDSCAPE PLAN AND ASSOCIATED NOTES FOR SPECIFICATIONS AND INSTALLATION.
5	RED BARK OAK	11 PL.	SEE LANDSCAPE PLAN AND ASSOCIATED NOTES FOR SPECIFICATIONS AND INSTALLATION.
6	RED BARK OAK	11 PL.	SEE LANDSCAPE PLAN AND ASSOCIATED NOTES FOR SPECIFICATIONS AND INSTALLATION.
7	RED BARK OAK	11 PL.	SEE LANDSCAPE PLAN AND ASSOCIATED NOTES FOR SPECIFICATIONS AND INSTALLATION.
8	RED BARK OAK	11 PL.	SEE LANDSCAPE PLAN AND ASSOCIATED NOTES FOR SPECIFICATIONS AND INSTALLATION.
9	RED BARK OAK	11 PL.	SEE LANDSCAPE PLAN AND ASSOCIATED NOTES FOR SPECIFICATIONS AND INSTALLATION.
10	RED BARK OAK	11 PL.	SEE LANDSCAPE PLAN AND ASSOCIATED NOTES FOR SPECIFICATIONS AND INSTALLATION.
11	RED BARK OAK	11 PL.	SEE LANDSCAPE PLAN AND ASSOCIATED NOTES FOR SPECIFICATIONS AND INSTALLATION.
12	RED BARK OAK	11 PL.	SEE LANDSCAPE PLAN AND ASSOCIATED NOTES FOR SPECIFICATIONS AND INSTALLATION.
13	RED BARK OAK	11 PL.	SEE LANDSCAPE PLAN AND ASSOCIATED NOTES FOR SPECIFICATIONS AND INSTALLATION.
14	RED BARK OAK	11 PL.	SEE LANDSCAPE PLAN AND ASSOCIATED NOTES FOR SPECIFICATIONS AND INSTALLATION.
15	RED BARK OAK	11 PL.	SEE LANDSCAPE PLAN AND ASSOCIATED NOTES FOR SPECIFICATIONS AND INSTALLATION.

ITEM	LANDSCAPE	QUANTITIES	COMMENTS
16	RED BARK OAK	11 PL.	SEE LANDSCAPE PLAN AND ASSOCIATED NOTES FOR SPECIFICATIONS AND INSTALLATION.
17	RED BARK OAK	11 PL.	SEE LANDSCAPE PLAN AND ASSOCIATED NOTES FOR SPECIFICATIONS AND INSTALLATION.
18	RED BARK OAK	11 PL.	SEE LANDSCAPE PLAN AND ASSOCIATED NOTES FOR SPECIFICATIONS AND INSTALLATION.
19	RED BARK OAK	11 PL.	SEE LANDSCAPE PLAN AND ASSOCIATED NOTES FOR SPECIFICATIONS AND INSTALLATION.
20	RED BARK OAK	11 PL.	SEE LANDSCAPE PLAN AND ASSOCIATED NOTES FOR SPECIFICATIONS AND INSTALLATION.
21	RED BARK OAK	11 PL.	SEE LANDSCAPE PLAN AND ASSOCIATED NOTES FOR SPECIFICATIONS AND INSTALLATION.
22	RED BARK OAK	11 PL.	SEE LANDSCAPE PLAN AND ASSOCIATED NOTES FOR SPECIFICATIONS AND INSTALLATION.
23	RED BARK OAK	11 PL.	SEE LANDSCAPE PLAN AND ASSOCIATED NOTES FOR SPECIFICATIONS AND INSTALLATION.
24	RED BARK OAK	11 PL.	SEE LANDSCAPE PLAN AND ASSOCIATED NOTES FOR SPECIFICATIONS AND INSTALLATION.
25	RED BARK OAK	11 PL.	SEE LANDSCAPE PLAN AND ASSOCIATED NOTES FOR SPECIFICATIONS AND INSTALLATION.

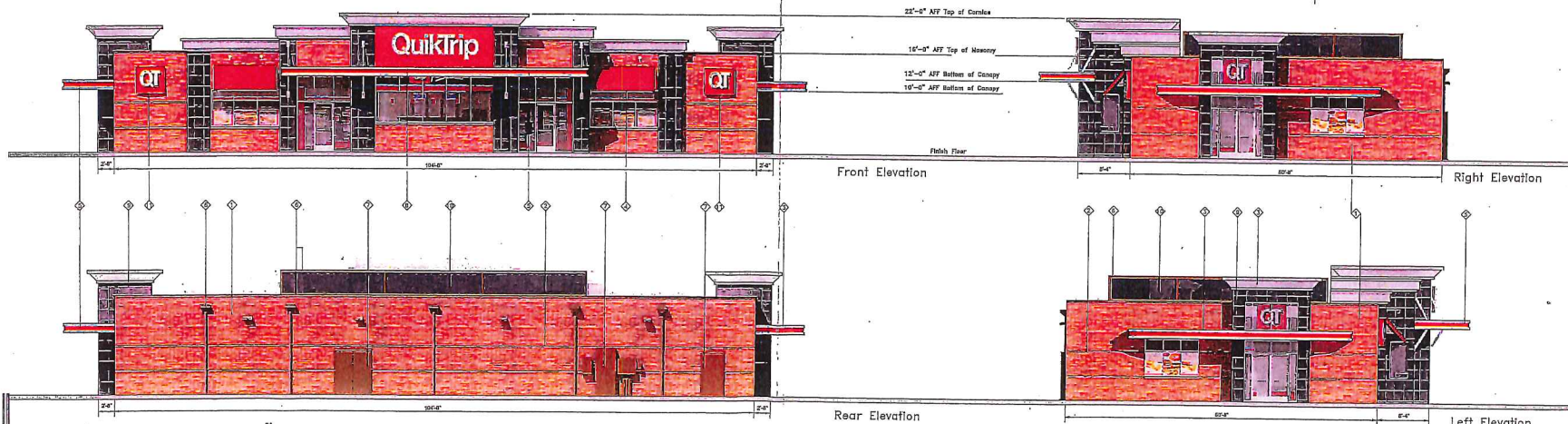


QuikTrip No. 0535
1501 EAST GRAND AVENUE
DES MOINES, IA

12

LANDSCAPE PLAN

12



QuikTrip
 4705 South 129th East Ave.
 Tulsa, OK 74134-7028
 P.O. Box 5475
 Tulsa, OK 74101-5475
 (918) 475-7700

Store # 0535 Flat Entry Building Elevations

Address: 1501 E Grand Ave

City, State: Des Moines, IA

Serial # 04-0535-BFT1

Scale: 1/8" = 1'-0"

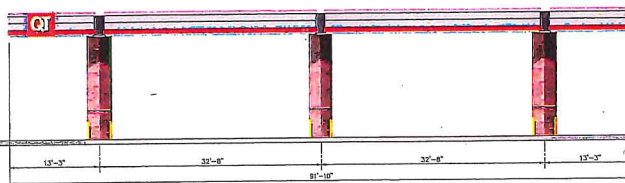
Issue Date: 05.10.16

Drawn By: JK

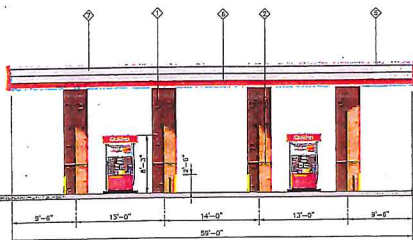
Rev/Notes:

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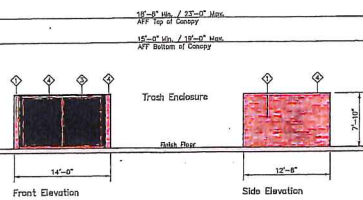
Q	FRESH	MANUFACTURER	SPECIFICATION
1	BRICKWORK	INTERSTATE BRICK	TYPE S BRICKWORK
2	GLASS	PIPER-HAWK	PIPER-HAWK GLASS
3	GLASS	PIPER-HAWK	PIPER-HAWK GLASS
4	GLASS	PIPER-HAWK	PIPER-HAWK GLASS
5	GLASS	PIPER-HAWK	PIPER-HAWK GLASS
6	GLASS	PIPER-HAWK	PIPER-HAWK GLASS
7	GLASS	PIPER-HAWK	PIPER-HAWK GLASS
8	GLASS	PIPER-HAWK	PIPER-HAWK GLASS
9	GLASS	PIPER-HAWK	PIPER-HAWK GLASS
10	GLASS	PIPER-HAWK	PIPER-HAWK GLASS
11	GLASS	PIPER-HAWK	PIPER-HAWK GLASS



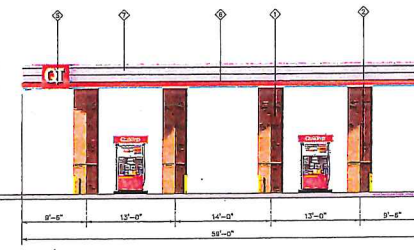
Front Elevation



Left Elevation



Side Elevation



Right Elevation



Store # 0535 Double Stack 6 Canopy Elevations
 Serial # 04-0535-GD06
 Scale: 1/8" = 1'-0"
 Issue Date: 05.10.16

Address: 1501 E Grand Ave
 Drawn By: JK
 Rev/Notes:

City, State: Des Moines, IA

Q	FINISH	MANUFACTURER	SPECIFICATION
1	IMPERIAL STONE	IMPERIAL STONE	AS PER SPECIFICATION SHEET
2	IMPERIAL BRICK	IMPERIAL BRICK	AS PER SPECIFICATION SHEET
3	BRICK	IMPERIAL BRICK	AS PER SPECIFICATION SHEET
4	BRICK	IMPERIAL BRICK	AS PER SPECIFICATION SHEET
5	BRICK	IMPERIAL BRICK	AS PER SPECIFICATION SHEET
6	BRICK	IMPERIAL BRICK	AS PER SPECIFICATION SHEET
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10	BRICK	IMPERIAL BRICK	AS PER SPECIFICATION SHEET

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