



Date March 8, 2018

**RESOLUTION APPROVING "LINC" PUD DEVELOPMENT PLAN
FOR PROPERTY AT 210-310 SOUTHWEST 11TH STREET**

WHEREAS, on July 24, 2017, by Roll Call No. 17-1295, the City Council conditionally approved Hubbell Realty Company's "Gray's Station" PUD Conceptual Plan for real property located in the vicinity of 1300 Tuttle Street ("Property") to allow redevelopment of 83.73 acres with mixed use, low-medium density residential, high-density residential, and open space areas, which approval required the developer to submit a PUD Development Plan for review and approval by the Plan and Zoning Commission and the City Council for each phase of the project; and

WHEREAS, on August 14, 2017, by Roll Call No. 17-1408 the City Council approved Ordinance No. 15,600 rezoning the Property from "C3-B" Central Business Mixed Use District and "FW" Floodway District to "PUD" Planned Unit Development District; and

WHEREAS, at a public hearing held on February 15, 2018, the City Plan and Zoning Commission voted 7-0-1 in support of a motion to recommend **APPROVAL** of a request from Hubbell Development Services (developer), represented by Kris Saddoris (officer), for a "LINC" PUD Development Plan representing a portion of Phase 1 of the "Gray's Station" PUD Conceptual Plan, to allow development of that portion of the Property locally known as 210 Southwest 11th Street and 310 Southwest 11th Street with a 5-story, 84-unit multiple-family residential dwelling, a 5-story, 131-unit multiple family residential dwelling, and a 1-story clubhouse building, subject to the following conditions:

1. Compliance with all administrative comments of the Permit and Development Center.
2. Pedestrian crossings shall be identified on site by the use of paint markings and/or material variation to the satisfaction of the Planning Administrator.
3. All trash enclosures shall be constructed of masonry that is compatible with the primary buildings and shall have solid steel gates.
4. All trash enclosures shall include a pedestrian entrance and be constructed in a manner that shields direct views into the enclosure without the use of a gate to the satisfaction of the Planning Administrator.
5. All site lighting shall be directed downward and shielded from adjoining properties. Any pole mounted lighting along private walkways shall not exceed 15 feet in height and any pole mounted lighting in a parking area shall not exceed 20 feet in height.
6. Review and approval of all exterior building materials by the Planning Administrator.
7. All rooftop mechanical equipment shall be screened with material that is architecturally compatible with the building to the satisfaction of the Planning Administrator.
8. All utility meters, transformers, ground-mounted equipment, and other utilities shall be placed along side or rear facades of the building.
9. All signage shall be provided in accordance with the standards identified in the PUD Conceptual Plan.
10. The developer shall coordinate with DART on any necessary transit facilities to the satisfaction of the Planning Administrator.
11. Provision of bikeracks throughout the site to the satisfaction of the Planning Administrator.
12. Provision of tree mitigation plan information on the Development Plan to the satisfaction of the Planning Administrator.
13. Provision of street trees to the satisfaction of the City Arborist and Planning Administrator.

(continued)



Date March 8, 2018

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14. Review and approval of the finalize landscape plan, including the hardscape and softscape elements of the plaza by the Planning Administrator.
15. Existing elements of the Martin Luther King, Jr. Parkway streetscape shall be maintained unless determined non-essential by the Planning Administrator; and

WHEREAS, the portion of the Property included in the "LINC" PUD Development Plan is legally described as follows:

A PART OF OUTLOT 'Y', CENTRAL DES MOINES INDUSTRIAL PARK, AN OFFICIAL PLAT, AND A PART OF LOTS 81, 82 AND 85, FACTORY ADDITION, AN OFFICIAL PLAT, ALL BEING IN THE CITY OF DES MOINES, POLK COUNTY, IOWA AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE SOUTHEAST CORNER OF LOT 4, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE NORTH 0°31'45" WEST ALONG THE WESTERLY LINE OF SAID OUTLOT 'Y', A DISTANCE OF 63.43 FEET; THENCE NORTH 87°29'10" EAST CONTINUING ALONG SAID WESTERLY LINE, 10.37 FEET; THENCE NORTH 0°12'49" WEST CONTINUING ALONG SAID WESTERLY LINE, 554.99 FEET TO THE NORTHEAST CORNER OF LOT 5, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE SOUTH 89°04'27" EAST ALONG THE NORTH LINE OF SAID OUTLOT 'Y', 15.36 FEET TO THE NORTHEAST CORNER OF SAID OUTLOT 'Y'; THENCE NORTH 0°21'12" WEST ALONG THE WEST LINE OF SAID LOT 85, A DISTANCE OF 8.59 FEET TO THE SOUTHERLY RIGHT OF WAY LINE OF MARTIN LUTHER KING JR. PARKWAY; THENCE NORTH 86°10'40" EAST ALONG SAID SOUTHERLY RIGHT OF WAY LINE, 100.23 FEET; THENCE NORTH 84°40'04" EAST CONTINUING ALONG SAID SOUTHERLY RIGHT OF WAY LINE, 199.44 FEET TO THE WESTERLY RIGHT OF WAY LINE OF SW 11TH STREET; THENCE SOUTH 15°27'22" EAST ALONG SAID WESTERLY RIGHT OF WAY LINE, 173.21 FEET; THENCE SOUTH 74°42'29" WEST, 99.77 FEET; THENCE SOUTH 15°25'34" EAST, 75.29 FEET; THENCE NORTH 74°32'27" EAST, 99.81 FEET TO SAID WESTERLY RIGHT OF WAY LINE; THENCE SOUTH 15°27'22" EAST ALONG SAID WESTERLY RIGHT OF WAY LINE, 344.37 FEET TO A CORNER ON THE EASTERLY LINE OF SAID LOT 81; THENCE SOUTH 39°15'32" WEST ALONG SAID EASTERLY LINE, 16.08 FEET; THENCE SOUTH 81°39'24" WEST, 474.36' TO THE POINT OF BEGINNING AND CONTAINING 5.36 ACRES (233,696 SQUARE FEET).

NOW THEREFORE, BE IT RESOLVED, by the City Council of the City of Des Moines, Iowa, as follows:

1. The communication from the Plan and Zoning Commission is hereby received and filed.
2. The proposed "LINC" PUD Development Plan, as on file in the Community Development Department, is hereby found to be in conformance with PlanDSM: Creating Our Tomorrow Plan and is hereby approved, subject to revision of the Plan to add the conditions stated above.



Roll Call Number

Agenda Item Number

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Date March 8, 2018

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MOVED BY _____ TO ADOPT.

FORM APPROVED:

Glenna K. Frank, Assistant City Attorney

(10-2018-7.69)

COUNCIL ACTION	YEAS	NAYS	PASS	ABSENT
COWNIE				
BOESEN				
COLEMAN				
GATTO				
GRAY				
MANDELBAUM				
WESTERGAARD				
TOTAL				
MOTION CARRIED		APPROVED		

CERTIFICATE

I, DIANE RAUH, City Clerk of said City hereby certify that at a meeting of the City Council of said City of Des Moines, held on the above date, among other proceedings the above was adopted.

IN WITNESS WHEREOF, I have hereunto set my hand and affixed my seal the day and year first above written.

Mayor

City Clerk



February 26, 2018

Date March 8, 2018

Agenda Item 32

Roll Call # _____

Honorable Mayor and City Council
City of Des Moines, Iowa

Members:

Communication from the City Plan and Zoning Commission advising that at their February 15, 2018 meeting, the following action was taken regarding a request from Hubbell Development Services (developer) represented by Kris Saddoris (officer) for review and approval of a PUD Development Plan "LINC", to allow development of a 5-story, 84-unit multiple-family residential dwelling at 210 Southwest 11th Street; a 5-story, 131-unit multiple-family residential dwelling at 310 Southwest 11th Street; and a 1-story clubhouse building. The subject property is owned by HRC NFS I, LLC.

COMMISSION ACTION:

After public hearing, the members voted 7-0-1 (Greg Wattier abstained).

Commission Action:	Yes	Nays	Pass	Absent
Francis Boggus				X
Dory Briles	X			
Chris Cutler				X
David Courard-Hauri	X			
Jacqueline Easley				X
Jann Freed				X
John "Jack" Hilmes	X			
Lisa Howard	X			
Carolyn Jenison	X			
Greg Jones	X			
William Page	X			
Mike Simonson				X
Rocky Sposato				X
Steve Wallace				X
Greg Wattier			X	

APPROVAL of the of the proposed Linc PUD Development Plan subject to the following conditions:

1. Compliance with all administrative comments of the Permit and Development Center.
2. Pedestrian crossings shall be identified on site by the use of paint markings and/or material variation to the satisfaction of the Planning Administrator.
3. All trash enclosures shall be constructed of masonry that is compatible with the primary buildings and shall have solid steel gates.
4. All trash enclosures shall include a pedestrian entrance and be constructed in a manner that shields direct views into the enclosure without the use of a gate to the satisfaction of the Planning Administrator.
5. All site lighting shall be directed downward and shielded from adjoining properties. Any pole mounted lighting along private walkways shall not exceed 15 feet in height and any pole mounted lighting in a parking area shall not exceed 20 feet in height.
6. Review and approval of all exterior building materials by the Planning Administrator.
7. All rooftop mechanical equipment shall be screened with material that is architecturally compatible with the building to the satisfaction of the Planning Administrator.
8. All utility meters, transformers, ground-mounted equipment, and other utilities shall be placed alongside or rear facades of the building.
9. All signage shall be provided in accordance with the standards identified in the PUD Conceptual Plan.
10. The developer shall coordinate with DART on any necessary transit facilities to the satisfaction of the Planning Administrator.
11. Provision of bike racks throughout the site to the satisfaction of the Planning Administrator.
12. Provision of tree mitigation plan information on the Development Plan to the satisfaction of the Planning Administrator.
13. Provision of street trees to the satisfaction of the City Arborist and Planning Administrator.
14. Review and approval of the finalize landscape plan, including the hardscape and soft cape elements of the plaza by the Planning Administrator.
15. Existing elements of the Martin Luther King, Jr. Parkway streetscape shall be maintained unless determined non-essential by the Planning Administrator.

(10-2018-7.69)

STAFF RECOMMENDATION TO THE P&Z COMMISSION

Staff recommends approval of the proposed Linc PUD Development Plan subject to the following conditions:

1. Compliance with all administrative comments of the Permit and Development Center.
2. Pedestrian crossings shall be identified on site by the use of paint markings and/or material variation to the satisfaction of the Planning Administrator.
3. All trash enclosures shall be constructed of masonry that is compatible with the primary buildings and shall have solid steel gates.
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11. Provision of bike racks throughout the site to the satisfaction of the Planning Administrator.
12. Provision of tree mitigation plan information on the Development Plan to the satisfaction of the Planning Administrator.
13. Provision of street trees to the satisfaction of the City Arborist and Planning Administrator.
14. Review and approval of the finalize landscape plan, including the hardscape and soft cape elements of the plaza by the Planning Administrator.

15. Existing elements of the Martin Luther King, Jr. Parkway streetscape shall be maintained unless determined non-essential by the Planning Administrator.

STAFF REPORT TO THE PLANNING COMMISSION

I. GENERAL INFORMATION

1. **Purpose of Request:** The Gray's Station PUD Conceptual Plan and associated rezoning was approved by the City Council in 2017. The PUD allows 83.73 acres of former industrial land to be converted into a residential and mixed use development. The northeast and southwest portions of the site would contain mixed-use development. A large-scale storm water basin area is proposed along the southern perimeter of the development that would include a wetland park component. The core of the development would allow for a mix of detached, semi-attached and rowhouse single-family dwellings as well as small multi-family buildings. Multi-story, multiple-family residential buildings are proposed along the north and south perimeters of the street network.

The PUD Conceptual Plan sets a minimum density for the development and basic design parameters. The exact makeup of the development would be determined by the PUD Development Plan of each phase. A total of three phases are identified on the Conceptual Plan that would have a combined total of at least 1,100 dwelling units. This equates to 13 dwelling units per gross acre (83.73 acres) or 28 dwelling units per net acre (39.2 acres).

The submitted Development Plan represents a portion of the phase one area. The applicant is currently preparing a preliminary plat for the balance of the phase one area for submittal in the near future. Site plans and final plats will follow accordingly for the phase one area.

The City Council's action requires all PUD Development Plans to be reviewed by the Plan and Zoning Commission and approved by the City Council. PUD Development Plans are typically reviewed administratively through the site plan review process. The subject site plan and building elevations are also being reviewed by the City's Urban Design Review Board as the applicant has entered into a development agreement with the City.

2. **Size of Site:** 5.36 acres (233,695 square feet).
3. **Existing Zoning (site):** "PUD" Planned Unit Development, "D-O" Downtown Overlay District, "GGP" Gambling Games Prohibition District and "FSO" Freestanding Sign Overlay District.
4. **Existing Land Use (site):** Vacant land.
5. **Adjacent Land Use and Zoning:**

4. Existing Land Use (site): Vacant land.

5. Adjacent Land Use and Zoning:

East - "C-3B" & "PUD"; Uses are a cell tower, a hotel, and vacant land within the Gray's Landing Office I PUD.

West - "C-3B"; Use is vacant land.

North - "C-3B"; Use is the Martin Luther King, Jr. Parkway corridor.

South - "PUD" & C-3B"; Uses are vacant industrial property.

6. General Neighborhood/Area Land Uses: The subject site is located in the southwest portion of the downtown. The surrounding area consists of vacant land, commercial uses, multiple-family residential uses, light industrial uses, the Raccoon River and Gray's Lake.

7. Applicable Recognized Neighborhood(s): The subject property is located within the Downtown Des Moines Neighborhood. All neighborhood associations were notified of the meeting by mailing of the Preliminary Agenda for the February 1, 2017 meeting of the Commission on January 12, 2018. Additionally, separate notifications of the hearing for this specific item were mailed on January 22, 2018 (10 days prior to the hearing [2/1/18]) to the neighborhood association and to the primary titleholder on file with the Polk County Assessor for each property within 250 feet of the subject property. A Final Agenda for the February 1, 2017 meeting was mailed to all the recognized neighborhood associations on January 26, 2018. A Final Agenda for the February 15, 2017 meeting was mailed to all the recognized neighborhood associations on February 9, 2018. The Downtown Des Moines Neighborhood Association mailings were sent to Tony Filippini, 1719 Grand Avenue #210, Des Moines, IA 50309.

8. Relevant Zoning History: On July 24, 2017, the City Council conditionally approved the Gray's Station PUD Conceptual Plan and the first reading of the rezoning ordinance by Roll Call Number 17-1295. On August 2, 2017, the City Council approved the second reading of the rezoning ordinance by Roll Call Number 17-1307. On August 14, 2017, the City Council approved the final reading of the rezoning by Roll Call Number 17-1407, thereby approved Ordinance Number 15,600.

The rezoning and PUD Conceptual Plan were reviewed by the Plan and Zoning Commission on July 6, 2017 and July 20, 2017.

9. PlanDSM Land Use Plan Designation: The subject site is designated as "Downtown Mixed Use" on the Future Land Use Map. The balance of the Gray's Station PUD is designated a mix of "Downtown Mixed Use" and "Neighborhood Mixed Use" on the Future Land Use Map.

10. Applicable Regulations: Taking into consideration the criteria set forth in Chapter 18B of the Iowa Code, every Development Plan and required documents submitted pursuant to Division 13 of the Zoning Ordinance shall be reviewed by the Planning Director, who

shall approve the Development Plan if it complies with the standards of Division 13 and it is in compliance with the Conceptual Plan.

In this case, the Development Plan is being referred to the Plan and Zoning Commission and the City Council for review and approval in accordance with the requirements of the PUD Conceptual Plan.

II. ADDITIONAL APPLICABLE INFORMATION

- 1. PUD Conceptual Plan Standards:** The following are relevant standards from the Conceptual Plan that must be considered in the review of the proposed Development Plan.

A) Overall Neighborhood Character

1. Development shall establish vehicular, mass transit, bicycle, and pedestrian linkages with the following:
 1. Downtown Central Business District;
 2. Gray's Lake;
 3. Development of the DICO site.
2. Each project phase/component shall provide consistency and compatibility of character across the development.

The balance of Section II of this report covers items that impact neighborhood character.

B) Street Character

1. All components of the development shall provide a unified streetscape system that utilizes the same materials, design elements and equipment throughout the redevelopment area. Minor variations are allowed from street to street to create street identity or highlight focal areas of the plan, but the overall design of each street shall still contribute to the neighborhood identity.
2. Development shall provide a street system primarily related to downtown street grid that provides safety and ease of use by vehicular users.
 1. Public streets shall not have cul-de-sacs or tight winding curves.
 2. Development shall include human scaled, user friendly elements.
 3. Development shall provide a streetscape system that is walkable and that proved safety, comfort, and convenience for pedestrians.
3. Development shall create a pedestrian friendly environment along Tuttle and SW 11th Street that provides clear connections between the residential, mixed use and commercial uses.
4. Street cross section configurations shall be as depicted in the approved Conceptual Plan – Sheet 12: Conceptual Street & Alley Sections. Street cross

sections may vary where existing or planned utility infrastructure will constrain and/or limit them.

These standards (B1, B2, B3 & B4) address street design and transportation. In addition, the Conceptual Plan includes a note that states "any development plan review is subject to providing a traffic impact analysis reviewed by the City Traffic Engineer."

A traffic study has been prepared and reviewed by the City's Traffic and Transportation Division. The right-of-way widths shown on the Development Plan for the adjoining segments of Tuttle Street and SW 11th Street comply with the Conceptual Plan. Tuttle Street would include a cycle track along the south side of the street that would feed into the rest of the development as it is built out. A cycle track on SW 11th Street is not possible due to constraints. Traffic engineering staff is currently working with the traffic study consultant to evaluate how best to provide adequate automobile and bicycle facilities on this portion of SW 11th Street within the limits of the right-of-way.

- C) Building exteriors in areas A, B, H, N, O, W, X, Y and Z shall be clad in durable materials such as brick, metal, stone, cement board, etc.
 - 1. Exterior insulation systems shall not be used.
 - 2. Industrial pre-engineered buildings shall not be allowed.
 - 3. Rooftop mechanical equipment shall be enclosed or otherwise screened from public rights of way.
- D) A minimum of 75% of the surface area (exclusive of windows and doors) of facades fronting and perpendicular to a public street must be glass, brick, concrete panels, architectural concrete block (such as split-face or burnished block), architectural metal panels or stone. Fiber cement or wood panels are also acceptable. Vinyl is prohibited.

The development would consist of two buildings that are sided with a mix of resin panels, metal panels and fiber cement board. These materials would be elevated above the ground by a short concrete base. The resin panels would have exposed fasteners and would utilize a rainscreen installation method. The metal siding would be 24-gage thickness and would have concealed fasteners. The portion of the north building that contains commercial tenant bays would be sided with cast stone.

Balconies would be constructed of wood that is wrapped in light gage metal and would have metal railing. Doors and windows on the ground level would consist of aluminum storefront products. Windows in the upper levels would consist of vinyl products.

The proposed resin siding material is a relatively new product and staff is not aware of any local examples of its use. Staff is open to the use of new materials and appreciates the development team's effort to give the project a unique character that is different from other downtown buildings. When staff has evaluated new materials

in the past, a warranty period of at least 25 years has been used in determining a material to be durable. This product has a warranty period of 10 years.

Staff has provided the development team a list of questions and will continue to evaluate the material. Staff recommends approval subject to approval of all building materials by the Planning Administrator. This will give the applicant time to provide additional information and for staff to seek input from the Urban Design Review Board.

- E) Primary facades of mixed-use and commercial structures shall be primarily parallel to the public right-of-way except in the subareas of W and X.

The proposed building facades along West Martin Luther King, Jr. Parkway and SW 11th Street comply with this standard. The south building is proposed to have an angled offset to allow for a plaza space near the SW 11th Street intersection. This offset would provide immediate views of the remainder of the Gray's Station development as drivers and pedestrians enter the development from SW 11th Street once it is built out. Staff believes that the proposal meets the intent of this standard.

- F) Building Height – Downtown Mixed Use
1. Minimum height for all uses shall be four stories.

Both buildings would consist of five stories, which complies with this standard.

- G) Building Entrances - Downtown Mixed Use
1. Buildings shall have entrances oriented toward primary street(s) and public plaza(s).
2. At least one building entrance for residential uses should directly access the street or public plaza when a residential use is located above street-level retail or commercial uses.

The proposed buildings would have a multitude of entrances oriented toward the adjoining streets, providing access to public spaces within the building, as well as direct access to walkup residential units and the commercial bays.

Commercial units would front West Martin Luther King, Jr. Parkway. An entrance to the residential units would be located at the east end of the north façade.

- H) Building Frontage
1. Building frontage calculations exclude the length of common alleys serving multiple properties, city-mandated planting areas, and utility corridors. In Low-Medium Density Residential areas, pedestrian connections between buildings shall also be excluded from frontage calculations.
2. Downtown Mixed Use
a. All buildings should have frontage on principal street(s), public plaza, park or greenway of not less than 70 percent of the lot with the following exceptions:
i. Frontage is used for an existing utility corridor.
ii. The curvature of the road precludes efficient frontage.

- iii. Where exceptions J.2.a.1 and J.2.a.2 preclude a building from being one foot from the property line, the design of the buildings should seek a similar design intent through building and site design to frame the street/public way and create an engaging pedestrian environment.
- b. For commercial and mixed-use buildings, at least 70 percent of the building frontage should be within one foot of the property line with the following exceptions:
 - i. Frontage is used for plaza space or an existing utility corridor,
 - ii. The curvature of the road precludes efficient frontage along the property line.
 - iii. Where exceptions J.2.b.1 and J.2.b.2 preclude a building from being one foot from the property line, the design of the buildings should seek a similar design intent through building and site design to frame the street/public way and create an engaging pedestrian environment.

The portions of the buildings that front West Martin Luther King, Jr. Parkway and SW 11th Street would have setbacks that range from zero to 3 feet due to the irregular shape of the parcel. The building setback along Tuttle Street would vary due to the inclusion of a plaza space on the site. The proposal exceeds the 70% building frontage minimum standard and complies with these standards.

I) Building Roof Form –Downtown Mixed Use

- 1. All buildings shall have a parapet or flat roof. Tower elements are allowed.

The proposed building would have a parapet roof design.

J) Downtown Mixed Use & High Density Residential Signs. Signs shall be in accordance with the following:

- 1. Any combination of two on-premises type A, type B or type C identification and/or advertising signs per occupant of a building not to exceed one and one-half square feet in area per lineal foot of building frontage per occupant, and not to exceed a maximum of 100 square feet per occupant, shall be permitted. If building frontage consists of 25 percent or less of the total lot frontage, the occupant or occupants of that building shall be permitted, in lieu of the above, one square foot in area per lineal foot of lot frontage per lot and not to exceed a maximum of 100 square feet per occupant. Such signs shall be permitted on public or private canopies attached to and supported by a building.
- 2. All building or wall signs shall project no more than seven feet from the building, unless such projection is over the street right-of-way, in which case such sign shall project no more than 36 inches. Such projecting signs shall be not less than ten feet from the ground in areas in which there is only pedestrian traffic and not less than 15 feet from the ground in areas in which there is vehicular traffic. Signs shall be not more than 20 square feet in area per occupant and shall not extend more than four feet above the roofline. Projecting signs may be anchored to the roof or building.

3. The permitted types of free-standing signs are monumental and sandwich board signs. Pole signs and portable signs other than sandwich board signs are not permitted.
4. One sandwich board sign shall be permitted per building tenant for identification and on-premises advertising. Such signs shall comply with ingress and egress requirements outlined in the city building code.
5. Sandwich board signs are permitted upon the sidewalks within the public rights-of-way, subject to the following:
 - a. Such signs are permitted only during the hours the business is open to the public, and must be located adjacent to the premises.
 - b. Such signs shall be placed no closer than two feet from the back of the street curb.
 - c. Such signs shall be no more than ten square feet in area on each side, no more than five feet in height, and no more than three feet in width.
 - d. There shall be no electrical service to such signs.
6. One non-illuminated building or wall identification sign per occupant, not to exceed two square feet in area, shall be permitted on buildings where occupants have no occupant frontage, and shall be located immediately adjacent to the principal entrance to said occupant's premises.
7. Two on-premises type B attraction panel signs shall be permitted on theatre marquees. The total sign area per marquee shall not exceed 160 square feet.
8. Off-premise advertising signs are not permitted.

Wall mounted signage that identifies the development is shown on the submitted building elevation drawings. Signage for the commercial tenant spaces cannot be determined until tenants are secured. Signage will continue to need to be reviewed in the future due to tenant turnover. A 42-square foot, "Grays Station" sign would be located on the west façade of the north building. A 47.25-square foot, "Grays Station" sign would be located on a portion of the east façade of the north building that faces north towards the SW 11th Street and West Martin Luther King, Jr. Parkway intersection. These signs comply with the PUD Conceptual Plan standards.

The Conceptual Plan includes language on the use of sandwich board signs within right-of-way. The use of right-of-way is regulated by Chapter 102 of the Municipal Code. The Zoning Ordinance, including PUDs, cannot supersede Chapter 102. Any signage or other use of the right-of-way must be approved by the City Traffic Engineer.

K) Parking Requirements. Non-residential areas.

1. Surface parking spaces shall not exceed 3.75 spaces per 1000 GSF of commercial area.

The northern building contains approximately 14,500 square feet of commercial space. This standard suggests that a maximum of 15 parking spaces should be provided for the commercial space. This building would also include 84 dwelling units. The southern building would include 131 dwelling units, for a combined total of 215 units. The entire site would contain 197 surface parking spaces. The

south building would include private garages attached to ground floor units that contain the equivalent of 30 spaces.

The PUD Conceptual Plan does not set minimum or maximum parking standards for residential uses. Private garage spaces are not typically counted towards meeting off-street parking requirements. Based on this, the proposal represents 0.92 parking spaces per dwelling unit. This ratio is reduced to 0.85 if the maximum allowed 15 parking spaces for the commercial uses are removed from the total available for the dwelling units. Staff believes the proposed amount of parking is appropriate and complies with the intent of the PUD Conceptual Plan.

2. Surface parking shall have an emphasis of trees, shrubs and other plantings placed around the perimeter of any parking area and within large parking lots to create a more attractive area. The landscape plan should generally enhance the visual appearance of the building, parking area and any pedestrian areas. Design of surface parking landscape enhancements to be reviewed by the Planning & Zoning Commission and/or City Council.
3. Vehicular use areas with 50 or more car stalls require interior landscape area as detailed below:
 - a. Interior landscaping shall at a minimum consist of one planting bed no smaller than 9' x 17' for every 25 interior spaces containing 1 overstory tree and 3 shrubs.
 - b. Required landscaping and planting beds may be clustered but there may be no more than 25 spaces in a single row, or 50 spaces in a double row without an intervening planting bed.
 - c. Trees planted in Parking Lot Interior Plantings are to be a minimum of 1 ½" caliper.
 - d. Use of overstory trees is required to increase shade and reduce "urban heat island" effect.
 - e. A minimum of one (1) overstory tree and three (3) shrubs for every twenty-five (25) spaces exclusive of tree planting required in perimeter landscape area.
 - f. Corner islands beyond the 7' perimeter landscape strip may be counted toward interior planting requirements even though they are contiguous with perimeter landscape areas.
 - g. The 7' perimeter landscape area may not be counted toward interior planting requirements.
 - h. Perimeter landscaped areas in excess of 7' may partially be counted toward internal planting requirements.
 - i. Interior planting islands that are at least 9'x17' should be spaced no further than 20 parking spaces or 180' apart.
4. Parking lots adjacent to public streets shall be screened by walls, fences, berms, landscaping or a combination thereof. Non-paved surfaces shall be landscaped to meet Des Moines Landscape Standards for zoning districts C-3, & C-3A.

Staff has reviewed the submitted Development Plan against these standards (K2, K3 & K4) and believes that the proposal complies with the PUD Conceptual Plan.

L) Mass Transit integration

1. Development shall accommodate long term transit connections within and between neighborhoods.
2. The Des Moines Area Regional Transit (DART) Authority shall review all streetscape plans and designate locations for transit stops and shelters.
3. Developers and DART shall work together to establish feasible transportation linkages on a case by case basis.

The submitted Development Plan does not identify how these standards are being met. Staff recommends approval subject to the applicant coordinating with DART on any necessary transit facilities to the satisfaction of the Planning Administrator.

M) Public amenities

1. Bicycle and Pedestrian Facilities:
 1. Development shall provide on-street bike lanes and off-street trails that connect to existing urban system.

Tuttle Street would include a cycle track along the south side of the street that will feed into the rest of the development as it is built out. A cycle track on SW 11th Street is not possible due to constraints. Traffic engineering staff is currently working with the traffic study consultant to evaluate how best to provide adequate automobile and bicycle facilities in this portion of SW 11th Street within the limits of the right-of-way.

2. Development will include a Pedestrian Bridge connection to Gray's Lake over the Raccoon River. The Pedestrian Bridge is anticipated to be located near the southwest corner of the identified PUD boundary and west of the existing pump station associated with the enhanced stormwater basin.

The applicant is currently working with the Engineering Department, the Park and Recreation Department, and the Office of Economic Development on coordinating the construction of the pedestrian bridge and stormwater basin with an anticipated 2018 construction date. A trail from the south side of Tuttle Street to the bridge is planned to be constructed this year as well.

3. Development shall pursue the inclusion of bicycle sharing services (e.g., B-Cycle).

The submitted Development Plan does not identify how this standard is being met.

4. Sidewalks along streets, in common open space areas, and between buildings shall maintain a minimum width of 5 feet.

The minimum clear walk path of all proposed public sidewalks and all onsite walks is 6 feet, which complies with this minimum. A 6-foot minimum is the appropriate standard for a development of this nature.

1. **Natural Features:** Development of the site must be in compliance with the City's Tree Removal and Mitigation Ordinance (Section 42-550 of the City Code).

The PUD Conceptual Plan includes the following language:

"This site is subject to the Tree Removal and Mitigation Ordinance of the City Code. Hubbell Realty Company is entering into a development agreement with the City of Des Moines that outlines a custom approach to tree removal and mitigation for the Gray's Station Planned Unit Development. No tree survey or canopy area method information was submitted for review; this information will be provided with the final development plan."

A tree survey for the entire PUD has been provided. The submitted Development Plan must be updated to note how many of the mitigation plantings this phase of the development is providing. The mitigation planting must be provided across the entire PUD in amounts that are appropriate to the size of each phase.

2. **Grading & Storm Water Management:** All grading is subject to an approved grading permit and soil erosion control plan. The applicant is required to demonstrate compliance with the City's Stormwater Management requirements to the satisfaction of the City's Permit and Development Center. The proposed stormwater management facilities for this project and future phases of the Gray's Station PUD are depending on the proposed basin facility. The applicant is currently working with the Office of Economic Development and the Engineering Department on coordinating the construction of the stormwater basin with the understanding that it must be constructed in conjunction with the LINC project. These include the infrastructure necessary to convey the storm water to the basin.
3. **Urban Design:** The south building is proposed to have an angled offset to allow for a plaza space near the SW 11th Street intersection. This offset would provide immediate views of the remainder of the Gray's Station development as drivers and pedestrians enter the development from SW 11th Street once it is built out. The submitted Development Plan shows the plaza space consisting of a mix of hard and soft scape areas. The applicant is proposing to not include street trees along the adjoining portion of Tuttle Street with the intention of providing a stronger visual connection of the plaza to the street to enhance the sense that this is a public plaza. Staff believes that street trees should be provided and that overstory trees or some other mechanism for providing shade within the plaza should be provided given its southern exposure. Staff is open to discussing the possibility of altering or reducing the typical street tree pattern along the plaza to allow for visual connectiveness.

The site would include a drive along the west edge of the project to provide access to Tuttle Street. This drive would act as a transition point from this project to what gets built by the adjoining developer in the future. Staff is working with both development teams to ensure that this drive has a street feel to it to enhance pedestrian circulation and the sense of connectivity between the two developments. The submitted Development Plan includes parallel parking on the east side of the drive and a sidewalk. Trees are

provided along the drive once it is north of the Tuttle Street building. Staff believes that two trees should be provided along the east side of the drive, where it adjoins the building. In addition, the walk that runs along the back side of the Tuttle Street building should be extended west to the drive. When the developer to the west builds, staff expects that matching trees, walk and inset parking will be installed along the west side of the drive.

The intersection of West Martin Luther King, Jr. Parkway and SW 11th Street includes brick pillars on each corner that were constructed as part of the parkway at many of the intersection along the parkway. The applicant is proposing to remove these columns as they believe they would conflict with views of the building and that the building would do a better job of "holding the corner" than the pillars. Staff is concerned by the potential impact of losing these elements in providing a consistent appearance for the parkway and its potential in setting a precedent that allows future changes to the detriment of the integrity of the original streetscape design. Staff recommends approval of the Development Plan subject to additional analysis by staff, including seeking input from the Urban Design Review Board.

SUMMARY OF DISCUSSION

John "Jack" Hilmes asked if anyone was present to speak on this item. No one was present or requested to speak.

Dory Briles made a motion to move item #4 to the consent agenda. Motion carried 7-0-1 (Greg Wattier abstained)

COMMISSION ACTION:

Greg Wattier Indicated he was abstaining from participation on this item due to conflict of interest.

Dory Briles moved approval of the proposed PUD Development Plan "Linc" subject to the following conditions.

1. Compliance with all administrative comments of the Permit and Development Center.
2. Pedestrian crossings shall be identified on site by the use of paint markings and/or material variation to the satisfaction of the Planning Administrator.
3. All trash enclosures shall be constructed of masonry that is compatible with the primary buildings and shall have solid steel gates.
4. All trash enclosures shall include a pedestrian entrance and be constructed in a manner that shields direct views into the enclosure without the use of a gate to the satisfaction of the Planning Administrator.

5. All site lighting shall be directed downward and shielded from adjoining properties. Any pole mounted lighting along private walkways shall not exceed 15 feet in height and any pole mounted lighting in a parking area shall not exceed 20 feet in height.
6. Review and approval of all exterior building materials by the Planning Administrator.
7. All rooftop mechanical equipment shall be screened with material that is architecturally compatible with the building to the satisfaction of the Planning Administrator.
8. All utility meters, transformers, ground-mounted equipment, and other utilities shall be placed alongside or rear facades of the building.
9. All signage shall be provided in accordance with the standards identified in the PUD Conceptual Plan.
10. The developer shall coordinate with DART on any necessary transit facilities to the satisfaction of the Planning Administrator.
11. Provision of bike racks throughout the site to the satisfaction of the Planning Administrator.
12. Provision of tree mitigation plan information on the Development Plan to the satisfaction of the Planning Administrator.
13. Provision of street trees to the satisfaction of the City Arborist and Planning Administrator.
14. Review and approval of the finalize landscape plan, including the hardscape and soft cape elements of the plaza by the Planning Administrator.
15. Existing elements of the Martin Luther King, Jr. Parkway streetscape shall be maintained unless determined non-essential by the Planning Administrator.

Motion passed 7-0-1 (Greg Wattier abstained)

Respectfully submitted,



Erik Lundy, AICP
Senior City Planner

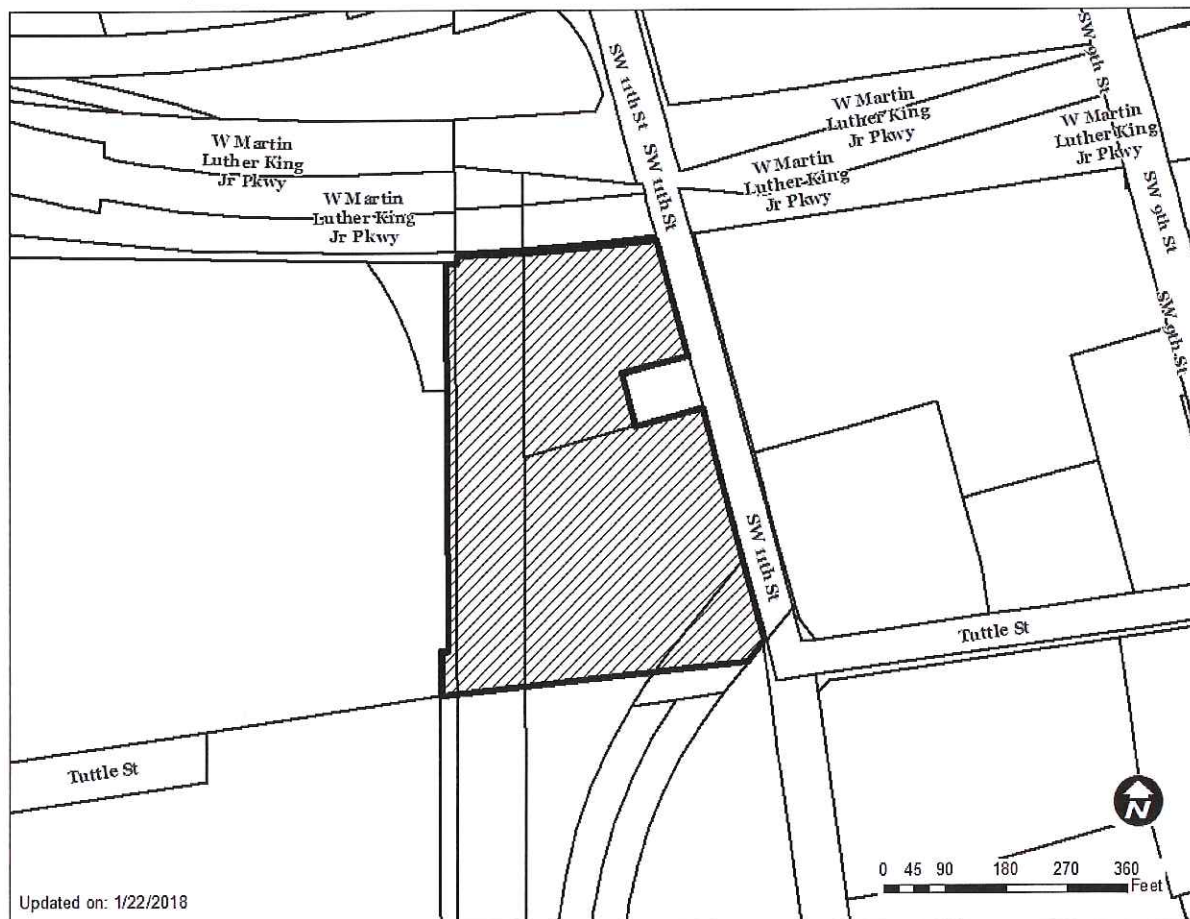
EML:tjh

cc: HUBBELL DEVELOPMENT SERVICES
CIVIL DESIGN ADVANTAGE, LLC
SLINGSHOT ARCHITECTURE

Hubbell Development Services (developer) represented by Kris Saddoris (officer) for property located at 210 Southwest 11th Street and 310 Southwest 11th Street. The subject property is owned by HRC NFS I, LLC.				File # 10-2018-7.69	
Description of Action	Review and approval of a PUD Development Plan "LINC", to allow development of a 5-story, 84-unit multiple-family residential dwelling; a 5-story, a 131-unit multiple-family residential dwelling and a 1-story clubhouse building.				
PlanDSM Future Land Use	Current: Downtown Mixed Use. Proposed: N/A.				
Mobilizing Tomorrow Transportation Plan	No planned improvements.				
Current Zoning District	"PUD" Planned Unit Development, "GGP" Gambling Games Prohibition Overlay District, "D-O" Downtown Overlay District, and "FSO" Freestanding Signs Overlay District.				
Proposed Zoning District	N/A.				
Consent Card Responses	In Favor	Not In Favor	Undetermined	% Opposition	
Subject Property					
Outside Area (200 feet)					
Plan and Zoning Commission Action	Approval	X	Required 6/7 Vote of the City Council	Yes	
	Denial			No	X

Hubbell Development Services, LINC, 210 & 310 Southwest 11th Street

10-2018-7.69



1 inch = 176 feet

32

Item #1

GRAYS STATION LINC - 1709.517

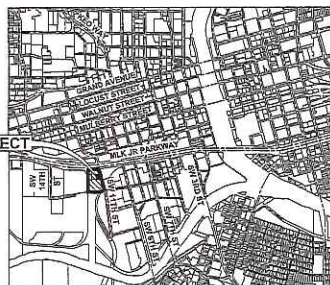
24

SITE PLAN FOR:

LINC

210 SW 11TH ST. & 310 SW 11TH ST.
DES MOINES, IOWA

VICINITY MAP



PROJECT SITE ADDRESS
210 SW 11TH STREET & 310 SW 11TH STREET

LAND USE
EXISTING: UNDEVELOPED
PROPOSED: APARTMENT HOUSING

DEVELOPMENT SUMMARY
SITE AREA: 5.38 ACRES (233,695 SF)

RESIDENTIAL UNIT SUMMARY
NORTH: 84 UNITS
SOUTH: 215 UNITS

PARKING PROVIDED
STANDARD: 79 SPACES NORTH, 100 SPACES SOUTH, 158 SPACES TOTAL
GARAGE: 0 SPACES NORTH, 30 SPACES SOUTH, 30 SPACES TOTAL
ADA: 4 SPACES NORTH, 9 SPACES SOUTH, 9 SPACES TOTAL
83 SPACES 144 SPACES 227 SPACES

ZONING: GRAY'S STATION PUD-ZON2017-00057

BIKE RACK CAPACITY: 30 BIKES

BUILDING HEIGHT: 62 FEET

IMPERVIOUS AREA
BUILDINGS: 68,044 SF
PAVEMENT: 109,059 SF
SIDEWALKS: 29,138 SF
TOTAL: 207,201 SF

OPEN SPACE PROVIDED: 25,494 SF (11%)

BUILDING SUMMARY
NORTH BUILDING
TOTAL SQUARE FOOTAGE: 112,374 SF
BUILDING HEIGHT: 62'-5-5/8"
NUMBER OF FLOORS: 5 STORIES
SOUTH BUILDING
TOTAL SQUARE FOOTAGE: 189,381 SF
BUILDING HEIGHT: 55'-5-5/8"
NUMBER OF FLOORS: 5 STORIES

SUBMITTAL DATES
FIRST SUBMITTAL: 01/09/2018
SECOND SUBMITTAL: 02/07/2018

DATE OF SURVEY: 03/30/2018

BENCHMARK
1. BRASS CAP IN NE CORNER OF TRAFFIC SIGNAL BASE
ELEV. 258.54
CITY OF DES MOINES ID. 725

UTILITY WARNING
ANY UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY AND RECORDS OBTAINED BY THIS SURVEYOR. THE SURVEYOR MAKES NO GUARANTEE THAT THE UTILITIES SHOWN COMPRISE ALL THE UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UTILITIES SHOWN ARE IN THE EXACT LOCATION SHOWN.

NO.	DESCRIPTION
C0.1	COVER SHEET
C1.1-1.2	TOPOGRAPHIC SURVEY/ DEMOLITION PLAN
C2.1	REFERENCE PLAN
C2.2-2.3	DIMENSION PLAN
C3.1-3.2	GRADING PLAN
C4.1-4.2	UTILITY PLAN
C4.3-4.4	STORM WATER FACILITIES MAINTENANCE PLAN
C5.1	DETAILS
L1.1-1.2	LANDSCAPE PLAN

GENERAL LEGEND

PROPOSED	EXISTING
LOT LINE	SANITARY MANHOLE
CENTER LINE	FIRE HYDRANT
PERMANENT EASEMENT	WATER CURB STOP
TYPE SW-501 STORM INTAKE	STORM SEWER MANHOLE
TYPE SW-503 STORM INTAKE	DECIDUOUS TREE
TYPE SW-505 STORM INTAKE	ELECTRIC POWER POLE
TYPE SW-508 STORM INTAKE	GUY ANCHOR
TYPE SW-513 STORM INTAKE	STREET LIGHT
TYPE SW-401 STORM MANHOLE	UTILITY POLE W/ LIGHT
TYPE SW-301 SANITARY MANHOLE	ELECTRIC BOX
STORM/SANITARY CLEANOUT	TRAFFIC SIGN
WATER VALVE	GAS VALVE BOX
FIRE HYDRANT ASSEMBLY	BENCHMARK
SIGN	SOIL BORING
DETECTABLE WARNING PANEL	GAS MAIN
SANITARY SEWER WITH SIZE	FIBER OPTIC
SANITARY SERVICE	OVERHEAD ELECTRIC
STORM SEWER	UNDERGROUND ELECTRIC
STORM SERVICE	SANITARY SEWER W/ SIZE
WATERMAIN WITH SIZE	STORM SEWER W/ SIZE
WATER SERVICE	WATER MAIN W/ SIZE
SAWCUT (FULL DEPTH)	
SILT FENCE	

APPLICANT

HUBBELL DEVELOPMENT SERVICES
CONTACT: KORI SANDOZ
6900 WESTOWN PKWY
WEST DES MOINES, IA 50266
PH: (515) 243-3328
FX: (515) 280-2000

ENGINEER/SURVEYOR

CIVIL DESIGN ADVANTAGE, LLC
CONTACT: RYAN HARDISTY
3405 SE CROSSROADS DRIVE, SUITE G
GRIMES, IOWA 50111
PH: (515) 369-4400
FX: (515) 369-4410

ARCHITECT

SUNSHINE ARCHITECTURE
CONTACT: CREG WATNER
303 EAST COURT AVE
DES MOINES, IOWA 50309
PH: (515) 243-0074

OWNER

HRD NR LLC
6900 WESTOWN PKWY
WEST DES MOINES, IA 50266

LEGAL DESCRIPTION

A PART OF OUTLOT "Y", CENTRAL DES MOINES INDUSTRIAL PARK, AN OFFICIAL PLAT, AND A PART OF LOTS B1, B2 AND B5, FACTORY ADDITION, AN OFFICIAL PLAT, ALL BEING IN THE CITY OF DES MOINES, POLK COUNTY, IOWA AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE SOUTHEAST CORNER OF LOT 4, SAID CENTRAL DES MOINES INDUSTRIAL PARK, THENCE NORTH 031°45' WEST ALONG THE WESTERLY LINE OF SAID OUTLOT "Y", A DISTANCE OF 63.43 FEET; THENCE NORTH 87°29'10" EAST CONTINUING ALONG SAID WESTERLY LINE, 10.37 FEET, THENCE NORTH 012°40' WEST CONTINUING ALONG SAID WESTERLY LINE, 554.99 FEET TO THE NORTHEAST CORNER OF LOT 5, SAID CENTRAL DES MOINES INDUSTRIAL PARK, THENCE SOUTH 89°04'27" EAST ALONG THE NORTH LINE OF SAID OUTLOT "Y", 18.36 FEET TO THE NORTHEAST CORNER OF SAID OUTLOT "Y", THENCE NORTH 021°12' WEST ALONG THE WEST LINE OF SAID LOT B5, A DISTANCE OF 6.59 FEET TO THE SOUTHERLY RIGHT OF WAY LINE OF MARTIN LUTHER KING JR. PARKWAY, THENCE NORTH 86°10'40" EAST ALONG SAID SOUTHERLY RIGHT OF WAY LINE, 100.23 FEET, THENCE NORTH 84°00'00" EAST CONTINUING ALONG SAID SOUTHERLY RIGHT OF WAY LINE, 199.44 FEET TO THE WESTERLY RIGHT OF WAY LINE OF SW 11TH STREET, THENCE SOUTH 15°27'52" EAST ALONG SAID WESTERLY RIGHT OF WAY LINE, 173.21 FEET, THENCE SOUTH 74°42'28" WEST, 88.77 FEET, THENCE SOUTH 15°27'52" EAST, 75.09 FEET, THENCE NORTH 74°32'17" EAST, 89.91 FEET TO SAID WESTERLY RIGHT OF WAY LINE, THENCE SOUTH 15°27'52" EAST ALONG SAID WESTERLY RIGHT OF WAY LINE, 244.27 FEET TO A CORNER ON THE EASTERLY LINE OF SAID LOT B1, THENCE SOUTH 35°15'32" WEST ALONG SAID EASTERLY LINE, 16.08 FEET, THENCE SOUTH 81°39'24" WEST, 474.35' TO THE POINT OF BEGINNING AND CONTAINING 5.39 ACRES (233,696 SQUARE FEET).

PROPERTY IS SUBJECT TO ANY AND ALL EASEMENTS OF RECORD.

IOWA
ONE CALL
1-800-292-8989
www.iowaonecall.com

GA
CIVIL DESIGN ADVANTAGE
3405 SE CROSSROADS DR. SUITE G, GRIMES, IOWA 50111
PH: (515) 369-4400 Fax: (515) 369-4410
PROJECT NO. 1709.517

SITE PLAN

APPROVED ☐ APPROVED WITH CONDITION ☐

IN ACCORDANCE WITH SECTION 62-207 (a.) 2000 Des Moines MUNICIPAL CODES AS AMENDED.

NO CHANGES TO THIS PLAN UNLESS APPROVED IN WRITING FROM THE PLANNING DIRECTOR OR NEW AMENDED DATED PLAN.

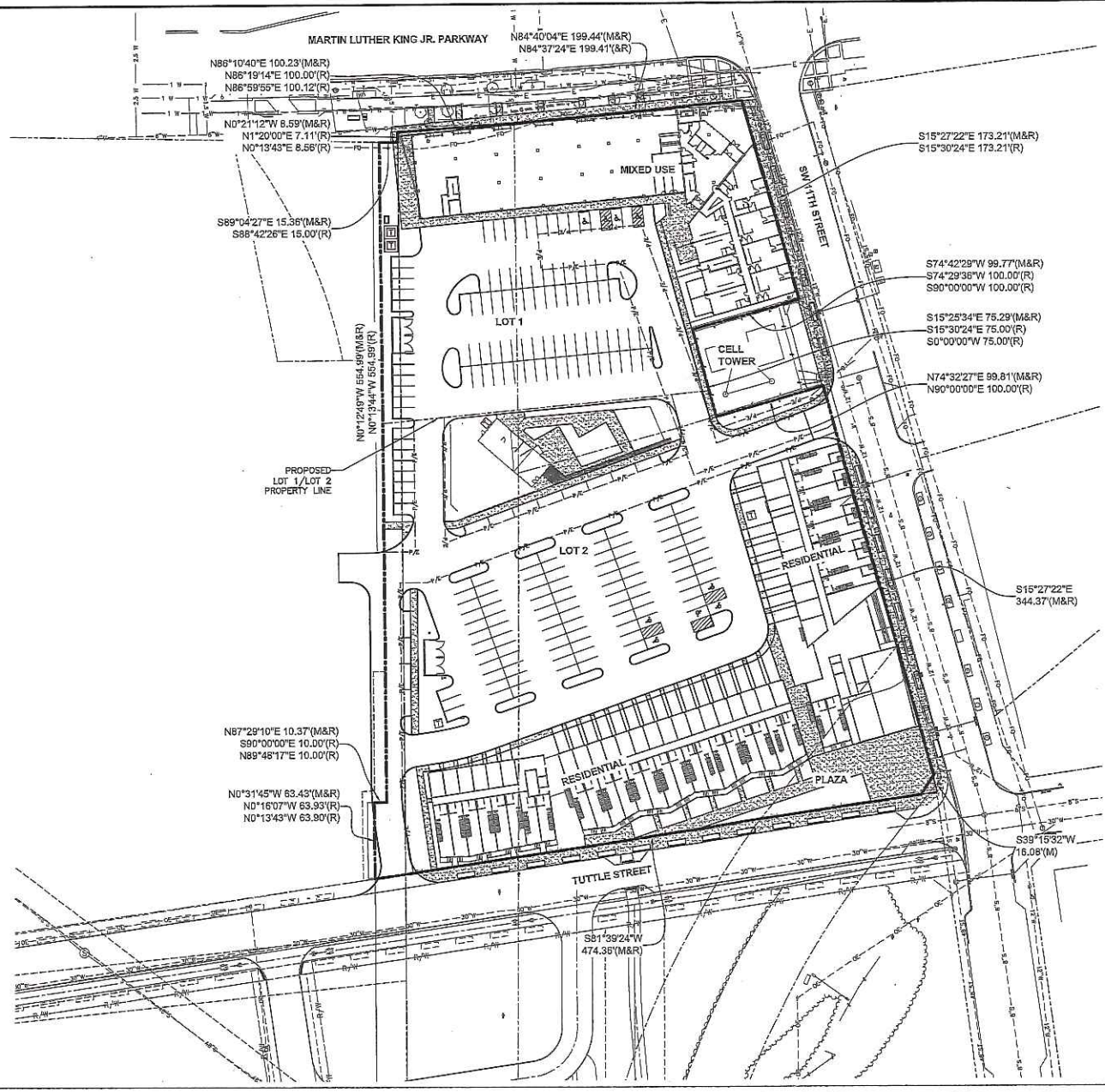
DATE _____ PLANNING DIRECTOR _____

ALL CONSTRUCTION MATERIALS, BUMPSTERS, DETACHED TRAILERS OR SIMILAR ITEMS ARE PROHIBITED ON PUBLIC STREETS OR WITHIN THE PUBLIC R.O.W.

THE MOST RECENT EDITION OF THE SDAS STANDARD SPECIFICATIONS AND ALL CITY SUPPLEMENTALS, IF APPLICABLE, SHALL APPLY TO ALL WORK ON THIS PROJECT UNLESS OTHERWISE NOTED.

I HEREBY CERTIFY THAT THIS ENGINEERING DOCUMENT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF IOWA.

PROFESSIONAL ENGINEER
DATE _____
BY EXPIRATION DATE IS DECEMBER 31, 2019
PAGE(S) OF SHEETS COVERED BY THIS SEAL
C0.1-C5.1



 C2.1 1709.517	LINC REFERENCE PLAN	DES MOINES, IOWA CIVIL DESIGN ADVANTAGE ENGINEERING	EITC
		3408 S.E. CROSSROADS DRIVE, SUITE G GRIMES, IOWA 50111 PHONE: (515) 369-4400 FAX: (515) 369-4410	
		REVISIONS	DATE
		SECOND SUBMITTAL FINAL SUBMITTAL	RELEASED 10/14/11



LINC



CD

CIVIL DESIGN ADVANTAGE

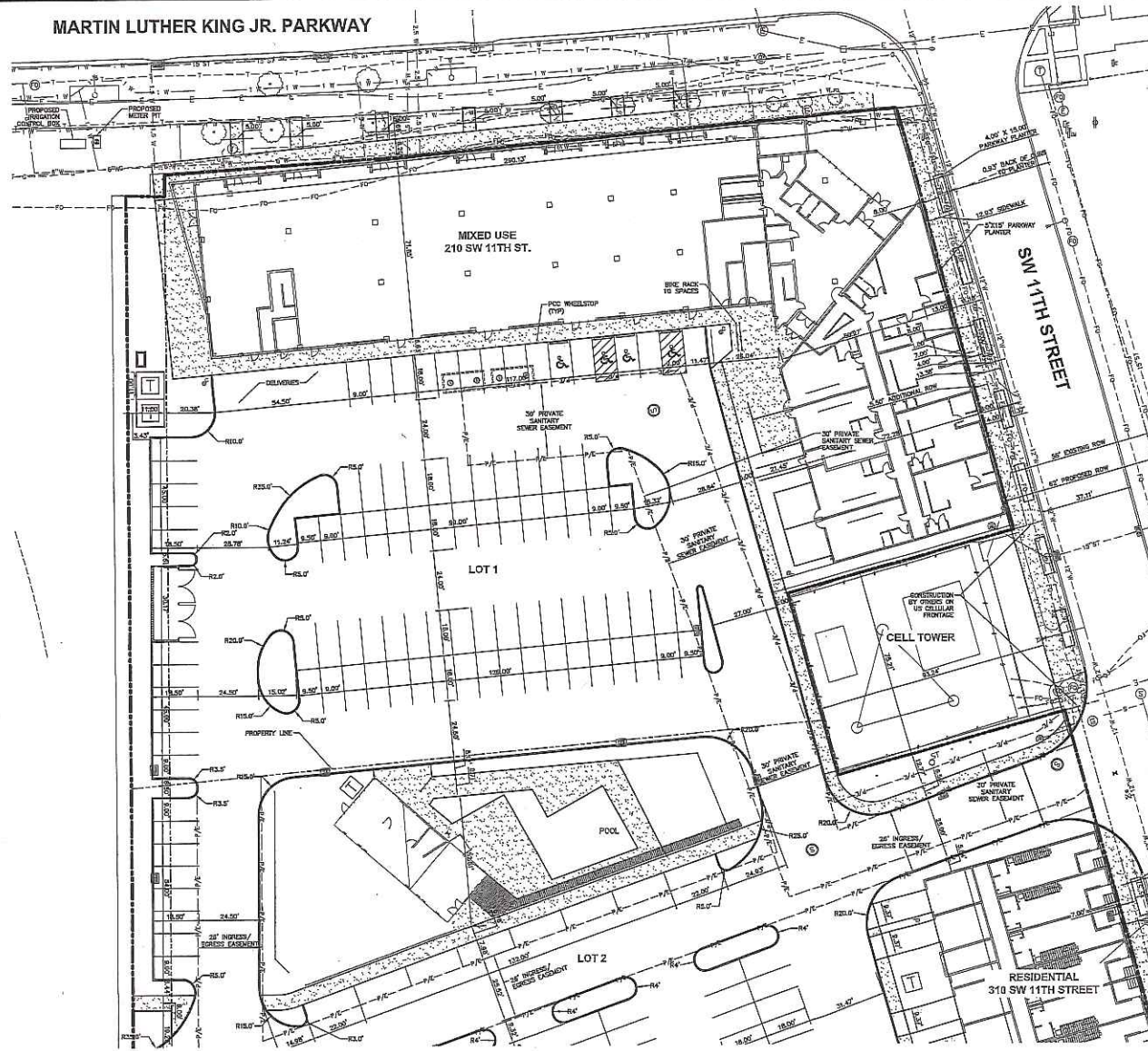
ENGINEER/RAH

ELT/C

3405 S.E. CROSSROADS DRIVE, SUITE G
GRIMES, IOWA 50111
PHONE: (515) 369-4000 FAX: (515) 369-4410

REVISIONS	DATE
SECOND SUBMITTAL	2016-02-07
FIRST SUBMITTAL	2016-01-08

MARTIN LUTHER KING JR. PARKWAY



GENERAL NOTES

1. THE MOST RECENT EDITION OF THE SUBMITTAL SPECIFICATIONS AND ALL CITY SUPPLEMENTALS, IF APPLICABLE, SHALL APPLY TO ALL WORK ON THIS PROJECT UNLESS OTHERWISE NOTED.
2. ALL WORK SHALL COMPLY WITH ALL APPLICABLE CODES AND STANDARDS FOR BUILDING AND FIRE SAFETY.
3. ALL WORK SHALL BE IN ACCORDANCE WITH CITY CODES AND STANDARDS, NOTHING INDICATED ON THE DRAWINGS SHALL RELIEVE THE CONTRACTOR FROM COMPLIANCE WITH ANY APPLICABLE CITY REGULATIONS.
4. PRIOR TO ANY WORK AT THE SITE, THE CONTRACTOR SHALL EXAMINE ANY APPLICABLE DRAWINGS AVAILABLE FROM THE OWNER, ENGINEER, ARCHITECT, AND CONSULT WITH THE CITY ENGINEER, AND UTILITY COMPANY REPRESENTATIVES. NO COMPENSATION WILL BE ALLOWED FOR DAMAGE FROM FAILURE TO COMPLY WITH THIS REQUIREMENT.
5. ONE WEEK PRIOR TO CONSTRUCTION WHEN CITY BLOCK OR ANY CONNECTION TO PUBLIC SEWER CONTRACTOR SHALL NOTIFY THE CITY CONSTRUCTION DIVISION. CONTRACTOR SHALL NOTIFY CITY OF DES MOINES ENGINEERING DEPT. AT LEAST 48 HOURS PRIOR TO COMMENCEMENT OF CONSTRUCTION TO COORDINATE INSPECTIONS. CALL 515-281-4177 OR 515-281-4177.
6. ALL CONSTRUCTION WITHIN PUBLIC R.O.W. / EASEMENTS AND/OR ANY CONNECTION TO PUBLIC SEWERS AND STREETS SHALL COMPLY WITH THE CITY STANDARDS. NO STOPS ARE ALLOWED IN SANITARY MANHOLES.
7. ALL EASEMENTS ARE TO BACK OF CURB BUILDING FACE ON PROPERTY LINE UNLESS OTHERWISE NOTED.
8. CONTRACTOR TO NOTIFY BUILDING DEPARTMENT WITH ARCHITECTURAL PLANS.
9. PLACE 2 1/4" RICH EXPANSION JOINT BETWEEN ALL P.C.C. PAVEMENT/SURFACES AND BUILDING. PLACE 1/2" RICH EXPANSION JOINT BETWEEN SIDEWALKS AND P.C.C. PAVEMENT/SURFACES.
10. REMOVE ALL GRASS SPILLS INTO R.O.W. AT THE END OF EACH WORK DAY.
11. ALL PROPERTY SHALL BE PROTECTED FROM GRUBS OR OTHER OPERATIONS. ANY PUS DISTURBED SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE.
12. DO NOT STORE CONSTRUCTION MATERIALS AND EQUIPMENT IN THE RIGHT OF WAY.
13. DO NOT EXPOSE DESTRUCTIBLE MATERIALS AND EQUIPMENT OUTSIDE THE CONSTRUCTION LIMITS. THE CONTRACTOR WILL NOT BE PERMITTED TO PARK OR STORE VEHICLES AND EQUIPMENT ON USE THESE AREAS FOR STORAGE OF MATERIALS, STORAGE, PARKING AND SERVICE AREAS WILL BE SUBJECT TO THE APPROVAL OF THE OWNER.
14. THE CONTRACTOR IS RESPONSIBLE FOR REPLACING ANY AREAS OF PAVEMENT OR SIDEWALK NOT TO BE REMOVED THAT IS DAMAGED DUE TO UPGRADING EQUIPMENT ON THE SIDEWALK.
15. THE CONTRACTOR MAY BE REQUIRED TO PLACE TEMPORARY WARNING DEVICES AND SAFETY FENCE AT CERTAIN LOCATIONS WHERE REPLACEMENT FEATURES ARE NOT INSTALLED THE SAME DAY, AS DIRECTED BY THE ENGINEER OR THE CITY.
16. THE ORIGINAL CONTRACTOR SHALL BE RESPONSIBLE FOR THE COORDINATION OF WORK BETWEEN SUPPLIERS AND SUBCONTRACTORS INVOLVED IN THE PROJECT, INCLUDING OBTAINING OF CONSTRUCTION DETAILS.
17. CONCRETE REMOVAL FOR DRAINAGE IMPROVEMENTS SHALL BE REMOVED TO THE NEAREST MANHOLE OR JUMP.
18. CONTRACTOR SHALL VERIFY REMOVAL LIMITS WITH CITY'S CONSTRUCTION INSPECTOR PRIOR TO ANY CONCRETE REMOVAL.
19. ALL WORK WITHIN CITY RIGHT-OF-WAY SHALL REQUIRE A RIGHT-OF-WAY PERMIT.
20. THE CONTRACTOR IS REQUIRED TO OBTAIN A SCHEDULED DRIVE APPROACH PERMIT.
21. ALL PERMITS AND ADDITIONAL FEES REQUIRED TO COMPLETE THE WORK SHALL BE INCLUDED IN THE CONTRACTOR BID. ALL WORK SHALL BE IN ACCORDANCE WITH THE CITY'S STANDARD SPECIFICATIONS FOR THE CITY OF DES MOINES FIRE DEPARTMENT.

CITY OF DES MOINES STANDARD NOTES

1. ANY AMENDMENTS OR CHANGES TO THE PROJECT SITE THAT DO NOT MEET WHAT IS SHOWN ON THE SITE PLAN NEED TO BE APPROVED BY THE OWNER AND DEVELOPMENT CENTER PRIOR TO INSTALLATION/CONSTRUCTION.
2. LIGHTING SHALL CONSIST OF LOW-GLARE CUT OFF TYPE FIXTURES TO REDUCE THE RISK OF LIGHT POLLUTION ON ADJACENT PROPERTIES.
3. THE REQUIRED LANDSCAPING, BOTH EXISTING AND PROPOSED, SHALL BE MAINTAINED FOR THE LIFE OF THE PROJECT OR OCCUPANCY OF CERTIFICATE OF ZONING COMPLIANCE.
4. NO MECHANICAL OR UTILITY EQUIPMENT OVER 2' IN HEIGHT SHALL BE ALLOWED IN ANY MINIMUM RESIDENTIAL FRONT YARD SETBACK AREA.
5. ALL EXISTING MECHANICAL EQUIPMENT SHALL BE SCREENED FROM STREET LEVEL VIEW ON FROM RESIDENTIALLY ZONED PROPERTIES.
6. ALL DISTURBED AREAS SHALL BE RESTORED BY SEEDING OR SOILING.
7. THIS SITE SHALL BE MAINTAINED IN COMPLIANCE WITH ALL CITY CODES APPLICABLE ON THE DATE OF SITE PLAN APPROVAL.
8. ANY OVERHEAD-OR MOUNTED UTILITIES AND METERS SHALL BE SCREENED AND NOT VISIBLE FROM A PUBLIC STREET.
9. ANY WALL-MOUNTED UTILITIES SHALL BE SCREENED ON ALL SIDES AND FINISHED TO MATCH BUILDING EXTERIOR.
10. ALL UTILITIES SHALL BE UNDERGROUND.
11. SIGNAGE SHALL BE MOUNTED OR DIRECTORY BUT IN NO CASE SHALL BE POLE SIGNAGE.

PAVEMENT THICKNESS

1. SIDEWALKS 4" P.C.C.
2. DRIVE Lanes 6" P.C.C.
3. PAVEMENT WITHIN RIGHT-OF-WAY 8" P.C.C.

SITE DEVELOPMENT INFORMATION

SITE DISTURBED AREA	5.82 AC
EXISTING IMPERVIOUS AREA	0.84 AC
TOTAL PROPOSED IMPERVIOUS AREA	5.94 AC

3405 S.E. CROSSROADS DRIVE, SUITE G
 GRIMES, IOWA 50111
 PHONE: (515) 389-4400 FAX: (515) 389-4410

DES MOINES, IOWA

CIVIL DESIGN ADVANTAGE

ENGINEER/RAH

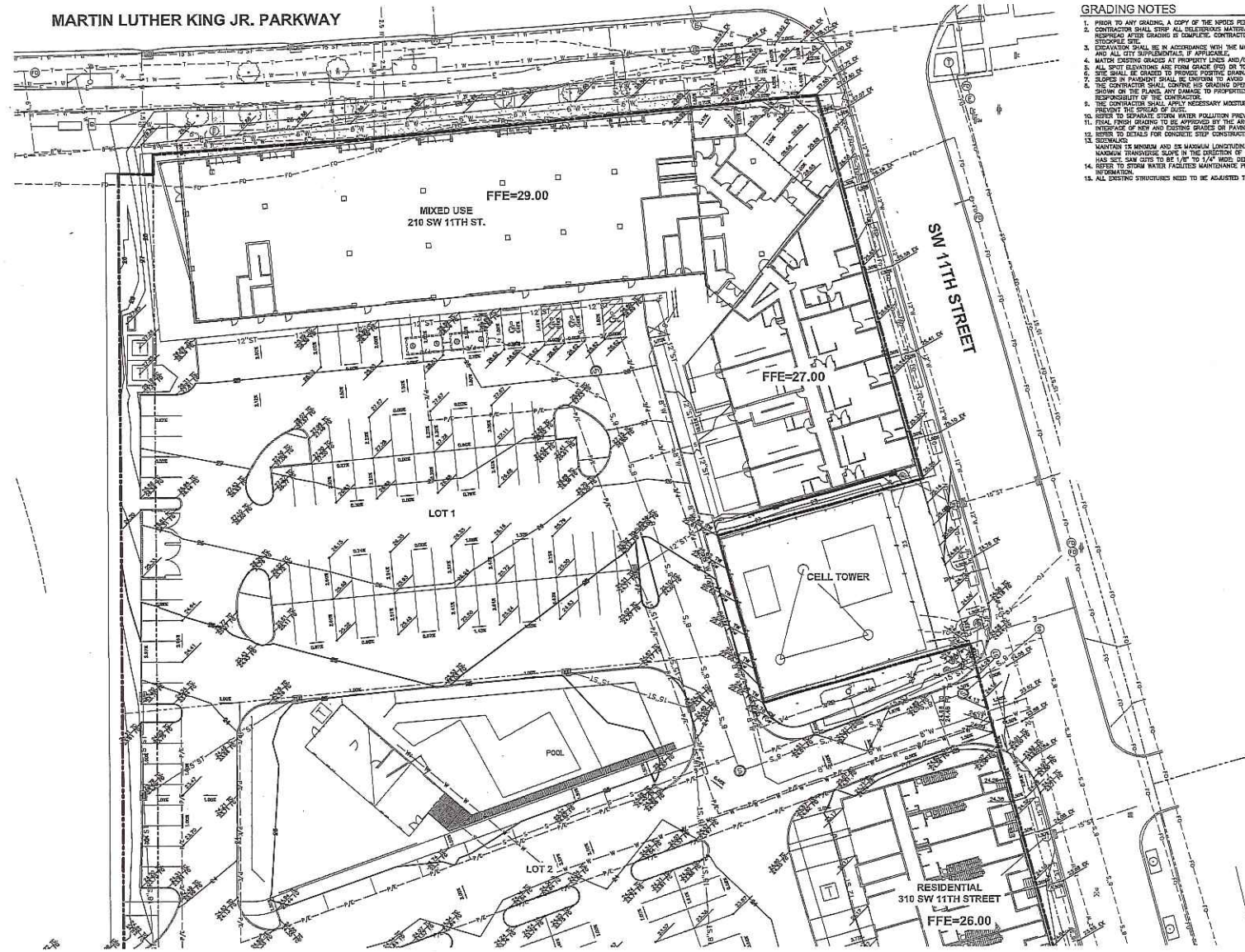
LINC

DIMENSION PLAN

C2.2
 1709.517

00

MARTIN LUTHER KING JR. PARKWAY



GRADING NOTES

1. PRIOR TO ANY GRADING, A COPY OF THE NPDES PERMIT SHALL BE PROVIDED TO THE CITY'S BUILDING DIVISION. CONTRACTOR SHALL STOP ALL EROSION CONTROL MATERIAL. THE TOP OF TOPSOIL IS TO BE STOCKPILED AND REPLENISHED AFTER GRADING IS COMPLETE. CONTRACTOR WILL BE RESPONSIBLE FOR PROVIDING A SUFFICIENT TOPSOIL STOCKPILE ON-SITE.
2. EROSION CONTROL SHALL BE IN ACCORDANCE WITH THE MOST RECENT EDITION OF THE SUDAS STANDARD SPECIFICATIONS AND ALL CITY SUPPLEMENTALS, IF APPLICABLE.
3. MATCH EXISTING GRADES AT PROPERTY LINES AND/OR CONSTRUCTION LIMITS.
4. ALL SPOT ELEVATIONS ARE FROM GRADE (GEO) OR TOP OF FINISHED SURFACES UNLESS OTHERWISE NOTED.
5. THE CONTRACTOR SHALL MAINTAIN SLOPES TO PREVENT EROSION DURING CONSTRUCTION. SLOPES IN PLACED SHALL BE SUFFICIENT TO AVOID FLOODING.
6. THE CONTRACTOR SHALL MAINTAIN SLOPES TO PREVENT EROSION DURING CONSTRUCTION. SLOPES IN PLACED SHALL BE SUFFICIENT TO AVOID FLOODING.
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10. THE CONTRACTOR SHALL MAINTAIN SLOPES TO PREVENT EROSION DURING CONSTRUCTION. SLOPES IN PLACED SHALL BE SUFFICIENT TO AVOID FLOODING.
11. FINAL FINISH GRADING TO BE APPROVED BY THE ARCHITECT AND CIVIL ENGINEER. MATCH EXISTING GRADES AT THE INTERSECTION OF NEW AND EXISTING GRADES OR PAVEMENT.
12. REFER TO DETAILS FOR CONCRETE STEP CONSTRUCTION.
13. SLOPES SHALL MAINTAIN 1% MINIMUM AND 2% MAXIMUM LONGITUDINAL SLOPES ON ALL PAVED WALKWAYS. ALL WALKS TO HAVE 2% MAXIMUM TRANSVERSE SLOPE IN THE DIRECTION OF NATURAL DRAINAGE. SAW CUT JOINTS AS SHOWN ON CONCRETE. HAS SET. SAW CUTS TO BE 1/8" TO 1/4" WIDE. SLOPE LONGITUDINAL 1/2% TRANSVERSE 1/4%.
14. REFER TO STORM WATER FACILITIES MAINTENANCE PLAN FOR DETENTION INSULATION BASIN CONSTRUCTION INFORMATION.
15. ALL EXISTING STRUCTURES NEED TO BE ADJUSTED TO MATCH NEW SIDEWALK GRADES.



	DATE	2015.04.27
	RECORD SUBMITTAL	2015.04.27
	REVISIONS	
	BY	
	3405 S.E. CROSSROADS DRIVE, SUITE G	
	GRIMES, IOWA 50111	
	PHONE: (515) 369-4400	FAX: (515) 369-4410
	ENGINEER/PAH	BYTC
	CIVIL DESIGN ADVANTAGE	
	DES MOINES, IOWA	
	LINC	
	GRADING PLAN	
	C3.1	
	1709.517	

24



LINC
GRADING PLAN



3406 S.E. CROSSROADS DRIVE, SUITE G
 GRIMES, IOWA 50111
 PHONE: (515) 369-4400 FAX: (515) 369-4410

REVISIONS	DATE

DES MOINES, IOWA

CIVIL DESIGN ADVANTAGE ENGINEER:RAH

E:TC

SECOND SUBMITTAL 2018-02-07
 FIRST SUBMITTAL 2018-01-09

MARTIN LUTHER KING JR. PARKWAY

210 SW 11TH ST.

MIXED USE

LOT 1

LOT 2

CELL TOWER

POOL

WATER STORAGE

SW 11TH STREET

RESIDENTIAL 310 SW 11TH STREET

[illegible]

THE PROPERTY OWNER IS RESPONSIBLE FOR COORDINATING STREET LIGHTING / UTILITY POLE REMOVAL/REPLACEMENT NECESSARY FOR MID-AMERICAN ENERGY. BLACK LED "AURORA" SERIES LIGHT FIXTURES WITH A BLACK POLE IS REQUIRED FOR ALL STREET LIGHTS ASSOCIATED WITH THE PROJECT AREA AND OVERSPANNING OF STREET LINES. THE PROPERTY OWNER IS RESPONSIBLE FOR THE REMOVAL OF EXISTING STREET LIGHTS AND THE INSTALLATION OF NEW LIGHTING WILL REQUIRE A PLAN FOR THE NEW LAYOUT AND AN EVALUATION OF THE LIGHTING LEVELS. IN ADDITION, A FIXTURE ILLUSTRATION SHALL BE PROVIDED ON THE SITE PLAN.

REVISIONS	DATE
SECOND SUBMITTAL	2016-02-07

3405 S.E. CROSSROADS DRIVE, SUITE G
GRIMES, IOWA 50111
PHONE: (515) 369-4400 FAX: (515) 369-4410

E1:TC

GINEER:RAH



CIVIL DESIGN ADVANTAGE

www.elsevier.com/locate/jmb

LINC
UTILITY PLAN

C4.1
1709.517

24.

1709.5.17
C4.2
1709.5.17



C4.2

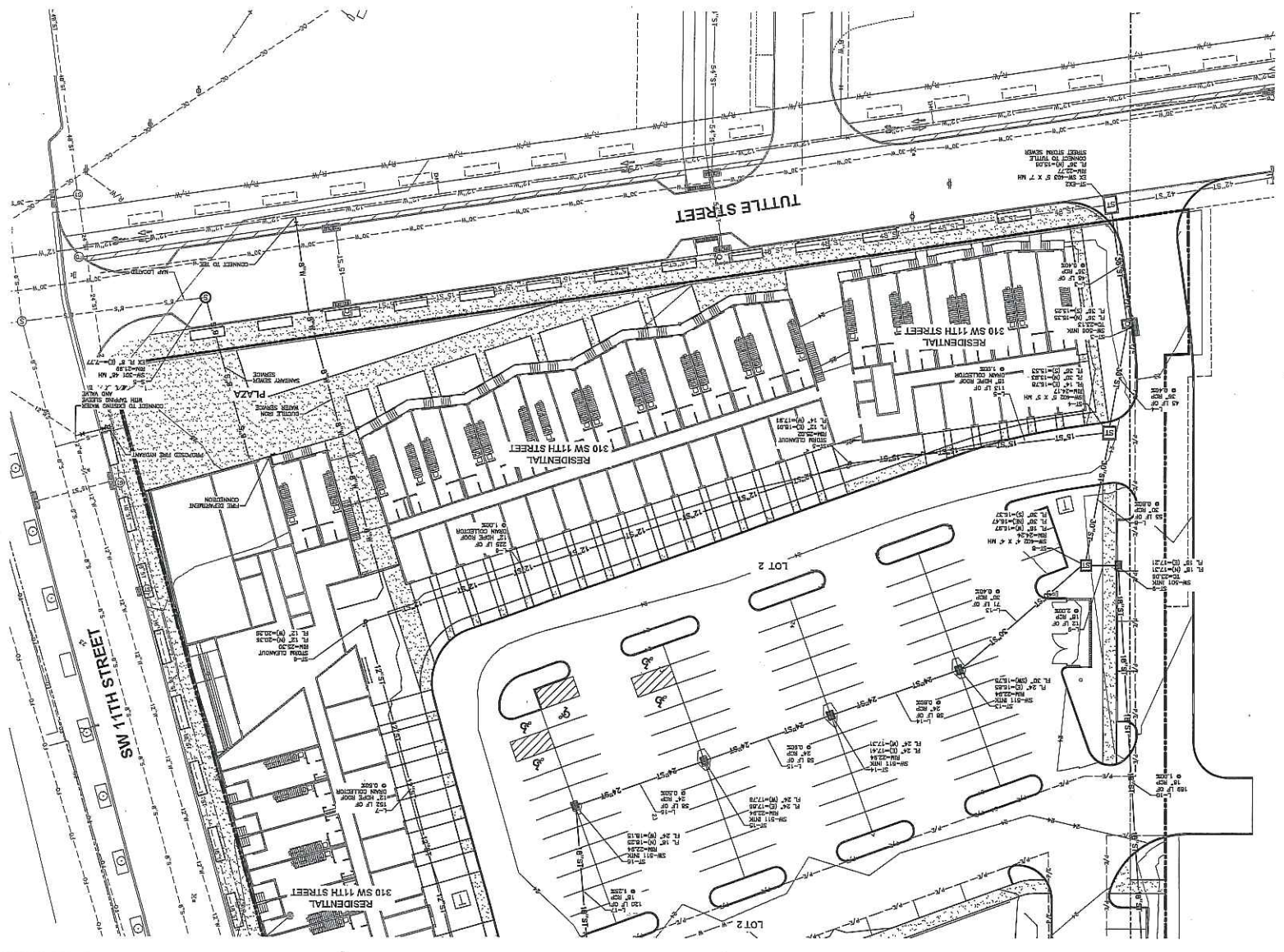
LINC UTILITY PLAN

DES. MONITOR, IOWA



3405 S.E. GROSSROADS DRIVE, SUITE G
PHONES: (515) 399-4400 FAX: (515) 399-4410

REVISIONS	DATE
RECORD SUBMITTAL	2012.02.27
FINAL SUBMITTAL	2012.03.29



MARTIN LUTHER KING JR. PARKWAY

MIXED USE
210 SW 11TH ST.

LOT 1

CELL TOWER

LOT 2

RESIDENTIAL
310 SW 11TH STREET

SW 11TH STREET

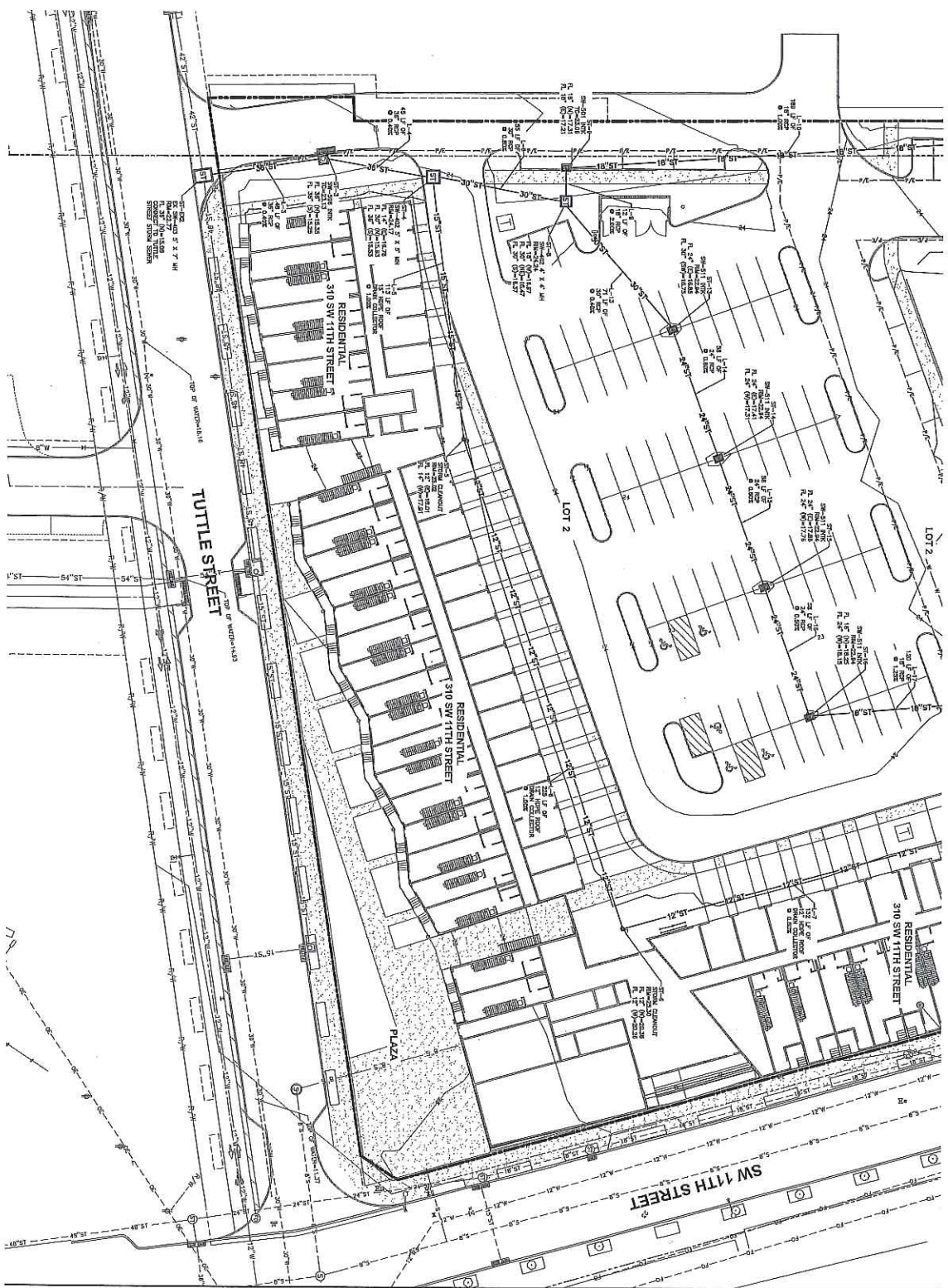
STORM WATER DETENTION

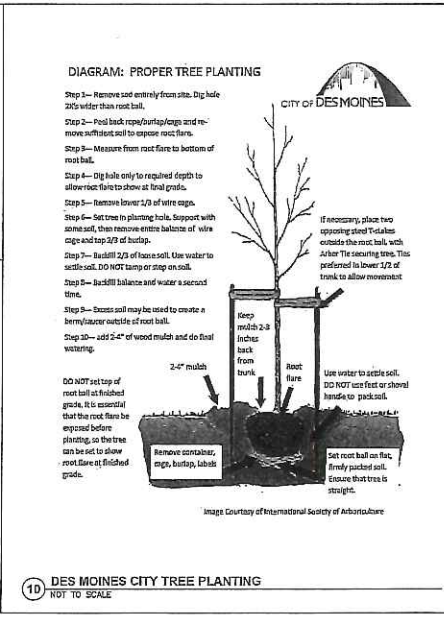
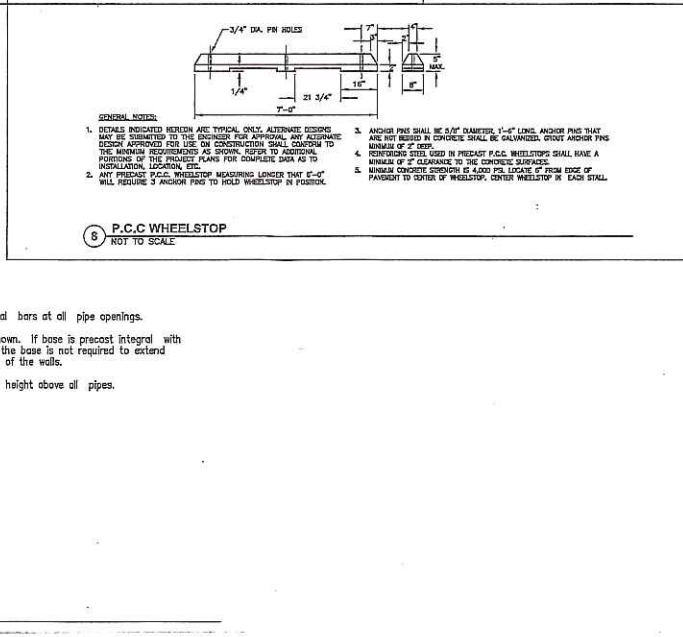
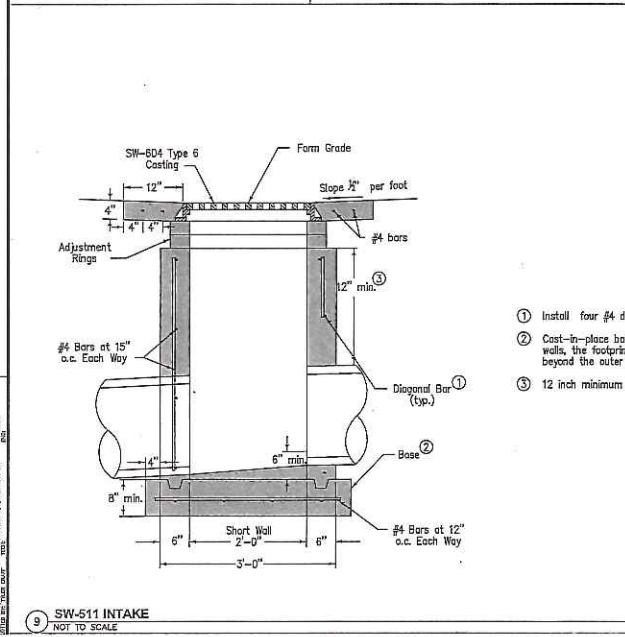
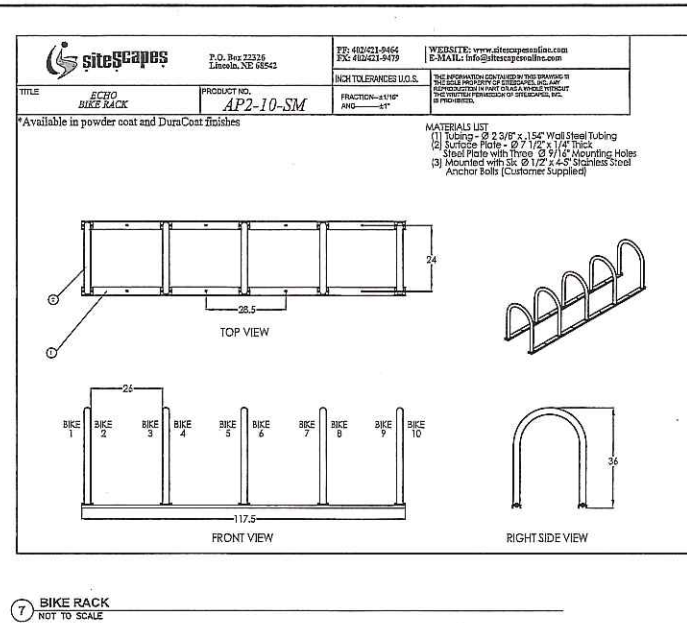
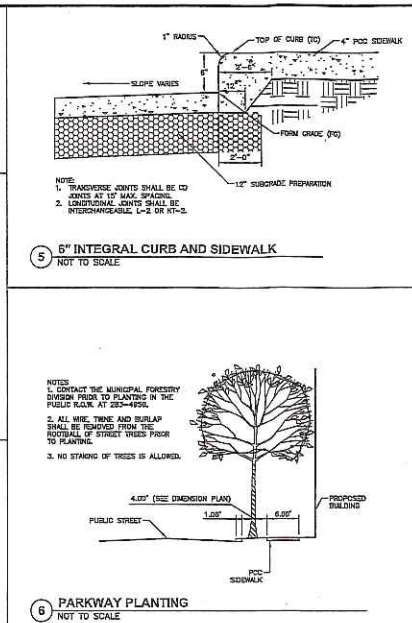
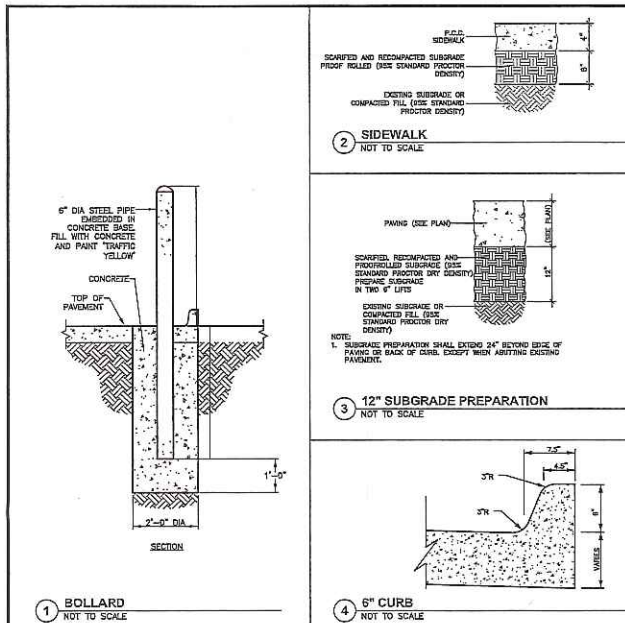
NO STORM WATER DETENTION OR TREATMENT OF THE 1.0" RAINFALL EVENT IS PROPOSED ON SITE. STORM WATER DETENTION AND TREATMENT OF THE 1.0" RAINFALL EVENT IS PROPOSED DOWNSIDE IN THE FUTURE GRAY'S STATION REGIONAL DETENTION BASIN.

CITY CODE SECTION 106-136(H) LAND OWNER/ MANAGER STORM WATER FACILITIES MAINTENANCE REQUIREMENTS

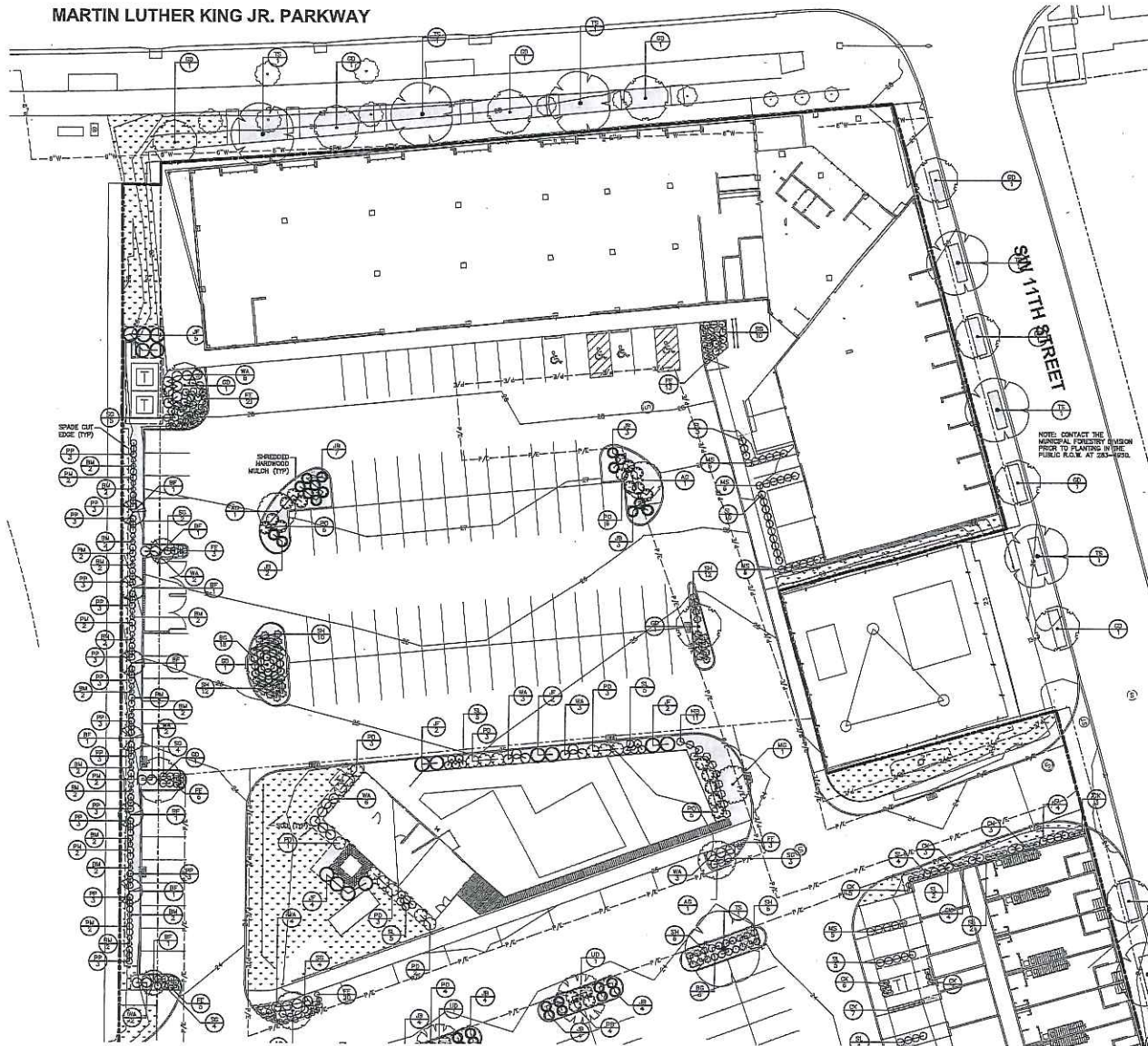
- THIS STORMWATER MANAGEMENT FACILITIES, INCLUDING PIPES, DUCTS AND OUTLETS, SHALL BE ANNUALLY INSPECTED AND MAINTAINED IN SUBSTANTIAL COMPLIANCE WITH THE APPLICABLE STORMWATER MANAGEMENT PLAN.
- LITTER, DEBRIS, SEDIMENT AND OTHERS SHALL BE PROMPTLY REMOVED FROM SUCH FACILITIES.
- THE PLANTS AND VEGETATION WITHIN THE STORMWATER MANAGEMENT FACILITIES SHALL BE MAINTAINED IN CONFORMANCE WITH THE APPROVED STORMWATER MANAGEMENT PLAN. IF ANY VEGETATION DIES, IT SHALL BE REPLACED WITH VEGETATION IN CONFORMANCE WITH THE PLAN AS SOON AS THE WEATHER PERMITS.
- NO GRASS CLIPPINGS, LEAVES OR OTHER YARD WASTES, SOIL, ROCKS, CONCRETE, OR SIMILAR MATERIALS SHALL BE PLACED WITHIN A SHALE OR DETENTION OR DETENTION POND UNLESS THE PRIOR WRITTEN CONSENT OF THE CITY ENGINEER.
- NO PORTION OF THE STORMWATER MANAGEMENT FACILITIES SHALL BE ALLOWED OR REMOVED WITHOUT THE PRIOR WRITTEN CONSENT OF THE CITY ENGINEER.
- RECORDS OF INSPECTION, MAINTENANCE AND REPAIR MUST BE MAINTAINED AND KEPT FOR AT LEAST FIVE YEARS AND MADE AVAILABLE UPON REQUEST TO THE CITY ENGINEER.

	DATE	2018.02.07
	REVISIONS	
	SECOND SUBMITTAL	
	FIRST SUBMITTAL	
LINC STORM WATER FACILITIES MAINTENANCE PLAN <small>DES MOINES, IOWA</small>	3405 S.E. CROSSROADS DRIVE, SUITE G GRIMES, IOWA 50111 PHONE: (515) 399-4400 FAX: (515) 399-4410 CIVIL DESIGN ADVANTAGE ENGINEERING	EITC EITC
	C4.3 1709.517	1709.517





MARTIN LUTHER KING JR. PARKWAY



GENERAL NOTES

1. LOCATE ALL UTILITIES BEFORE ANY PLANTING BEGINS.
2. THE MOST RECENT EDITION OF THE SUGAR STANDARD SPECIFICATIONS AND ALL CITY SUPPLEMENTALS, IF APPLICABLE, SHALL APPLY TO ALL WORK ON THIS PROJECT UNLESS OTHERWISE NOTED.
3. TYPE, SIZE, AND QUALITY OF PLANT MATERIAL SHALL CONFORM TO THE MOST CURRENT EDITION OF THE AMERICAN STANDARDS FOR NURSERY STOCK AND ZONES.
4. ALL PLANT MATERIAL SHALL BE HEALTHY SPECIMENS WITHOUT DEFORMITIES, WOUNDS AND OPEN SPACES, WITH WELL DEVELOPED BRANCH AND ROOT SYSTEMS, TRUNK TO HEIGHT, SHAPE AND CHARACTER OF GROWTH OF THE SPECIES OR VARIETY.
5. ALL DISTURBED AREAS AS DIRECTED BY OWNER.
6. WEED PREVENTER (PRE-SUBURBENT) SHALL BE SPREAD OVER SOIL AFTER PLANTING AND BEFORE MULCHING IN ALL PLANTING AREAS PER MANUFACTURER'S RECOMMENDATIONS.
7. SHREDDER HARDWOOD MULCH SHALL BE PLACED AROUND ALL TREES AND SHRUBS TO A (MIN) DEPTH OF 3".
8. SHREDDER HARDWOOD MULCH WITH WEED PREVENTER FIBER SHALL BE PLACED IN ALL PLANTING BEDS TO A (MIN) DEPTH OF 3".
9. ALL EDGES SHALL BE 4" GRADE CUT EDGE.
10. PLANT QUANTITIES ARE SHOWN FOR INFORMATION ONLY. THE DRAWING SHALL PREVAIL IF ANY CONFLICTS ARISE.
11. ALL TREES SPALLED IN THE FIELD R.O.W. SHALL BE PICKED UP BY THE CONTRACTOR AT THE END OF EACH WORK DAY.
12. CONTRACTOR SHALL WARRANTY ALL PLANT MATERIALS FOR A PERIOD OF ONE YEAR FROM DATE OF INSTALLATION.
13. CONTACT THE ADJUTANT GENERAL PRIOR TO PLANTING IN THE PUBLIC R.O.W. AT 240-4410.
14. ALL WEED TRIM AND BURLAP SHALL BE REMOVED FROM THE ROOTBALL OF STREET TREES PRIOR TO PLANTING.
15. NO STAKING OF TREES IS ALLOWED.

PLANT SCHEDULE

PLANT SCHEDULE	QTY	COMMON NAME	BOTANICAL NAME	COMMENTS
DECEASED TREES				
12	12	Populus nigra	Ash	12 CAL
14	14	Dubautia pinnatifida	White Birch	14 CAL
16	16	Salix purpurea	Willow	16 CAL
18	18	Salix purpurea	Willow	18 CAL
20	20	Salix purpurea	Willow	20 CAL
22	22	Salix purpurea	Willow	22 CAL
24	24	Salix purpurea	Willow	24 CAL
26	26	Salix purpurea	Willow	26 CAL
28	28	Salix purpurea	Willow	28 CAL
30	30	Salix purpurea	Willow	30 CAL
32	32	Salix purpurea	Willow	32 CAL
34	34	Salix purpurea	Willow	34 CAL
36	36	Salix purpurea	Willow	36 CAL
38	38	Salix purpurea	Willow	38 CAL
40	40	Salix purpurea	Willow	40 CAL
42	42	Salix purpurea	Willow	42 CAL
44	44	Salix purpurea	Willow	44 CAL
46	46	Salix purpurea	Willow	46 CAL
48	48	Salix purpurea	Willow	48 CAL
50	50	Salix purpurea	Willow	50 CAL
52	52	Salix purpurea	Willow	52 CAL
54	54	Salix purpurea	Willow	54 CAL
56	56	Salix purpurea	Willow	56 CAL
58	58	Salix purpurea	Willow	58 CAL
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62	62	Salix purpurea	Willow	62 CAL
64	64	Salix purpurea	Willow	64 CAL
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196	196	Salix purpurea	Willow	196 CAL
198	198	Salix purpurea	Willow	198 CAL
200	200	Salix purpurea	Willow	200 CAL

INTERIOR PARKING CALCULATIONS

PARKING SPACES PROVIDED: 127 SPACES

PLANTING BEDS (1/30 SPACES)	8 PLANTING BEDS
TREES (CONSIDERED 1/3 PLANTING BED)	8 TREES
SHRUBS (1/30 PLANTING BED)	24 SHRUBS
PLANTING BEDS	12 PLANTING BEDS
TREES (CONSIDERED 1/3 PLANTING BED)	12 TREES
SHRUBS (1/30 PLANTING BED)	24 SHRUBS

PERIMETER PARKING CALCULATIONS

WEST PARKING PERIMETER: 240 LF

TREES (1/40 LF)	6 TREES
SHRUBS (1/40 LF)	60 SHRUBS
TREES	6 TREES
SHRUBS	60 SHRUBS

PARKWAY PLANTINGS CALCULATIONS

MARTIN LUTHER KING JR. PARKWAY: 242 LF
SOUTHWEST 11TH STREET: 400 LF
NUTTLE STREET: 400 LF

PLANTING BEDS (1/30 LF)	8 TREES
TREES (CONSIDERED 1/30 LF)	8 TREES
SHRUBS (1/30 LF)	24 SHRUBS
PLANTING BEDS	12 PLANTING BEDS
TREES (CONSIDERED 1/30 LF)	12 TREES
SHRUBS (1/30 LF)	24 SHRUBS

DATE	10/02/07
REVISIONS	
RECORD NUMBER	1709-517
RECORD DATE	10/02/07

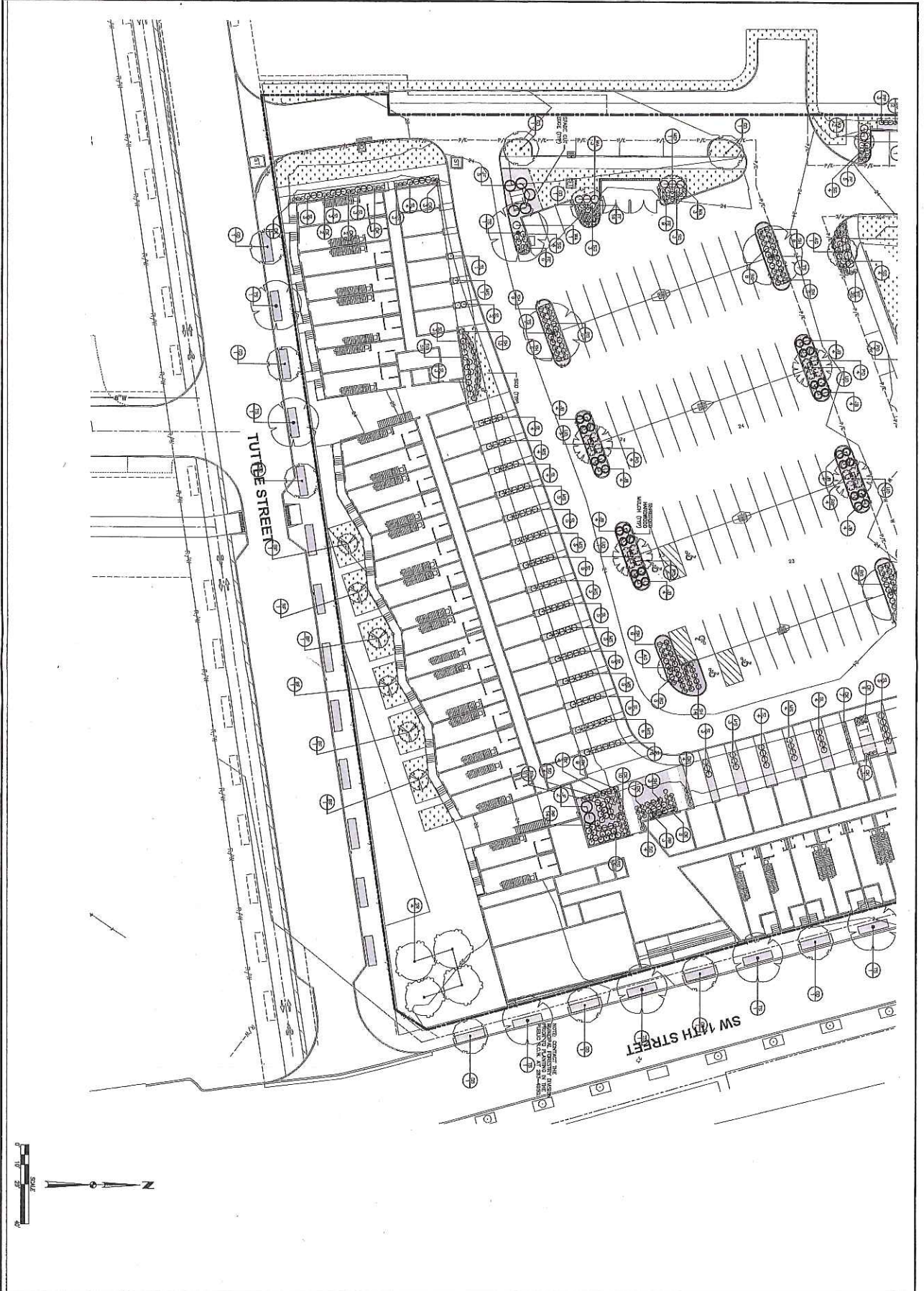
3405 S.E. CROSSROADS DRIVE, SUITE G
PHONE: (516) 365-4400 FAX: (516) 369-4410
EITC
DESIGN ADVANTAGE ENGINEERING



DESIGN ADVANTAGE ENGINEERING
DESIGN ADVANTAGE ENGINEERING

LINC
LANDSCAPE PLAN

L1.1
1709-517

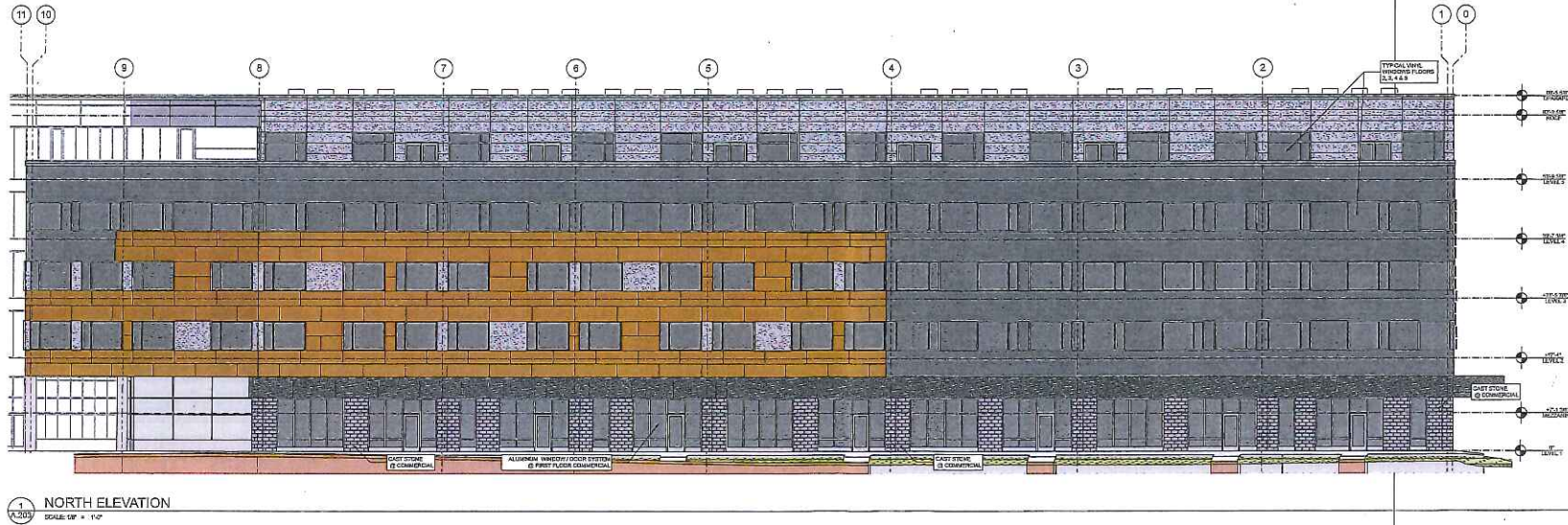
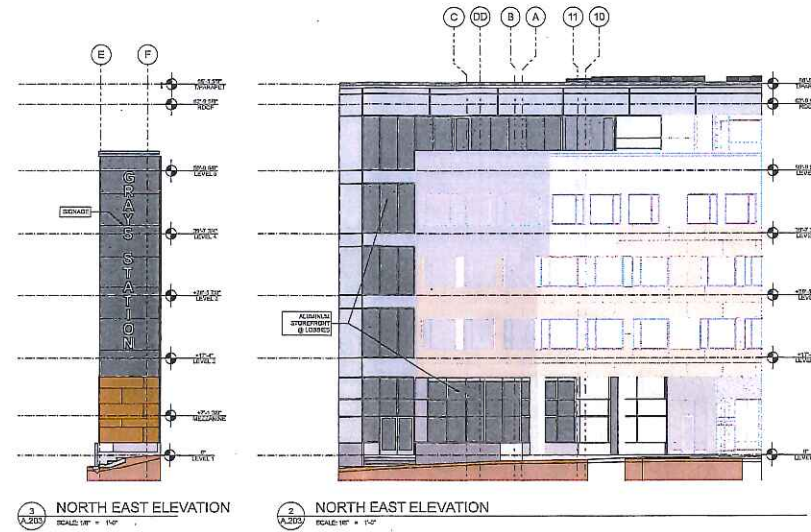


32

EXTERIOR MATERIAL LEGEND

- HORIZONTAL METAL PANEL 1
- HORIZONTAL METAL PANEL 2
- METAL PANEL
- CEMENT BOARD 1
- CEMENT BOARD 2
- CEMENT BOARD 3
- CEMENT BOARD 4
- RED BRICK
- CEMENT BOARD 5

GNITSHOT
ARCHITECTURE
1100 MLK, DES MOINES, IA
T. 515.283.1111



LINC PHASE I @ GRAYS STATION

SW INTERSECTION OF
1100 MLK, DES MOINES, IA

PRELIMINARY - NOT FOR CONSTRUCTION

2017.08
100% DESIGN
DEVELOPMENT

BUILDING
ELEVATIONS

A.203



5 SOUTH ELEVATION 2
SCALE 1/8" = 1'-0"

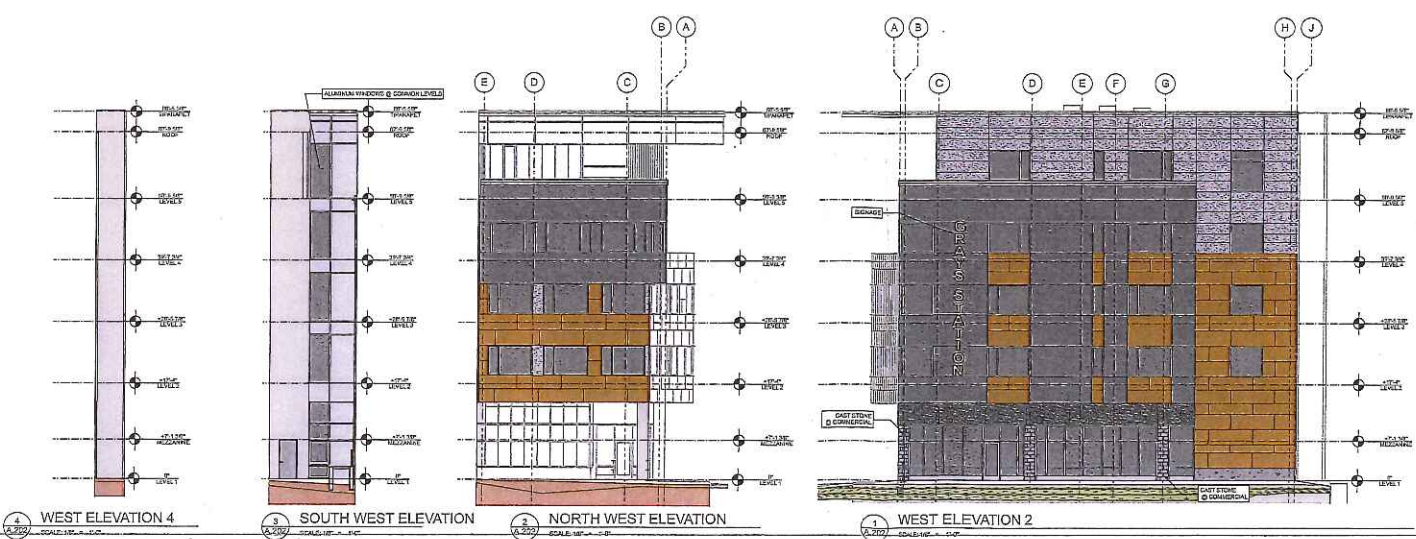
EXTERIOR MATERIAL LEGEND

	HORIZONTAL METAL PANEL 1
	HORIZONTAL METAL PANEL 2
	METAL PANEL
	CEMENT BOARD 1
	CEMENT BOARD 2
	CEMENT BOARD 3
	RESIN PANEL
	CEMENT BOARD 4

GNITSHOT
ARCHITECTURE
P.L.L.C.
1111 MARKET STREET, SUITE 200
DES MOINES, IA 50319
515.281.1111

LINC PHASE I @ GRAYS STATION
SW INTERSECTION OF
TWIN LANE, DES MOINES, IA

PRELIMINARY - NOT FOR CONSTRUCTION
DATE: 08/20/2018
100% DESIGN
EXTERIOR ELEVATIONS
BUILDING
ELEVATIONS
A.202



4 WEST ELEVATION 4
SCALE 1/8" = 1'-0"

3 SOUTH WEST ELEVATION
SCALE 1/8" = 1'-0"

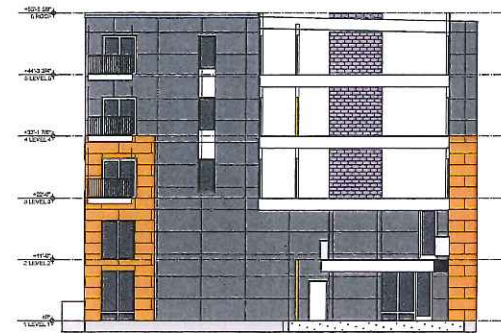
2 NORTH WEST ELEVATION
SCALE 1/8" = 1'-0"

1 WEST ELEVATION 2
SCALE 1/8" = 1'-0"

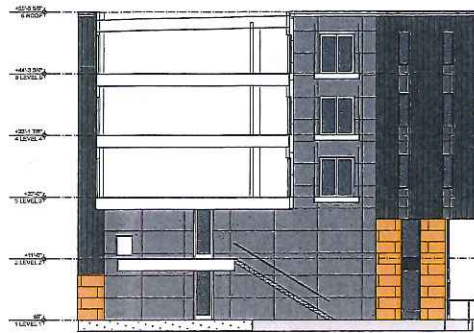
22



4 SOUTH WEST ELEVATION
SCALE: 1/8" = 1'-0"



3 WEST BRIDGE - WEST ELEVATION
SCALE: 1/8" = 1'-0"



2 WEST BRIDGE - EAST ELEVATION
SCALE: 1/8" = 1'-0"



1 SOUTH MIDDLE ELEVATION
SCALE: 1/8" = 1'-0"

EXTERIOR MATERIAL LEGEND

	VERTICAL METAL PANEL
	HORIZONTAL METAL PANEL
	CEMENT BOARD 1
	CEMENT BOARD 2
	CEMENT BOARD 3
	BRICK PANEL

GNITSHOT
ARCHITECTURE
P.O. BOX 100000, NEW YORK, NY 10001

LINC PHASE II @ GRAYS STATION

2017-13
PROJECT NAME
DATE OF ISSUE

REVISIONS
DATE
DESCRIPTION
BY

BUILDING
ELEVATIONS

A.203

32

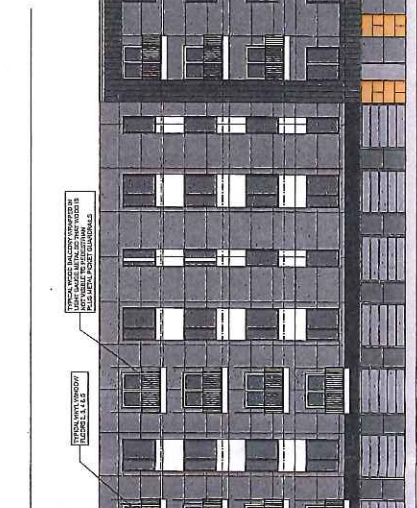
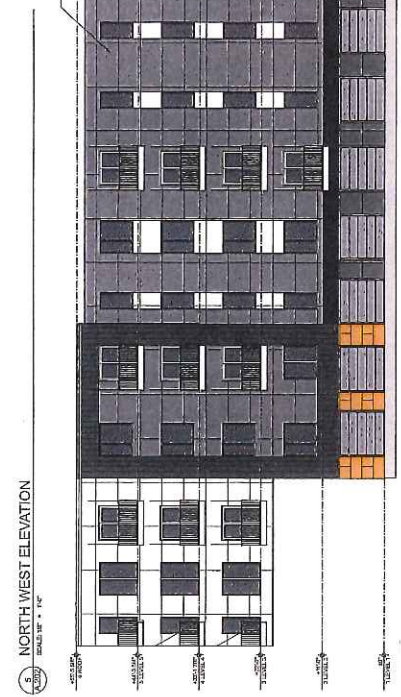
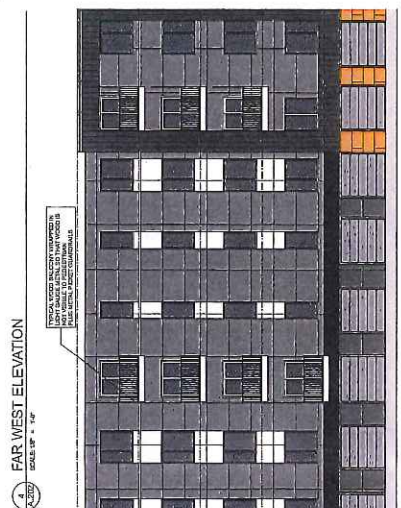
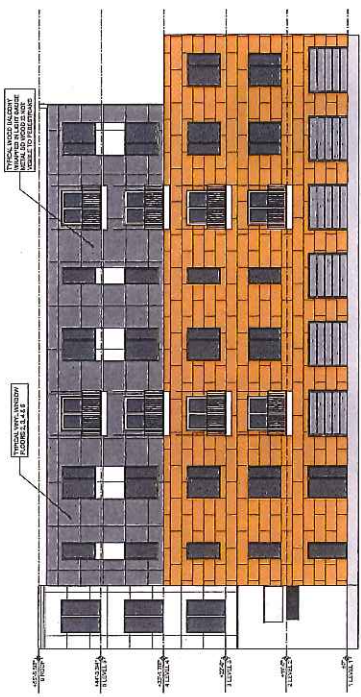
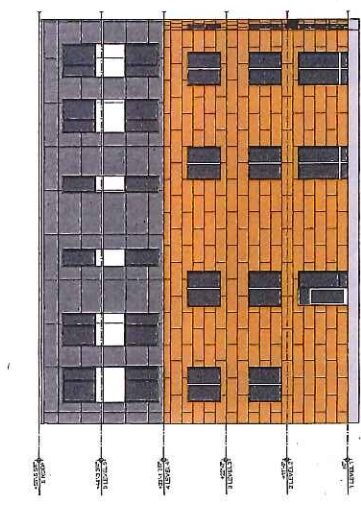
PROJECT: LINCOLN PHASE II @ GRAYS STATION
PROJECT LOCATION: 11111 111TH AVE, S.E.
PROJECT OWNER: LINCOLN PHASE II @ GRAYS STATION
DATE: 11/11/2021

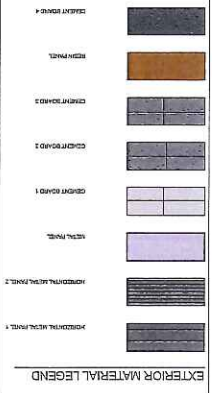
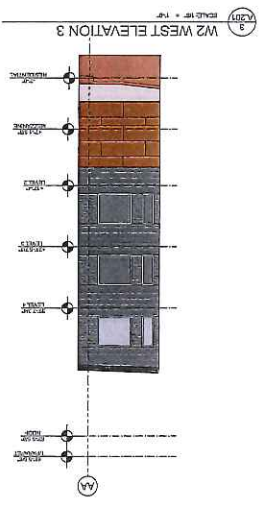
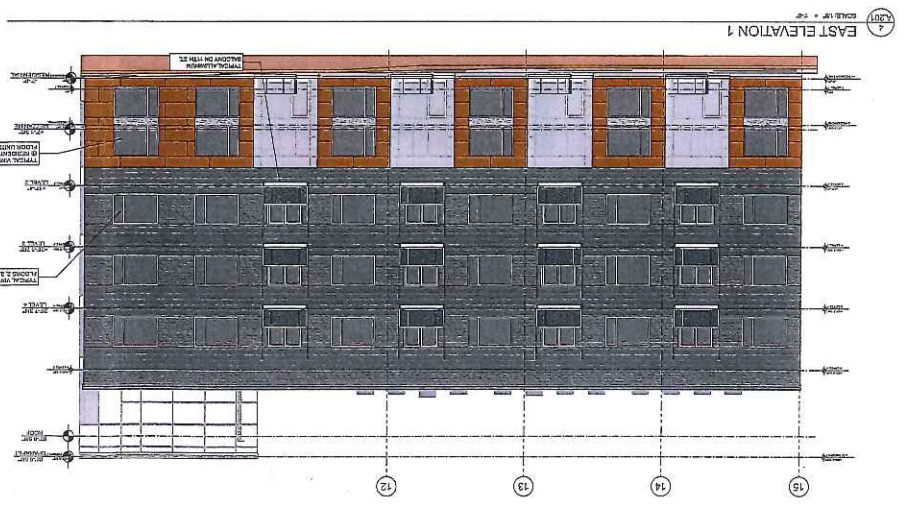
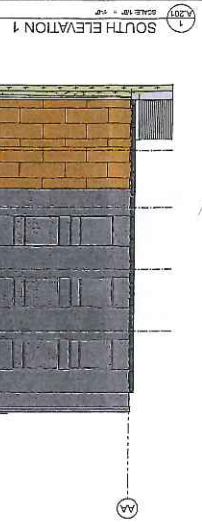
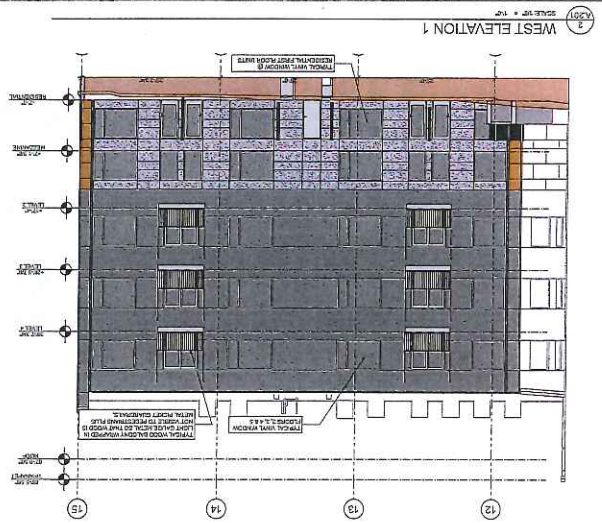
LINCOLN PHASE II @ GRAYS STATION

GNITSHOT ARCHITECTURE
3000 111TH AVE, S.E.
11111 111TH AVE, S.E.

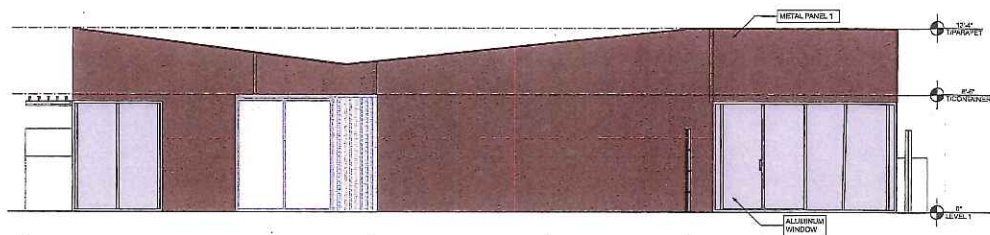
EXTERIOR MATERIAL LEGEND

VERTICAL CLAY TILE	CLAY TILE	CLAY TILE	CLAY TILE	CLAY TILE	CLAY TILE
CLAY TILE	CLAY TILE	CLAY TILE	CLAY TILE	CLAY TILE	CLAY TILE
CLAY TILE	CLAY TILE	CLAY TILE	CLAY TILE	CLAY TILE	CLAY TILE
CLAY TILE	CLAY TILE	CLAY TILE	CLAY TILE	CLAY TILE	CLAY TILE
CLAY TILE	CLAY TILE	CLAY TILE	CLAY TILE	CLAY TILE	CLAY TILE
CLAY TILE	CLAY TILE	CLAY TILE	CLAY TILE	CLAY TILE	CLAY TILE

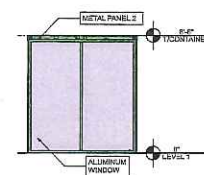




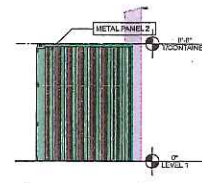
GN1SHOT
ARCHITECTURE
JASON CORREIA-DESIGN (MAY)
© 2016



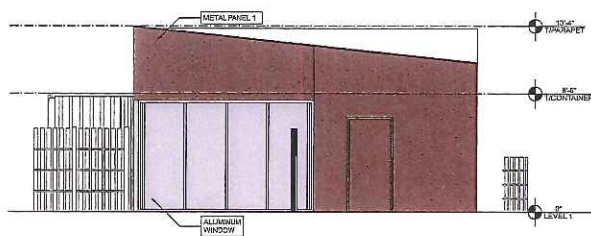
6 NORTH EAST ELEVATION
SCALE: 1/4" = 1'-0"



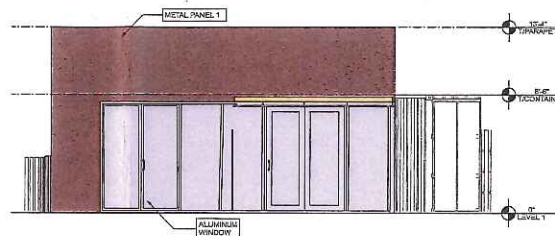
5 EAST ELEVATION
SCALE: 1/4" = 1'-0"



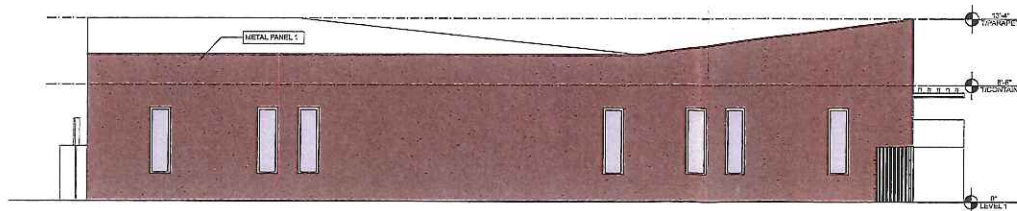
4 NORTH ELEVATION
SCALE: 1/4" = 1'-0"



3 NORTH WEST ELEVATION
SCALE: 1/4" = 1'-0"



2 SOUTH EAST ELEVATION
SCALE: 1/4" = 1'-0"



1 SOUTH WEST ELEVATION
SCALE: 1/4" = 1'-0"

EXTERIOR MATERIAL LEGEND



GNITSHOT
ARCHITECTURE
303 SOUTH COUNTY ROAD 100, SUITE 100, DALLAS, TX 75242

GRAY'S STATION

ADDRESS

*** PROJECT NUMBER ***

*** PROJECT STATUS ***

*** DATE OF ISSUE ***

REVISIONS

NO. DESCRIPTION DATE

BUILDING
ELEVATIONS

A.201

22