



**Date** March 6, 2023

**RESOLUTION HOLDING HEARING ON REQUEST FROM HUBBELL REALTY COMPANY (OWNER), REPRESENTED BY CALEB SMITH (OFFICER), FOR REVIEW AND APPROVAL OF PUD FINAL DEVELOPMENT PLAN FOR “GRAY’S STATION TELUS CONDOS,” FOR PROPERTY LOCATED IN THE VICINITY OF 1300 TUTTLE STREET**

**WHEREAS**, on February 20, 2023, by Roll Call No. 23-0225, the City Council received a communication from the City Plan and Zoning Commission advising that the Commission voted 8-1-1 to **APPROVE** a request from Hubbell Realty Company (Owner), represented by Caleb Smith (Officer), to approve the PUD Final Development Plan for “Gray’s Station Telus Condos” for property located in the vicinity of 1300 Tuttle Street to allow development of the property with the construction of two condo buildings containing 84 residential units, subject to compliance with all administrative review requirements and the following:

1. Provision of a “band” of architectural concrete masonry unit materials from grade up to 2’ for the standalone garages that matches the concrete masonry units proposed for the condo buildings.
2. Provision of a sidewalk connection from the southeastern corner of “Building 2” that crosses the internal vehicular drive to link up with the north-south pedestrian pathway that bisects this development area.
3. Provision of more-detailed exterior façade materiality information that matches the standards outlined within the PUD Conceptual Plan.
4. Verification that all exterior façade glazing contains a minimum 50% transmittance factor and a reflectance factor of no more than .25; and

**WHEREAS**, the Property is legally described as follows:

OUTLOTS Y AND Z AND A PART OF LOTS 6, 7 AND 9, CENTRAL DES MOINES INDUSTRIAL PARK, AN OFFICIAL PLAT AND PART OF LOTS 80, 81, 82, 85 AND 86 IN FACTORY ADDITION, AN OFFICIAL PLAT, ALL BEING IN THE CITY OF DES MOINES, POLK COUNTY, IOWA AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF SAID LOT 9, CENTRAL DES MOINES INDUSTRIAL PARK HAVING A STATE PLANE IOWA SOUTH COORDINATE VALUE OF NORTHING 575161.83 AND EASTING 1602813.57; THENCE SOUTH 89° (DEGREES) 48' (MINUTES) 50" (SECONDS) EAST ALONG THE NORTHERLY LINE OF SAID LOT 9, A DISTANCE OF 339.93 FEET TO THE SOUTHWEST CORNER OF LOT 2, SAID CENTRAL DES MOINES INDUSTRIAL



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PARK; THENCE SOUTH 89°46'52" EAST CONTINUING ALONG SAID NORTHERLY LINE, 419.55 FEET TO THE SOUTHEAST CORNER OF SAID LOT 2 ALSO BEING A POINT ON THE WEST RIGHT OF WAY LINE OF SW 14TH STREET; THENCE SOUTH 00°01'02" EAST CONTINUING ALONG SAID NORTHERLY LINE, 69.98 FEET TO THE SOUTHWEST CORNER OF STREET LOT B, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE SOUTH 89°35'21" EAST CONTINUING ALONG SAID NORTHERLY LINE, 70.00 FEET TO THE NORTHWEST CORNER OF LOT 8, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE SOUTH 00°08'06" WEST CONTINUING ALONG SAID NORTHERLY LINE, 423.70 FEET TO THE SOUTHWEST CORNER OF SAID LOT 8; THENCE SOUTH 89°19'18" EAST CONTINUING ALONG SAID NORTHERLY LINE, 307.20 FEET TO THE SOUTHEAST CORNER OF SAID LOT 8, ALSO BEING THE NORTHEAST CORNER OF SAID LOT 9; THENCE NORTH 00°08'11" WEST ALONG THE WESTERLY LINE OF SAID LOT 7, CENTRAL DES MOINES INDUSTRIAL PARK, 333.39 FEET; THENCE NORTHERLY CONTINUING ALONG SAID WESTERLY LINE AND A CURVE CONCAVE WESTERLY WHOSE RADIUS IS 804.20 FEET, WHOSE ARC LENGTH IS 93.82 FEET AND WHOSE CHORD BEARS NORTH 03°29'07" WEST, 93.77 FEET; THENCE NORTH 06°58'06" WEST CONTINUING ALONG SAID WESTERLY LINE, 15.78 FEET TO THE NORTHWEST CORNER OF SAID LOT 7 ALSO BEING THE SOUTH RIGHT OF WAY LINE OF TUTTLE STREET; THENCE NORTH 81°10'49" EAST ALONG THE NORTH LINE OF SAID LOT 7, A DISTANCE OF 411.78 FEET TO THE NORTHEAST CORNER OF SAID LOT 7; THENCE NORTH 00°05'48" WEST ALONG THE WEST LINE OF SAID LOT 6, CENTRAL DES MOINES INDUSTRIAL PARK, 72.11 FEET TO THE NORTHWEST CORNER OF SAID LOT 6; THENCE NORTH 81°52'47" EAST ALONG THE NORTH LINE OF SAID LOT 6, A DISTANCE OF 347.62 FEET TO THE SOUTHEAST CORNER OF LOT 4, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE NORTH 00°31'45" WEST ALONG THE WESTERLY LINE OF SAID OUTLOT Y, CENTRAL DES MOINES INDUSTRIAL PARK, 63.43 FEET; THENCE NORTH 87°29'10" EAST CONTINUING ALONG SAID WESTERLY LINE, 10.37 FEET; THENCE NORTH 00°12'49" WEST CONTINUING ALONG SAID WESTERLY LINE, 554.99 FEET TO THE NORTHEAST CORNER OF LOT 5, SAID CENTRAL DES MOINES INDUSTRIAL PARK AND HAVING A STATE PLANE IOWA SOUTH COORDINATE VALUE OF NORTHING 575907.16 AND EASTING 1604699.48; THENCE SOUTH 89°04'27" EAST ALONG THE NORTH LINE OF SAID OUTLOT Y, 15.36 FEET TO THE NORTHEAST CORNER OF SAID OUTLOT Y; THENCE NORTH 00°21'12" WEST ALONG THE WEST LINE OF SAID LOT 85, FACTORY ADDITION, 8.59 FEET TO THE SOUTHERLY RIGHT OF WAY LINE OF MARTIN LUTHER KING JR PARKWAY; THENCE NORTH 86°10'40" EAST ALONG SAID SOUTHERLY RIGHT OF WAY LINE, 100.23 FEET; THENCE NORTH 84°40'04" EAST CONTINUING ALONG SAID SOUTHERLY RIGHT OF WAY LINE, 199.44 FEET TO THE WESTERLY RIGHT OF WAY LINE OF SW 11TH STREET; THENCE SOUTH 15°27'22" EAST ALONG SAID WESTERLY RIGHT OF WAY LINE,



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173.21 FEET; THENCE SOUTH 74°42'29" WEST, 99.77 FEET; THENCE SOUTH 15°25'34" EAST, 75.29 FEET; THENCE NORTH 74°32'27" EAST, 99.81 FEET TO SAID WESTERLY RIGHT OF WAY LINE; THENCE SOUTH 15°27'22" EAST ALONG SAID WESTERLY RIGHT OF WAY LINE, 344.37 FEET TO A CORNER ON THE EASTERLY LINE OF SAID LOT 81; THENCE SOUTH 39°15'32" WEST ALONG SAID EASTERLY LINE, 97.08 FEET; THENCE SOUTH 79°27'19" WEST, 73.56 FEET TO THE EASTERLY LINE OF THE WEST HALF OF SAID LOT 81; THENCE SOUTHERLY ALONG SAID EASTERLY LINE AND A CURVE CONCAVE EASTERLY WHOSE RADIUS IS 716.25 FEET, WHOSE ARC LENGTH IS 712.02 FEET AND WHOSE CHORD BEARS SOUTH 06°02'51" WEST, 683.06 FEET; THENCE SOUTH 67°48'12" WEST, 51.41 FEET TO A POINT ON THE EAST LINE OF THE WEST 100 FEET OF SAID LOT 80, FACTORY ADDITION; THENCE SOUTH 00°01'40" EAST ALONG SAID EAST LINE, 767.72 FEET; THENCE SOUTH 45°36'19" EAST, 692.69 FEET TO THE WESTERLY LINE OF SAID LOT 81; THENCE SOUTH 76°09'29" EAST, 50.00 FEET TO SAID EASTERLY LINE OF THE WEST HALF OF LOT 81, HAVING A STATE PLANE IOWA SOUTH COORDINATE VALUE OF NORTHING 573317.97 AND EASTING 1605461.84; THENCE SOUTHERLY ALONG SAID EASTERLY LINE AND A CURVE CONCAVE WESTERLY WHOSE RADIUS IS 740.65 FEET, WHOSE ARC LENGTH IS 76.04 FEET AND WHOSE CHORD BEARS SOUTH 17°00'55" WEST, 76.01 FEET; THENCE NORTH 70°20'25" WEST, 50.00 FEET TO SAID WESTERLY LINE OF LOT 81; THENCE NORTH 45°36'19" WEST, 663.55 FEET TO SAID EAST LINE OF THE WEST 100 FEET OF LOT 80; THENCE SOUTH 00°01'40" EAST ALONG SAID EAST LINE, 439.03 FEET TO THE NORTH BANK AND MEANDER LINE OF THE RACCOON RIVER; THENCE NORTH 71°44'57" WEST AND ALONG SAID MEANDER LINE AND THE SOUTHERLY LINE OF SAID OUTLOT Z, CENTRAL DES MOINES INDUSTRIAL PARK, 888.50 FEET; THENCE NORTH 83°01'49" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 463.82 FEET; THENCE NORTH 89°10'53" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 291.24 FEET; THENCE SOUTH 82°06'06" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 296.82 FEET; THENCE SOUTH 72°02'02" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 228.32 FEET TO THE SOUTHWEST CORNER OF SAID OUTLOT Z; THENCE NORTH 00°03'17" EAST ALONG THE WEST LINE OF SAID CENTRAL DES MOINES INDUSTRIAL PARK, 1647.05 FEET TO THE POINT OF BEGINNING AND CONTAINING 83.73 ACRES (3,647,186 SQUARE FEET) AND IS SUBJECT TO ANY AND ALL EASEMENTS OF RECORD.

**WHEREAS**, on February 6, 2023, by Roll Call No. 23-0225, it was duly resolved by the City Council that the request for approval of the PUD Final Development Plan be set down for hearing on March 6, 2023, at 5:00 p.m. at City Hall Council Chambers, 400 Robert D. Ray Dr., Des Moines, Iowa 50309; and



**Date** March 6, 2023

**WHEREAS**, due notice of said hearing was published in the Des Moines Register, as provided by law, setting forth the time and place for hearing on said proposed PUD Final Development Plan; and

**WHEREAS**, in accordance with said notice, those interested in said proposed PUD Final Development Plan, both for and against, have been given opportunity to be heard with respect thereto and have presented their views to the City Council.

**NOW, THEREFORE, BE IT RESOLVED**, by the City Council of the City of Des Moines, Iowa, as follows:

1. Upon due consideration of the facts, and any and all statements of interested persons and arguments of counsel, any objections to the proposed “Gray’s Station Telus Condos” PUD Final Development Plan are hereby overruled, and the hearing is closed.
2. The proposed Amendment to the “Gray’s Station Telus Condos” PUD Final Plan, as on file in the Development Services Department, is hereby found to be in conformance with PlanDSM: Creating Our Tomorrow Plan and is hereby approved subject to compliance with all administrative review comments as well as those requirements stated above.

Moved by \_\_\_\_\_ to adopt. Second by \_\_\_\_\_

APPROVED AS TO FORM:

/s/ Chas M. Cahill  
Chas M. Cahill  
Assistant City Attorney

(SITE-2022-000152)

COUNCIL ACTION	YEAS	NAYS	PASS	ABSENT
COWNIE				
BOESEN				
GATTO				
MANDELBAUM				
SHEUMAKER				
VOSS				
WESTERGAARD				
TOTAL				

MOTION CARRIED APPROVED

**CERTIFICATE**

I, LAURA BAUMGARTNER, City Clerk of said City hereby certify that at a meeting of the City Council of said City of Des Moines, held on the above date, among other proceedings the above was adopted.

IN WITNESS WHEREOF, I have hereunto set my hand and affixed my seal the day and year first above written.

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
City Clerk

February 14, 2023

Communication from the City Plan and Zoning Commission advising that at their February 2, 2023 meeting, the following action was taken regarding a request from Hubbell Realty Company (owner), represented by Caleb Smith (officer), for the following regarding property located in the vicinity of 1300 Tuttle Street:

- A) Review and approval of a 1<sup>st</sup> amendment to the Gray's Station PUD Conceptual Plan, to update the realignment of Southwest 13<sup>th</sup> Street and to allow administrative review of future development site plans in leu of public review by the Plan and Zoning Commission and City Council.
- B) Review and approval of a PUD Final Development Plan "Gray's Station Telus Condos," to allow for the construction of two condo buildings containing 84 residential units.
- C) Review and approval of a PUD Final Development Plan "Gray's Station Plat 5 Townhomes and Clubhouse," to allow for the construction of 65 townhomes and a clubhouse amenity building.

**COMMISSION RECOMMENDATION:**

After public hearing, the members voted 8-1-1 as follows.

Commission Action:	Yes	Nays	Pass	Absent
Francis Boggus				X
Dan Drendel	X			
Leah Rudolphi	X			
Carol Maher		X		
Abby Chungath				X
Kayla Berkson	X			
Chris Draper			X	
Todd Garner				X
Johnny Alcivar	X			
Justyn Lewis	X			
Carolyn Jenison				X
William Page	X			
Andrew Lorentzen				X
Emily Webb	X			
Katie Gillette	X			

**APPROVAL** of Part A) Approval of the request to amend the Gray's Station PUD Conceptual Plan to update the realignment of Southwest 13th Street.

Furthermore, Staff recommends limited approval of the request to amend the Gray's Station PUD Conceptual Plan to revise the 'Notes' section to allow administrative review of future development site plans in lieu of public review by the Plan and Zoning Commission and City Council, as follows:

- Any final development plan for a new row home type development or a minor revision to an existing development is subject to administrative (by staff) review and approval.
- Any final development plan for a mixed-use building or a multiple-household residential building is subject to review and approval by the Plan and Zoning Commission, with any appeal of that decision considered by the City Council.

Part B) Approval of the proposed PUD Final Development Plan "Gray's Station Telus Condos," subject to compliance with all administrative review comments, plus the following conditions:

1. Provision of a "band" of architectural concrete masonry unit materials from grade up to 2' for the standalone garages that matches the concrete masonry units proposed for the condo buildings.
2. Provision of a sidewalk connection from the southeastern corner of "Building 2" that crosses the internal vehicular drive to link up with the north-south pedestrian pathway that bisects this development area.
3. Provision of more-detailed exterior façade materiality information that matches the standards outlined within the PUD Conceptual Plan.
4. Verification that all exterior façade glazing contains a minimum 50% transmittance factor and a reflectance factor of no more than .25.

Part C) Approval of the proposed PUD Final Development Plan "Gray's Station Plat 5 Townhomes and Clubhouse," subject to compliance with all administrative review comments, plus the following conditions:

1. Provision of a sidewalk connection from the clubhouse's outdoor recreation area/dog park that crosses a private drive to link up with a proposed courtyard space in between lots "40" and "41."
2. Verification that any proposed rooftop mechanical equipment will be screened from view from all sides.
3. Provision of additional information regarding mechanical equipment and associated screening (both ground- and roof-mounted) for the clubhouse building.
4. Provision of more-detailed materiality information for the clubhouse building that matches the standards outlined within the PUD Conceptual Plan.

5. Verification that all exterior façade glazing contains a minimum 50% transmittance factor and a reflectance factor of no more than .25.

## **STAFF RECOMMENDATION TO THE P&Z COMMISSION**

Part A) Staff recommends approval of the request to amend the Gray's Station PUD Conceptual Plan to update the realignment of Southwest 13th Street.

Furthermore, Staff recommends limited approval of the request to amend the Gray's Station PUD Conceptual Plan to revise the 'Notes' section to allow administrative review of future development site plans in lieu of public review by the Plan and Zoning Commission and City Council, as follows:

- Any final development plan for a new row home type development or a minor revision to an existing development is subject to administrative (by staff) review and approval.
- Any final development plan for a mixed-use building or a multiple-household residential building is subject to review and approval by the Plan and Zoning Commission, with any appeal of that decision considered by the City Council.

Part B) Staff recommends approval of the proposed PUD Final Development Plan "Gray's Station Telus Condos," subject to compliance with all administrative review comments, plus the following conditions:

1. Provision of a "band" of architectural concrete masonry unit materials from grade up to 2' for the standalone garages that matches the concrete masonry units proposed for the condo buildings.
2. Provision of a sidewalk connection from the southeastern corner of "Building 2" that crosses the internal vehicular drive to link up with the north-south pedestrian pathway that bisects this development area.
3. Provision of more-detailed exterior façade materiality information that matches the standards outlined within the PUD Conceptual Plan.
4. Verification that all exterior façade glazing contains a minimum 50% transmittance factor and a reflectance factor of no more than .25.

Part C) Staff recommends approval of the proposed PUD Final Development Plan "Gray's Station Plat 5 Townhomes and Clubhouse," subject to compliance with all administrative review comments, plus the following conditions:

1. Provision of a sidewalk connection from the clubhouse's outdoor recreation area/dog park that crosses a private drive to link up with a proposed courtyard space in between lots "40" and "41."
2. Verification that any proposed rooftop mechanical equipment will be screened from view from all sides.

3. Provision of additional information regarding mechanical equipment and associated screening (both ground- and roof-mounted) for the clubhouse building.
4. Provision of more-detailed materiality information for the clubhouse building that matches the standards outlined within the PUD Conceptual Plan.
5. Verification that all exterior façade glazing contains a minimum 50% transmittance factor and a reflectance factor of no more than .25.

## **STAFF REPORT TO THE PLANNING COMMISSION**

### **I. GENERAL INFORMATION**

1. **Purpose of Request:** The applicant is requesting an amendment to the Gray's Station PUD Conceptual Plan, which would alter the alignment of Southwest 13<sup>th</sup> Street between Tuttle Street and Grays Parkway. Further, the applicant is proposing to amend the PUD's 'Notes' to remove the requirement that "All Final Development Plans are subject to review and approval by the Plan & Zoning Commission and the City Council," which would instead allow administrative review in lieu.

Additionally, the developer is seeking to subdivide the 'Gray's Station Plat 5' area within the PUD to allow the development and construction of 65 one-household residential lots containing row homes, a clubhouse amenity building, a lot containing two condo buildings with 84 residential units, and two outlots.

2. **Size of Site:** Gray's Station PUD – 84.4 acres; Plat 5 Area – 11.1 acres.
3. **Existing Zoning (site):** Gray's Station PUD, Planned Unit Development District.
4. **Existing Land Use (site):** The area is currently undeveloped land.
5. **Adjacent Land Use and Zoning:**

**North** – "DX2"; Uses are Tuttle Street, open space, and vacant industrial buildings.

**South** – "Gray's Station PUD" & "F"; Uses are open space, a city stormwater management facility, the Raccoon River, and Gray's Lake Park.

**East** – "Gray's Station PUD"; Uses are townhomes and multiple-household dwelling units.

**West** – "Gray's Station PUD" & "DX2"; Uses are industrial warehousing and undeveloped land.

6. **General Neighborhood/Area Land Uses:** The subject property is located generally to the south of West Martin Luther King, Jr. Parkway and to the west of Southwest 11<sup>th</sup> Street in the southern fringes of Downtown. The site's vicinity consists of mixed-use and medium-density residential development, parkland, and undeveloped parcels. The Raccoon River flanks the southern boundary of the site. Gray's Lake Park lies further south of the river.

7. **Applicable Recognized Neighborhood(s):** The subject property is located within the Downtown Des Moines Neighborhood Association. All neighborhood associations were notified of the public hearing by emailing of the Preliminary Agenda on January 13, 2023, and by emailing of the Final Agenda on January 27, 2023. Additionally, separate notifications of the hearing for this specific item were mailed on January 13, 2023 (20 days prior to the public hearing) and January 23, 2023 (10 days prior to the public hearing) to the Downtown Des Moines Neighborhood Association and to the primary titleholder on file with the Polk County Assessor for each property within 250 feet of the site.

All agendas and notices are mailed to the primary contact(s) designated by the recognized neighborhood association to the City of Des Moines Neighborhood Services Department on the date of the mailing. The Sheridan Gardens Neighborhood does not have a mailing address on file. The Brook Run Neighborhood Association mailings were sent to Brandon Brown, 120 Southwest 5<sup>th</sup> Street, Unit 101, Des Moines, IA 50309.

The applicant can provide a summary of the neighborhood outreach at the public hearing.

8. **Relevant Zoning History:** The subject property was rezoned to "PUD" on August 14, 2017, at which time a general "PUD" Conceptual Plan was adopted.
9. **PlanDSM: Creating Our Tomorrow Plan Land Use Plan Designation:** Downtown Mixed Use and Neighborhood Mixed Use.
10. **Applicable Regulations:** Taking into consideration the criteria set forth in Chapter 18B of the Iowa Code, the Commission reviews all proposals to amend zoning boundaries or regulations within the City of Des Moines. Such amendments must be in conformance with the comprehensive plan for the City and designed to meet the criteria in 414.3 of the Iowa Code. The Commission may make recommendations to the City Council on conditions to be made in addition to the existing regulations so long as the subject property owner agrees to them in writing. The recommendation of the Commission will be forwarded to the City Council.
- The application, accompanying evidence and "PUD" Conceptual Plan shall be considered by the Plan and Zoning commission at a public hearing. The Commission shall review the conformity of the proposed development with the standards of the City Code and with recognized principles of civic design, land use planning, and landscape architecture. After the hearing, the Commission may vote to recommend either approval or disapproval of the amended "PUD" Conceptual Plan as submitted, or to recommend that the developer amend the plan or request to preserve the intent and purpose of this chapter to promote public health, safety, morals and general welfare. The recommendations of the Commission shall be referred to the City Council.

Taking into consideration the criteria set forth in Chapter 18B of the Iowa Code, the Commission shall determine if such Preliminary Plat conforms to the standards and requirements outlined in Chapter 354 of the Iowa Code, and the City Subdivision Ordinance and shall approve, conditionally approve or reject such Plat within 45 days after the date of submission to the City Permit and Development Center. Unless the applicant agrees in writing to an extension of time, the Preliminary Plat shall be

deemed approved if the Commission does not act within such 45-day period. The Commission's action for approval or conditional approval shall be null and void unless the Final Plat is submitted to the City Permit and Development Center within 270 days after the date of such action; provided, however, that the Permit and Development Administrator may grant, upon written request of the applicant, up to a 90-day extension for submittal of the Final Plat to the City Permit and Development Center.

Taking into consideration the criteria set forth in Chapter 18B of the Iowa Code, the Commission shall determine if such Preliminary Plat conforms to the standards and requirements outlined in Chapter 354 of the Iowa Code, and the City Subdivision Ordinance and shall approve, conditionally approve or reject such Plat within 45 days after the date of submission to the City Permit and Development Center. Unless the applicant agrees in writing to an extension of time, the Preliminary Plat shall be deemed approved if the Commission does not act within such 45-day period. The Commission's action for approval or conditional approval shall be null and void unless the Final Plat is submitted to the City Permit and Development Center within 270 days after the date of such action; provided, however, that the Permit and Development Administrator may grant, upon written request of the applicant, up to a 90-day extension for submittal of the Final Plat to the City Permit and Development Center.

Pursuant to Section 135-9.1.1.B of the Planning and Design Ordinance, the site plan review requirements of Chapter 135 are designed to ensure the orderly and harmonious development of property in a manner that shall:

- Promote the most beneficial relation between present and proposed future uses of land and the present and proposed future circulation of traffic throughout the city;
- Permit present development of property commensurate with fair and orderly planning for future development of other properties in the various areas of the city with respect to the availability and capacity, present and foreseeable, of public facilities and services. The factors to be considered in arriving at a conclusion concerning proposed present development of property shall include the following:
  - The maximum population density for the proposed development, the proposed density of use, and consideration of the effect the proposal will have on the capacity of existing water and sanitary sewer lines to the end that existing systems will not become overloaded or capacity so substantially decreased that site use will inhibit or preclude planned future development;
  - Zoning restrictions at the time of the proposal;
- The city's comprehensive plan;
- The city's plans for future construction and provision for public facilities and services; and

- The facilities and services already available to the area which will be affected by the proposed site use;
- Encourage adequate provision for surface and subsurface drainage, in order to ensure that future development and other properties in various areas of the city will not be adversely affected;
- Provide suitable screening of parking, truck loading, refuse and recycling disposal, and outdoor storage areas from adjacent residential districts;
- Encourage the preservation of canopied areas and mature trees and require mitigation for the removal of trees; and
- Consider the smart planning principles set forth in Iowa Code Chapter 18B.

## II. ADDITIONAL APPLICABLE INFORMATION

1. **Gray's Station PUD Conceptual Plan:** If the proposed amendment to the Gray's Station "PUD" is approved by the City Council, the applicant must submit to the Planning and Urban Design Administrator a revised version of the "PUD" Conceptual Plan that reflects any conditions of approval.
2. **Realignment of Southwest 13<sup>th</sup> Street:** The proposed amendment to the PUD incorporates a slight realignment of Southwest 13<sup>th</sup> Street. The initial alignment proposed for Southwest 13<sup>th</sup> Street was straight north-south between Tuttle Street and Grays Parkway. With the proposed realignment, the southern portion of the street curves slightly east to join Grays Parkway. The slight realignment would not change or impact the design of the PUD in any significant manner.
3. **Review and Approval:** Per the existing PUD Conceptual Plan, all final development plans within the PUD are currently subject to review and approval by the Plan and Zoning Commission and the City Council. The proposed amendment would eliminate the need for a Commission and Council review for future development plans and would allow administrative-only review. While an administrative review is appropriate for minor changes within the existing development plans and for row homes, a more-comprehensive review, including approval by the Plan and Zoning Commission, is necessary for larger mixed-use and multi-family residential developments. The PUD is well-established to allow developments such as row homes. However, there is some ambiguity relating to larger mixed-use and multi-family buildings which might be potentially sited in the southern portion of the PUD. Staff feels a public process is reasonably necessary to allow comprehensive review of those future development plans and other larger, mixed-use, and multiple-household residential buildings.
4. **Preliminary Plat:** The proposed preliminary plat would involve the creation and extension of public and private streets to serve the development. As an infill, Downtown site, the street layouts (both public and private) are proposed to complement the City street grid (in terms of both layout and construction) that already exists in this area. Murphy Street and Grays Parkway are proposed to extend to the west, with additional westward extensions of already-existing east-west private streets. A new segment of Southwest 13<sup>th</sup> Street is proposed to be constructed from Tuttle

Street to Grays Parkway. Additional east-west private streets are proposed in the southern portion of the development.

65 one-household lots would be created for clustered, rowhome-style development. Additionally, larger lots would be created for a neighborhood "clubhouse" building and open space, as well as a lot that would contain (2) 3-story residential buildings for a total of 84 dwelling units.

All new streets would be constructed with urban cross-sections generally consistent with the construction of streets within the Gray's Station area and overall Downtown. Public utilities (sanitary sewer, water service) are proposed to be extended both within public rights-of-way and within private streets. Public storm sewer is also proposed within the development, and the area's overall stormwater management has been contemplated and accounted for with the existing Gray's Station city stormwater facility that sits to the south of this development area.

On-street and off-street pedestrian connections are proposed that would provide linkages both within this development, as well as with the Gray's Station Trail to the south and the recently-platted area to the east. The Tuttle Street Cycle Track project is proposed to be extended through the northern border of this development area. A pedestrian "Paseo" is proposed to be extended from the east and bisect the northern half of the development.

- 5. Development Plan – "Gray's Station Telus Condos":** The Telus condos development plan consists of (2) 3-story residential buildings that would contain 84 dwelling units, as well three standalone garage buildings for motor vehicle parking.

The northern building ("Building 1") is proposed to frame the corner of Tuttle and Southwest 13<sup>th</sup> Streets, with the southern building ("Building 2") proposed to frame the corner of Murphy and Southwest 13<sup>th</sup> Streets. The aforementioned Paseo is proposed between the two buildings before intersecting with Southwest 13<sup>th</sup> Street. A motor vehicle parking area would sit behind the proposed residential buildings. A total of 137 motor vehicle parking spaces (53 uncovered, 84 in garages) are proposed. 20 bicycle parking spaces are proposed adjacent to the Paseo in between the residential buildings.

Building and site design is required to conform to the design criteria set forth in the Gray's Station PUD, specifically the criteria for "High Density Residential" buildings. The buildings are proposed to be clad in a mix of brick, concrete masonry units, fiber cement board, and architectural metal paneling in a manner consistent with the material palette stipulated within the PUD Conceptual Plan. Buildings are proposed to sit abutting street-facing lot lines, with street-facing entrances. Proposed floor plans demonstrate a stacking action amongst the units, with ground floor units opening to the street, and upper-floor units with street- or internal-facing balconies.

- 6. Development Plan – "Gray's Station Plat 5 Row Homes and Clubhouse":** The development plan for the row homes and clubhouse demonstrates 17 individual clusters of 3-5 row homes, as well as a larger area within the southern portion of the development area for the clubhouse and outdoor recreation areas. Each rowhome would sit on its own lot.

Row homes are proposed to be oriented toward either a public street or a designated pedestrian way. Rear-loading garages for motor vehicle parking are proposed for each rowhome. These garages would have space for up to 2 vehicles and are proposed to be accessed from the development's private drives that would function as alleys. Additional motor vehicle parking facilities are proposed to be either on street in a parallel fashion or off of a private drive directly abutting the clubhouse/outdoor recreation area. 20 bicycle parking spaces are demonstrated in a clumped fashion oriented around the outdoor recreation area.

The outdoor recreation area is proposed to consist of a swimming pool, dog park, pickleball courts, landscaping arrangements, and other unprogrammed open space. The clubhouse is designed as a flexible, general-use entertainment space that would open up to the pool area.

Building and individual site design is required to conform to the design criteria set forth in the Gray's Station PUD, specifically the criteria for "Low-Medium Residential" buildings. With the exception of the clubhouse, all buildings that are a part of this development plan are proposed to be at least 2 stories. Variations of 5 different rowhome "product types" are proposed to be placed in clusters throughout the area of this development plan. These product types differ in their building heights, exterior façade materials mixes, color palettes, entryway configurations, and proposed floor plans. Similar to the condo buildings, a mix of fiber cement board paneling, fiber cement board lap siding, architectural metal paneling, brick, stone, and finished concrete surfaces are proposed for the exterior of the row homes. Material placement and percentages differ by specific product type. Individual townhouse clusters are positioned between approximately 5 to 12 feet from the edges of streets or other pedestrian ways. Row homes would contain stoops/porches that would orient toward their respective streets/pedestrian ways.

## **SUMMARY OF DISCUSSION**

Nick Tarpey presented staff report and recommendation.

Chris Draper asked how large a project would need to be before it would come before the commission if the staff recommendation is adopted.

Nick Tarpey stated multi-family or mixed-use projects. Any one household residential project would be administratively reviewed.

Chris Draper asked if there are any future concerns with connectivity to broadband.

Jason Van Essen stated the question of broadband is outside the scope of this PUD. Amendments before the commission would not have an impact.

Will Page asked if the provision "to allow administrative review of future development site plans in lieu of public review by the Plan and Zoning Commission and City Council" is applicable to this project only.

Jason Van Essen stated there is an understood development pattern for the rowhomes and that staff supports reviewing those administratively. Other new project types should

have a public hearing, but staff believes that could be limited to just the Commission, eliminating the need to go before the City Council. These amendments would take effect for future projects.

Johnny Alcivar asked if there was a threshold for rowhome development being administratively reviewed.

Jason Van Essen stated if the staff recommendation is adopted, rowhome development would only be reviewed administratively.

Chris Draper asked if there were thoughts of how different a rowhome development would need to be before its reviewed publicly.

Jason Van Essen stated within the PUD, the developer has created several different designs. City staff feels comfortable reviewing proposed changes given the existing standards within the PUD and past negotiations on previous iterations.

Carol Maher asked if that would give city staff the ability to approve type 2 deign alternatives.

Jason Van Essen stated no, the development will need to match the standards within the PUD. If city staff and the developer disagreed on those standards, it would come before the commission.

Dan Drendel asked if it would be developed under the same PUD if another developer bought out future phases.

Jason Van Essen stated it is possible for another builder to follow the standards within the current PUD. If they wanted to go a different direction, amendments to the PUD would be needed to accommodate new designs.

Chris Draper asked if the existing powerlines would be undergrounded.

Jason Van Essen stated there are a few major lines that will remain.

Caleb Smith, Hubbell Realty Company, 6900 Westown Parkway, WDM stated after a simple restriping plan for the LINC, they thought it would make sense to suggest removing the clause that requires review by the Plan and Zoning Commission and City Council. City staff has come forward with a reasonable approach given the size, style and scope of the townhomes have been well established. It does make sense for the commission to review multi-family projects due to the variety they will have. The overhead utility line is a massive transmission line that would be a multi-million dollar project and something Mid-American Energy isn't supportive of. These plans do include utility easement corridors that will provide all units with telecom communication.

Will Page asked if they could explain their thoughts of darker colors being more appropriate for urban settings.

Caleb Smith stated they learned a lot during the first phase of this development and noticed those darker colors sold better. They also have a network of builders across the country that allows them to see these types of trends.

Carol Maher asked if these are all rental units.

Calen Smith stated there are no rental properties in this phase of the project.

Carol Maher asked about parking around the multi-family development.

Ryan Hardisty, Civil Design Advantage, 4121 NW Urbandale Drive, Urbandale, IA stated the TELUS project has 84 total units with 137 parking spaces. There are 84 total garages with a few being detached.

Carol Maher asked why the parking ratio is more than 1 to 1.

Ryan Hardisty stated given the success of the LINC project, parking issues started to occur once the building was fully rented. Based on those issues, they revamped parking for TELUS.

Carol Maher asked if a parking space is included with purchase of a unit.

Michael Bialas, Hubbell Realty Company, 6900 Westown Parkway, WDM stated the parking spaces and garages will be sold separately. If they are not sold, those spaces would be turned over to the association.

Carol Maher asked if they would offer indoor bike storage.

Michael Bialas stated that is something they are exploring. If all the garages are not sold, they would likely convert one to bike storage.

Carol Maher stated they are only focused on how to fit more cars, instead of encouraging other means of transportation. They should be more forward thinking and include adequate bike parking and EV charging stations.

Emily Webb stated she understands the need for more parking. She lives in an urban rowhouse setting and just having a garage makes it challenging for others to visit you.

Justyn Lewis stated there are parking issues around this area, but he would like to see this development promote bicycling and walkability.

Ryan Hardisty stated they are providing 20 bike parking spaces at the main entrance.

Justyn Lewis asked if that's inside the building.

Ryan Hardisty stated they're exterior, located off SW 13<sup>th</sup> Street.

Will Page asked if they agree with staff recommendations.

Caleb Smith stated yes.

## **CHAIRPERSON OPENED THE PUBLIC HEARING**

Troy Hall, 2530 University Avenue stated he is generally supportive of this development and would like to continue seeing densification of downtown. He would encourage the developer to embrace architectural diversity and encourage different means of transportation.

## **CHAIRPERSON CLOSED THE PUBLIC HEARING**

Chris Draper stated he hopes we aren't straying away from the underlying strategies within tax abatement that would reduce parking ratios.

## **COMMISSION ACTION:**

Will Page made a motion for:

Part A) Approval of the request to amend the Gray's Station PUD Conceptual Plan to update the realignment of Southwest 13th Street.

Furthermore, Staff recommends limited approval of the request to amend the Gray's Station PUD Conceptual Plan to revise the 'Notes' section to allow administrative review of future development site plans in lieu of public review by the Plan and Zoning Commission and City Council, as follows:

- Any final development plan for a new row home type development or a minor revision to an existing development is subject to administrative (by staff) review and approval.
- Any final development plan for a mixed-use building or a multiple-household residential building is subject to review and approval by the Plan and Zoning Commission, with any appeal of that decision considered by the City Council.

Part B) Approval of the proposed PUD Final Development Plan "Gray's Station Telus Condos," subject to compliance with all administrative review comments, plus the following conditions:

1. Provision of a "band" of architectural concrete masonry unit materials from grade up to 2' for the standalone garages that matches the concrete masonry units proposed for the condo buildings.
2. Provision of a sidewalk connection from the southeastern corner of "Building 2" that crosses the internal vehicular drive to link up with the north-south pedestrian pathway that bisects this development area.
3. Provision of more-detailed exterior façade materiality information that matches the standards outlined within the PUD Conceptual Plan.
4. Verification that all exterior façade glazing contains a minimum 50% transmittance factor and a reflectance factor of no more than .25.

Part C) Approval of the proposed PUD Final Development Plan "Gray's Station Plat 5 Townhomes and Clubhouse," subject to compliance with all administrative review comments, plus the following conditions:

1. Provision of a sidewalk connection from the clubhouse's outdoor recreation area/dog park that crosses a private drive to link up with a proposed courtyard space in between lots "40" and "41."
2. Verification that any proposed rooftop mechanical equipment will be screened from view from all sides.
3. Provision of additional information regarding mechanical equipment and associated screening (both ground- and roof-mounted) for the clubhouse building.
4. Provision of more-detailed materiality information for the clubhouse building that matches the standards outlined within the PUD Conceptual Plan.
5. Verification that all exterior façade glazing contains a minimum 50% transmittance factor and a reflectance factor of no more than .25.

Motion passed: 8-1-1

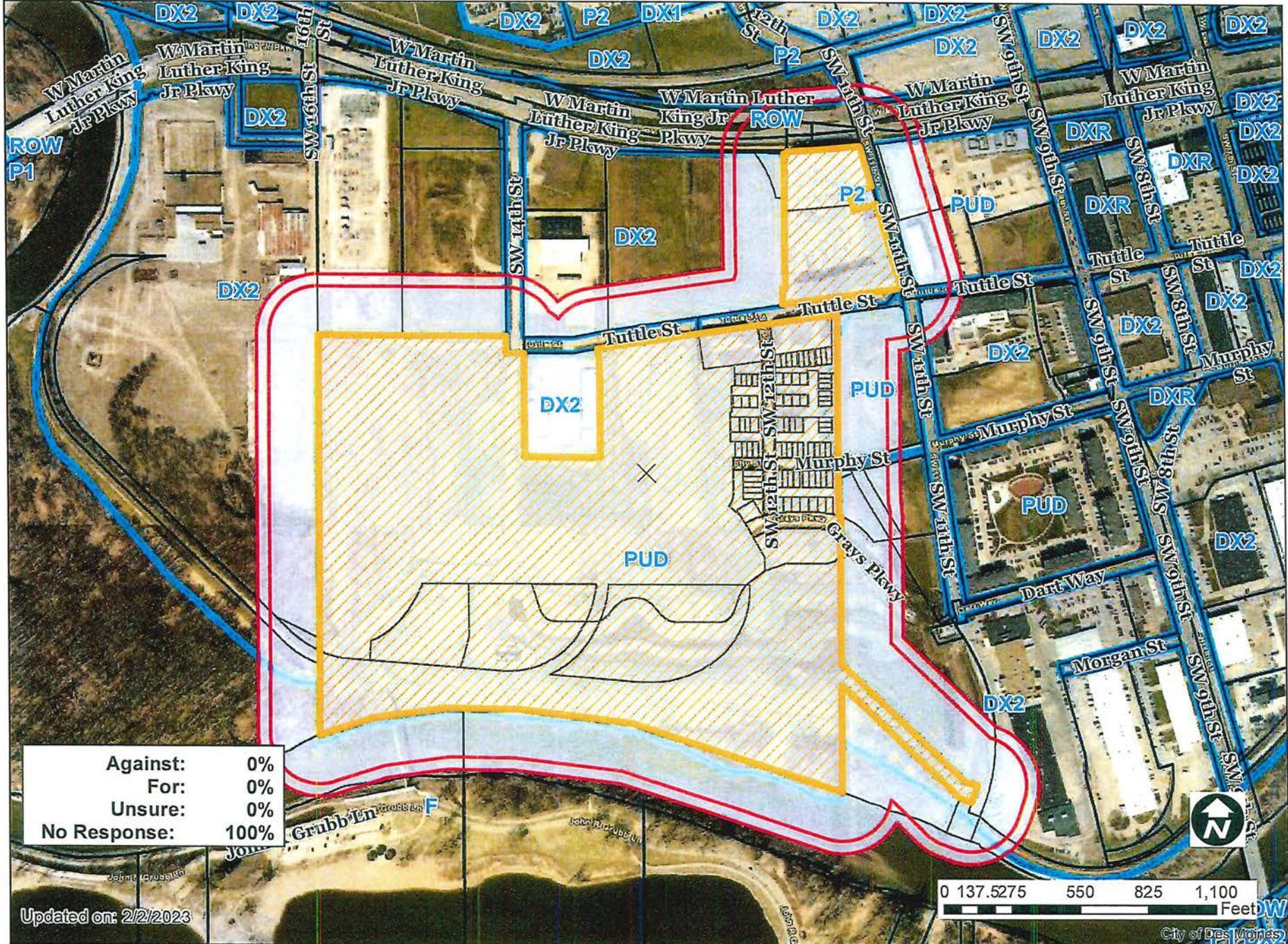
Respectfully submitted,



Jason Van Essen, AICP  
Planning & Urban Design Administrator

JMV:tjh





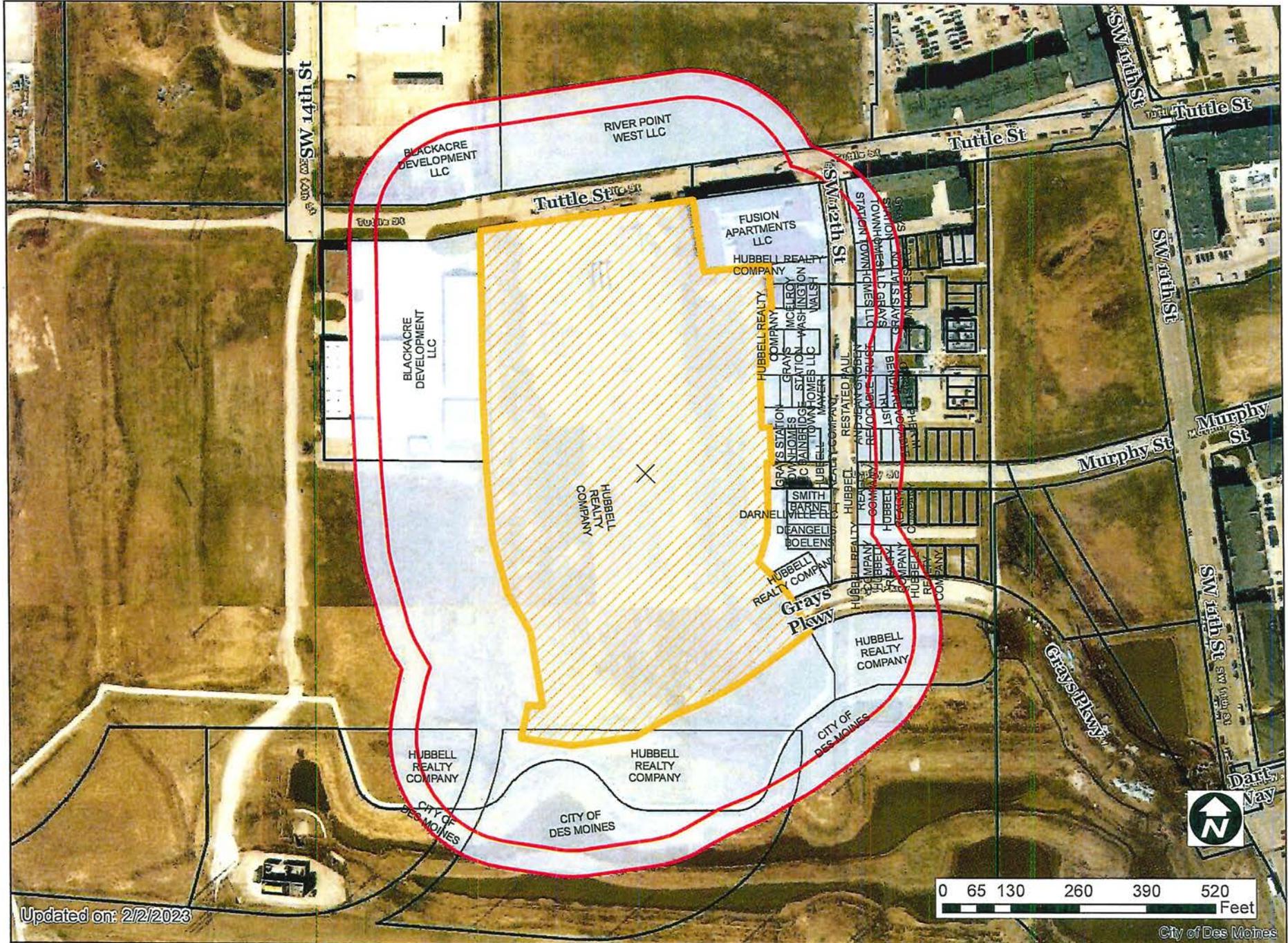
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Updated on: 2/2/2023

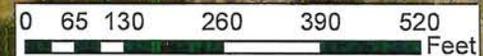
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1 inch = 517 feet



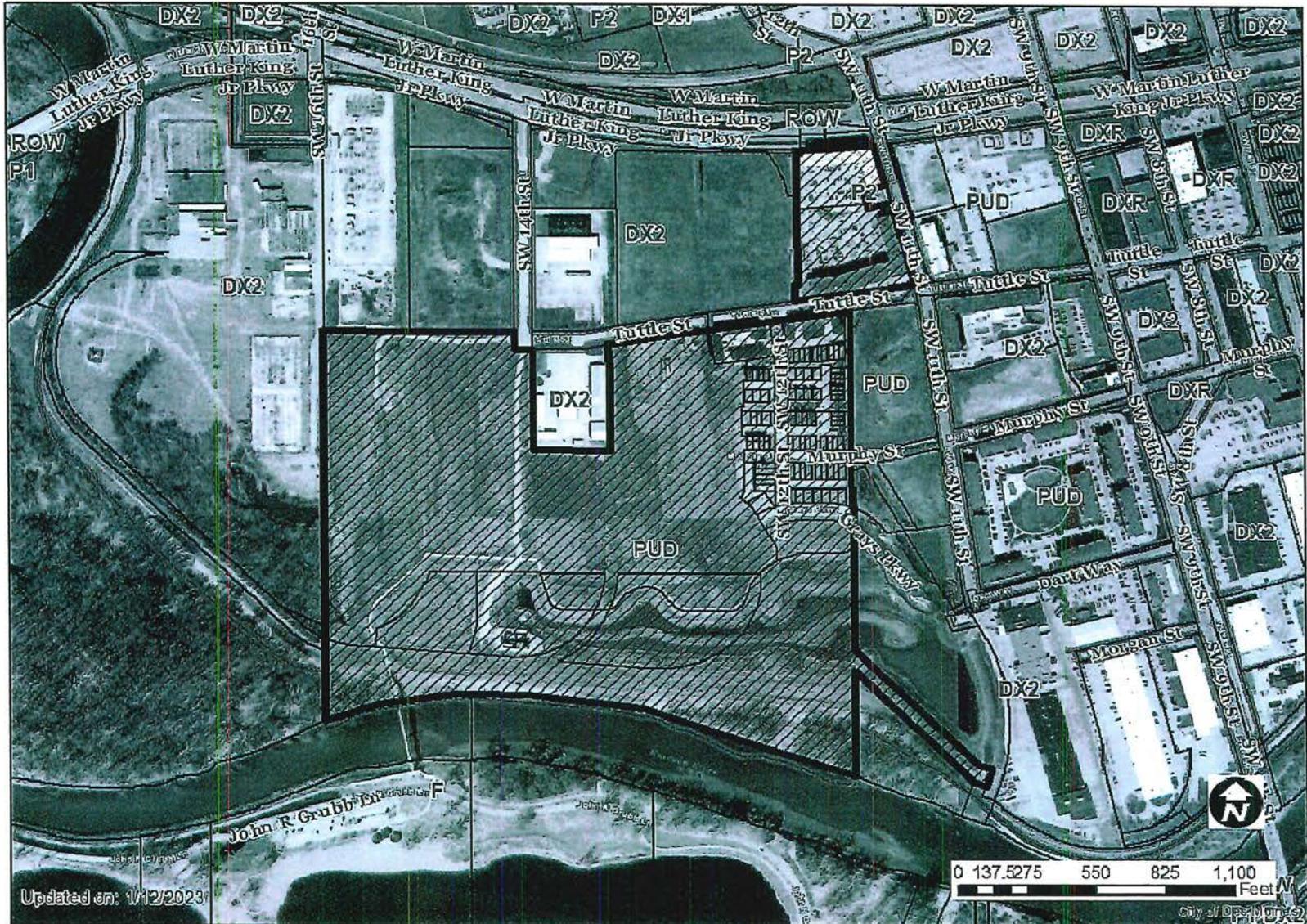


Updated on: 2/2/2023



City of Des Moines

1 inch = 243 feet



1 inch = 517 feet

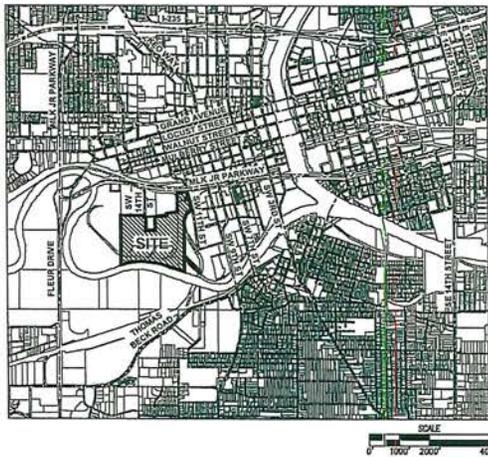


# CONCEPTUAL PLAN, FIRST AMENDMENT

# GRAY'S STATION

A PLANNED UNIT DEVELOPMENT IN THE CITY OF DES MOINES, COUNTY OF POLK, STATE OF IOWA

VICINITY MAP (1"=2000')



### DEVELOPER

HUBBELL REALTY COMPANY  
CONTACT: JOE PIETRUSZYNSKI  
3690 WESTOWN PARKWAY  
WEST DES MOINES, IOWA 50326  
PH. (515) 243-3228

### OWNER

NORFOLK SOUTHERN RY  
TAX DEPT  
3 COMMERCIAL PLACE, SUITE 29  
NORFOLK, VIRGINIA 23510

CITY OF DES MOINES  
400 ROBERT D RAY DRIVE  
DES MOINES, IOWA 50326

### SHEET INDEX

- 1 PLANNED UNIT DEVELOPMENT (COVER SHEET)
- 2 PLANNED UNIT DEVELOPMENT (BASE MAP)

### CITY PRE-APPLICATION MEETING DATE

MAY 2, 2017

### ZONING

- EXISTING:
- C-3B CENTRAL BUSINESS MIXED USE DISTRICT
  - FW FLOODWAY DISTRICT
  - DOWNTOWN OVERLAY DISTRICT
  - GAMBLING GAMES PROHIBITION OVERLAY DISTRICT

### PROPOSED:

- P.U.D.

### PLAN DSM LAND USE

DOWNTOWN MIXED USE

### ENGINEER

CIVIL DESIGN ADVANTAGE, LLC  
CONTACT: RYAN HARDISTY  
4121 NW URBANDALE DRIVE  
URBANDALE, IOWA 50322  
PH. (515) 369-4400

### NEIGHBORHOOD MEETINGS & MEETING DATE

The Gray's Station site is located such that it does not have many citizen neighbors to speak of. Hubbell Realty Company determined that the best way to solicit input related to the potential vision and development of the Gray's Station neighborhood was to invite community leaders to participate in a Steering Committee for the neighborhood. This Steering Committee included members of City Staff, members of the Urban Land Institute Iowa District Council, downtown neighborhood associations, a City Council member, and other individuals whose input would assist Hubbell Realty Company to create a unique new neighborhood for Des Moines. This group met twice in the Summer of 2016 to engage in setting the vision for the neighborhood and to review early concepts for Gray's Station. Their participation has influenced the resulting Conceptual Development Plan for Gray's Station.

### Steering Committee Meeting #1 - June 14, 2016

- o Location: RDC Planning & Design 301 and Avenue Des Moines, Iowa 50309
- o Time: 2 PM - 4 PM

### Steering Committee Meeting #2 - August 15, 2016

- o Location: RDC Planning & Design 301 and Avenue Des Moines, Iowa 50309
- o Time: 2 PM - 4 PM

### Neighborhood Open House - August 15, 2016

- o Location: CityVista on 6th/ Common Room 550 SW 8th ST Des Moines, Iowa 50309
- o Time: 5:30 PM - 8:30 PM

### P.U.D. PROPERTY DESCRIPTION

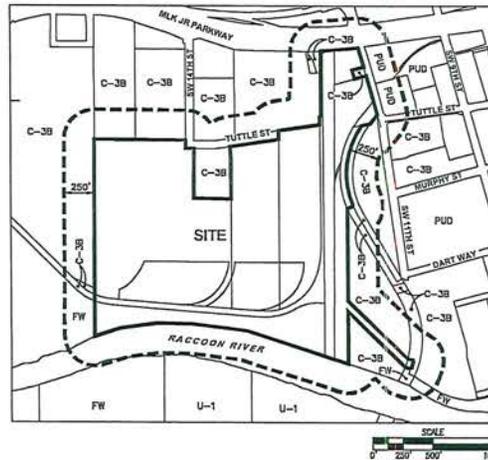
OUTLOTS Y AND Z, AND A PART OF LOTS 6, 7 AND 8, CENTRAL DES MOINES INDUSTRIAL PARK, AN OFFICIAL PLAT AND PART OF LOTS 81, 81A, 82, 85 AND 86 IN FACTORY ADDITION, AN OFFICIAL PLAT, ALL BEING IN THE CITY OF DES MOINES, POLK COUNTY, IOWA AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF SAID LOT 6, CENTRAL DES MOINES INDUSTRIAL PARK HAVING A STATE PLANE IOWA SOUTH COORDINATE VALUE OF NORTHERING 575161.03 AND EASTING 1602013.57; THENCE SOUTH 89° (DECIMALS) 40' (MINUTES) 50" (SECONDS) EAST ALONG THE NORTHERLY LINE OF SAID LOT 6, A DISTANCE OF 338.83 FEET TO THE SOUTHWEST CORNER OF LOT 2, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE SOUTH 89°46'52" EAST CONTINUING ALONG SAID NORTHERLY LINE, 418.55 FEET TO THE SOUTHEAST CORNER OF SAID LOT 2 ALSO BEING A POINT ON THE WEST RIGHT OF WAY LINE OF SW 14TH STREET; THENCE SOUTH 00°01'02" EAST CONTINUING ALONG SAID NORTHERLY LINE, 89.58 FEET TO THE SOUTHWEST CORNER OF STREET LOT 8, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE SOUTH 89°52'21" EAST CONTINUING ALONG SAID NORTHERLY LINE, 70.00 FEET TO THE NORTHWEST CORNER OF LOT 8, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE SOUTH 02°08'06" WEST CONTINUING ALONG SAID NORTHERLY LINE, 423.70 FEET TO THE SOUTHWEST CORNER OF SAID LOT 8; THENCE SOUTH 89°18'18" EAST CONTINUING ALONG SAID NORTHERLY LINE, 307.20 FEET TO THE SOUTHEAST CORNER OF SAID LOT 8, ALSO BEING THE NORTHEAST CORNER OF SAID LOT 9; THENCE NORTH 02°08'11" WEST ALONG THE WESTERLY LINE OF SAID LOT 7, CENTRAL DES MOINES INDUSTRIAL PARK, 333.39 FEET; THENCE NORTHERLY CONTINUING ALONG SAID WESTERLY LINE AND A CURVE CONCAVE WESTERLY WHOSE RADIUS IS 854.20 FEET, WHOSE ARC LENGTH IS 83.62 FEET AND WHOSE CHORD BEARS NORTH 03°26'07" WEST, 83.77 FEET; THENCE NORTH 88°38'06" WEST CONTINUING ALONG SAID WESTERLY LINE, 15.78 FEET TO THE NORTHWEST CORNER OF SAID LOT 7 ALSO BEING THE SOUTH RIGHT OF WAY LINE OF TUTTLE STREET; THENCE NORTH 81°04'49" EAST ALONG THE NORTH LINE OF SAID LOT 7, A DISTANCE OF 411.78 FEET TO THE NORTHEAST CORNER OF SAID LOT 7; THENCE NORTH 02°05'48" WEST ALONG THE WEST LINE OF SAID LOT 8, CENTRAL DES MOINES INDUSTRIAL PARK, 72.11 FEET TO THE NORTHWEST CORNER OF SAID LOT 8; THENCE NORTH 81°52'47" EAST ALONG THE NORTH LINE OF SAID LOT 8, A DISTANCE OF 347.62 FEET TO THE SOUTHEAST CORNER OF LOT 4, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE NORTH 02°31'45" WEST ALONG THE WESTERLY LINE OF SAID OUTLOT Y, CENTRAL DES MOINES INDUSTRIAL PARK, 63.43 FEET; THENCE NORTH 87°20'10" EAST CONTINUING ALONG SAID WESTERLY LINE, 10.37 FEET; THENCE NORTH 02°12'49" WEST CONTINUING ALONG SAID WESTERLY LINE, 554.59 FEET TO THE NORTHEAST CORNER OF LOT 5, SAID CENTRAL DES MOINES INDUSTRIAL PARK AND HAVING A STATE PLANE IOWA SOUTH COORDINATE VALUE OF NORTHERING 1504699.46; THENCE SOUTH 89°04'27" EAST ALONG THE NORTH LINE OF SAID OUTLOT Y, 15.36 FEET TO THE NORTHEAST CORNER OF SAID OUTLOT Y; THENCE NORTH 02°11'21" WEST ALONG THE WEST LINE OF SAID LOT 85, FACTORY ADDITION, 8.59 FEET TO THE SOUTHERLY RIGHT OF WAY LINE OF MARTIN LUTHER KING JR PARKWAY; THENCE NORTH 88°10'40" EAST ALONG SAID SOUTHERLY RIGHT OF WAY LINE, 100.23 FEET; THENCE NORTH 84°40'04" EAST CONTINUING ALONG SAID SOUTHERLY RIGHT OF WAY LINE, 198.44 FEET TO THE WESTERLY RIGHT OF WAY LINE OF SW 11TH STREET; THENCE SOUTH 15°27'22" EAST ALONG SAID WESTERLY RIGHT OF WAY LINE, 173.21 FEET; THENCE SOUTH 74°42'29" WEST, 89.77 FEET; THENCE SOUTH 15°25'34" EAST, 75.29 FEET; THENCE NORTH 74°32'27" EAST, 99.61 FEET TO SAID WESTERLY RIGHT OF WAY LINE; THENCE SOUTH 15°27'22" EAST ALONG SAID WESTERLY RIGHT OF WAY LINE, 344.27 FEET TO A CORNER ON THE SOUTH 79°27'18" WEST, 73.58 FEET TO THE EASTERLY LINE OF THE WEST HALF OF SAID LOT 81; THENCE SOUTHERLY ALONG SAID EASTERLY LINE AND A CURVE CONCAVE EASTERLY WHOSE RADIUS IS 716.25 FEET, WHOSE ARC LENGTH IS 51.41 FEET TO A POINT ON THE EAST LINE OF THE WEST 100 FEET OF SAID LOT 80, FACTORY ADDITION; THENCE SOUTH 02°10'40" EAST ALONG SAID EAST LINE, 767.72 FEET; THENCE SOUTH 43°51'01" EAST, 692.69 FEET TO THE WESTERLY LINE OF SAID LOT 81; THENCE SOUTH 78°02'20" EAST, 50.00 FEET TO SAID EASTERLY LINE OF THE WEST HALF OF LOT 81, HAVING A STATE PLANE IOWA SOUTH COORDINATE VALUE OF NORTHERING 573317.97 AND EASTING 1605461.84; THENCE SOUTHERLY ALONG SAID EASTERLY LINE AND A CURVE CONCAVE WESTERLY WHOSE RADIUS IS 740.85 FEET, WHOSE ARC LENGTH IS 78.04 FEET AND WHOSE CHORD BEARS SOUTH 17°03'55" WEST, 78.01 FEET; THENCE NORTH 79°20'25" WEST, 50.00 FEET TO SAID WESTERLY LINE OF LOT 81; THENCE NORTH 43°36'19" WEST, 653.55 FEET TO SAID EAST LINE OF THE WEST 100 FEET OF LOT 80; THENCE SOUTH 02°07'40" EAST ALONG SAID EAST LINE, 436.03 FEET TO THE NORTH BANK AND MEANDER LINE OF THE RACCOON RIVER; THENCE NORTH 71°44'57" WEST AND ALONG SAID MEANDER LINE AND THE SOUTHERLY LINE OF SAID OUTLOT Z, CENTRAL DES MOINES INDUSTRIAL PARK, 888.50 FEET; THENCE NORTH 83°01'48" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 463.82 FEET; THENCE NORTH 89°10'53" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 281.24 FEET; THENCE SOUTH 82°06'08" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 286.82 FEET; THENCE SOUTH 72°02'02" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 228.32 FEET TO THE SOUTHWEST CORNER OF SAID OUTLOT Z; THENCE NORTH 02°03'17" EAST ALONG THE WEST LINE OF SAID CENTRAL DES MOINES INDUSTRIAL PARK, 1547.00 FEET TO THE POINT OF BEGINNING AND CONTAINING 63.75 ACRES (3,447,188 SQUARE FEET) AND IS SUBJECT TO ANY AND ALL EASEMENTS OF RECORD.

### NOTES

1. THIS SITE IS SUBJECT TO THE TREE REMOVAL AND MITIGATION ORDINANCE OF THE CITY CODE. HUBBELL REALTY COMPANY IS ENTERING INTO A DEVELOPMENT AGREEMENT WITH THE CITY OF DES MOINES THAT OUTLINE A CUSTOM APPROACH TO TREE REMOVAL AND MITIGATION FOR THE GRAY'S STATION PLANNED UNIT DEVELOPMENT. NO TREE SURVEY OR CANOPY AREA METHOD INFORMATION WAS SUBMITTED FOR REVIEW THIS INFORMATION WILL BE PROVIDED WITH THE FINAL DEVELOPMENT PLAN.
2. ALL REQUIRED STORM-WATER MANAGEMENT CAN BE ACHIEVED UNDER THE PROPOSED CONCEPT. (NO DESIGN SUBMITTED)
3. ANY DEVELOPMENT PLAN REVIEW IS SUBJECT TO PROVIDING A TRAFFIC IMPACT ANALYSIS REVIEWED BY THE CITY TRAFFIC ENGINEER.
4. THE SITE SHALL BE PLATTED TO PRESERVE PUBLIC TRANSPORTATION CORRIDORS IN GENERAL CONFORMANCE WITH THE MASTER PLAN.
5. ALL SANITARY SEWER CONNECTIONS ARE AVAILABLE TO SERVICE THE PROPOSED CONCEPT.
6. NO LESS THAN SEVENTY-FIVE PERCENT (75%) OF THE LOW-MEDIUM DENSITY RESIDENTIAL UNITS SHALL HAVE A MINIMUM OF 2 STOREYS.
7. NO MORE THAN TWENTY-FIVE PERCENT (25%) OF THE LOW-MEDIUM DENSITY RESIDENTIAL UNITS SHALL HAVE A MINIMUM HEIGHT OF 1 STORY.
8. NO MORE THAN TWO 1-STORY LOW-MEDIUM DENSITY RESIDENTIAL UNITS SHALL BE CONSTRUCTED ON ADJOINING SITES.
9. THE 75%-25% MINIMUM HEIGHT RATIO FOR THE LOW-MEDIUM DENSITY RESIDENTIAL UNITS SHOULD BE ACHIEVED WITH EACH PHASE OF THE PUD PROJECT. SOME VARIATION CAN BE CONSIDERED WITH EACH DEVELOPMENT PLAN.
10. AN EMPHASIS ON TALLER BUILDING HEIGHTS SHALL BE PROVIDED FOR LOW-MEDIUM DENSITY RESIDENTIAL UNITS THAT FRONT PRIMARY STREETS.

ZONING OF ADJACENT PROPERTIES (1"=500')



**PUD CONCEPTUAL PLAN APPROVAL**

REVIEWED BY THE PLAN & ZONING COMMISSION ON JULY 6, 2017, AND JULY 20, 2017.

REVIEWED BY THE CITY COUNCIL ON JULY 24, 2017 (ROLL CALL NUMBER 17-1285), AND AUGUST 2, 2017 (ROLL CALL NUMBER 17-1307).

APPROVED BY THE CITY COUNCIL ON AUGUST 14, 2017, BY ROLL CALL NUMBER 17-1407. ORDINANCE NO. 15,000.

IN ACCORDANCE WITH SECTION 134-895 OF THE DES MOINES CODE, AS AMENDED.

DATE \_\_\_\_\_ PLANNING DIRECTOR \_\_\_\_\_

I HEREBY CERTIFY THAT THIS ENGINEERING DOCUMENT WAS PREPARED BY ME OR UNDER MY DIRECT PERSONAL SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF IOWA.

RYAN A. HARDISTY, P.E. DATE \_\_\_\_\_

BY LICENSE NUMBER DATE IS DECEMBER 31, 2023 PAGE OR SHEETS COVERED BY THIS SEAL SHEETS 1 & 6

DATE: \_\_\_\_\_

REVISIONS: \_\_\_\_\_

4121 NW URBANDALE DRIVE URBANDALE, IOWA 50322 PHONE: (515) 369-4400 FAX: (515) 369-4410

TECH: \_\_\_\_\_

ENGINEER: \_\_\_\_\_

CIVIL DESIGN ADVANTAGE

DES MOINES, IOWA

**GRAY'S STATION**

PLANNED UNIT DEVELOPMENT (COVER SHEET)

1/2

2202.113

# Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa

## Sheet 2A of 20



### DEVELOPMENT NARRATIVE:

The vision for Gray's Station is that of a vibrant and vital neighborhood where people enjoy a unique and essential combination of urban and natural amenities every day. With a focus on wellness the Gray's Station neighborhood will be built on a green framework that promotes the safe movement of pedestrians and bicyclists over the efficient movement of motorists. Physical and social connection among all people is fostered through the integration of lush greenways, ample neighborhood parks, and intimate passages throughout the neighborhood.

Long-time residents and those passing through on their way to Gray's Lake or Downtown Des Moines will stroll and bike along a naturalized detention area that has been engineered to clean and cool the stormwater that falls within the watershed that encompasses the Western Gateway area of Downtown and the Neighborhood. While this feature will be skillfully designed, most of those who linger along its edge and enjoy the summer evening song of the wealth of wildlife who call it home will believe it is a wetland that Gray's Station was delicately built around.

Gray's Station will be known as the neighborhood with the best views in town. The golden dome of the Iowa State Capitol in the morning, the verdant wetland, Raccoon River, and Gray's Lake throughout the day, and the striking Downtown Skyline at night will all contribute to this reputation. Whether walking on foot, riding on a bike, in a bus, or in a car each road for mobility has been laid out to provide optimal aesthetic views to points of interest that are both urban and natural. Whether in your first floor living room or gazing out of your home many stories above the plaza below there will always be something to catch your eye.

The abundance of walking paths, sidewalks, and bike facilities within the neighborhood combined with the strong connections to the Downtown and Gray's Lake will ensure a constant flow of people on foot and bike. This stream of children, women, and men will ensure interactions between people of all kinds and that is as it should be. These interactions mixed with the many positive attributes of the neighborhood will imbue Gray's Station with the sense of meaning necessary to solidify it as the Place for people who want to live well in Des Moines.

### LAND DEDICATION & ANTICIPATED LONG TERM MAINTENANCE:

- Enhanced Stormwater Basin
  - o Dedicated to City of Des Moines;
  - o Maintained by City of Des Moines.
- Raccoon River Levy and Shoreline
  - o Dedicated to City of Des Moines;
  - o Maintained by City of Des Moines.
- Pedestrian Bridge Landing Area
  - o Dedicated to City of Des Moines;
  - o Maintained by City of Des Moines.
- Pedestrian Bridge Landing Plaza
  - o Maintained by Commercial Association.
- Neighborhood Parks
  - o Maintained by Home Owner's Association
- Neighborhood Greenways
  - o Maintained by Home Owner's Association.

### PLAN NARRATIVE & DESIGN GUIDELINES

OCTOBER 19, 2022

### DESIGN GUIDELINES

- L. Overall Neighborhood Character**
- A. Development shall establish a new "township" mixed use urban neighborhood including medium density residential, high density residential, and mid-rise multi-family mixed use. The new neighborhood shall be located at the intersection of 507 1/2 Street and Martin Luther King Parkway shall accommodate residential and commercial uses.
  - B. Development shall create a sense of neighborhood and identity for the new neighborhood to be located in the downtown.
  - C. The neighborhood shall connect to a pedestrian bridge and landing park to Gray's Lake.
  - D. The development shall utilize methods to minimize the impact on existing infrastructure, including but not limited to existing street crossings, power lines, water mains, sewer sewer mains, storm sewers, and down water basins and electrical mechanics.
  - E. Development shall use public and private partnerships to modify existing storm water basins and turn them into public amenities located on water quality and site engagement.
  - F. Housing product diversity in all forms and scales that are appropriate in an urban setting is encouraged. These shall be varied urban densities with complete forms in a limited neighborhood, encouraging a mix of housing.
  - G. Detailed urban formwork shall be considered along green space areas and neighborhood park areas and be limited to the address as detailed on the Conceptual Illustrative Master Plan on Street X.
  - H. Green corridors are encouraged to minimize impact of storm water, provide urban pathways for wildlife, and provide recreation, wellness, and pedestrian scale mobility.
  - I. Development design shall encourage proximity of uses to each other to create an urban form.
  - J. Development shall provide an opportunity for housing that could include potential duplex, large units, small parks, and greenways, ways for living spaces, and other landscape amenities.
  - K. Housing that offers views towards the river, waterfront park, Gray's Lake, and downtown is encouraged.
  - L. Development shall provide street landscaping to encourage walking and cycling over a road network for motor vehicles.
  - M. Development shall create a unique place-making identity for the neighborhood through signage design both the private and public realm.
  - N. All elements and phases shall relate to, and integrate with, each other and previous components.
  - O. Development shall establish vehicular, mass transit, bicycle, and pedestrian linkages with the following:
    1. Downtown Central Business District;
    2. Gray's Lake;
    3. Development of the GDCO rail;
  - P. Each project phase/development shall provide consistency and compatibility of character across the development.
- II. Street Character**
- A. All components of Development shall provide a unified streetscape system that utilizes the same materials, design elements, and equipment throughout the redevelopment area. Motor vehicles are allowed from street to street to create street identity or highlight local areas of the plan like the streets along the Pedestrian Bridge Landing Plaza, but the overall design of each street shall still contribute to a neighborhood identity.
  - B. Development shall provide a street system primarily related to downtown street grid that provides safety and ease of use by vehicular users.
    1. Public streets shall have curb-trees or right-of-way trees.
    2. Development shall include landscaped sidewalks, street lighting, and other amenities.
    3. Development shall provide a streetscape system that is walkable and that provides safety, comfort, and convenience for pedestrians.
  - C. Development shall create a pedestrian-friendly environment along Tule and 507 1/2 Streets that provides clear connections between the residential, mixed-use, and commercial uses.
  - D. Street design and construction shall be consistent with the approved Conceptual Development Plan and the Planned Unit Development (PUD) - Conceptual Plan Submittal - Sheet 12, Conceptual Street & Alley Sections.
    1. Street cross sections may vary where existing or planned utility structures will constrain under/over/both.
- III. Building Character**
- A. Buildings shall be developed for each project phase/development and provide consistency and compatibility of character across the development while providing flexibility in building design and building materials.
    1. Building materials in areas A, B, H, O, W, X, Y, and Z shall be durable materials such as brick, metal, stone, cement board, etc.
    2. Exterior insulation systems shall not be used.
    3. Industrial pre-engineered buildings shall not be allowed.
    4. Heavy mechanical equipment shall be enclosed or otherwise screened from public rights of way.
    5. A minimum of 75% of the surface area (inclusive of windows and doors) of building facade and proportional to a public street must be glass, brick, concrete panels, architectural concrete blocks (such as perforated blocks), and/or natural stone panels or stone. Other materials or wood panels are also acceptable, except if prohibited.
  - B. Building Height, Placement, Density, Arrangement (See approved Conceptual Development Plan for Land Use Classifications)
    - A. Primary facades of mid-rise and commercial buildings shall be primarily parallel to the public right-of-way except in the address 407 and X.
    - B. Open space between building facades along Tule Street shall include a human scale.
    - C. Decorative metal framing shall be allowed within defined residential areas.
    1. Chain-link fencing shall not be allowed.
    2. Wood fencing shall not be allowed.
    3. Wood Fencing shall not be allowed at grade or on setbacks except for an second in a fence or wall.
    - D. Attached and Detached Residential uses shall have minimal setbacks from public streets to encourage neighborhood character and define the streetscape edge.
    - E. Attached and Detached Residential: Side and Corner Lots
      1. Lots with side elevations at street corners should include enhancements to foundation and details defined from side elevations interior to the block.
      2. Lots with side elevations at street corners, public streets or open spaces should include enhancements to foundation, eaves and roof lines or existing details from side elevations interior to the block and detail from alley corner side elevations.
    - F. Detached Residential uses on narrow lots shall have minimal side yard setbacks from adjacent structures to promote density and define the edge of the public realm.
    - G. Building Height
      1. Downtown Mixed Use
        - a) Maximum height for all uses shall be four stories.
      2. High Density Residential
        - a) Maximum height for all uses shall be as follows:
          - (1) Three stories for Subareas B, H, O, and Z
          - (2) Eight stories for Subarea Y and Z
        - b) Maximum height for all uses shall be one story except as noted in the Conceptual Illustrative Master Plan (Sheet 12).
    - H. Building Setbacks
      1. For all residential buildings, a minimum setback of 15 feet from the public right-of-way is permitted unless constrained by utility easements.
      2. Medium Density Residential
        - a) Attached and Detached residential buildings shall have side yard building separation (i.e., building face to building face) that are at minimum 5 feet and at maximum 15 feet except where walking paths exist between buildings.
        - (1) Where walking paths exist between buildings the minimum with between buildings shall increase across the maximum separation by the width of the walking path.
  - I. Building Entrances
    1. Downtown Mixed Use
      - a) Buildings shall have entrances oriented toward primary street(s) and public plaza(s).
- IV. Building Frontage**
1. At least one building entrance for residential uses shall directly access the street or public plaza when a residential use is located above street-level retail or commercial uses.
  2. High Density Residential
    - a) Buildings shall have entrances oriented toward primary street(s) and public plaza(s).
    - b) Buildings shall have entrances oriented toward primary street(s), public plaza(s), park, greenway or alley.
  3. Medium Density Residential
    - a) Buildings shall have entrances oriented toward primary street(s), public plaza(s), park, greenway or alley.
- V. Building Frontage**
1. Building Frontage
    1. Building frontage calculations exclude the length of common alleys serving multiple properties, re-landscaped planting areas, and utility corridors. In Medium Density Residential areas, pedestrian connections between buildings shall also be included in building frontage calculations.
    2. Downtown Mixed Use
      - a) All buildings shall have frontage on principal street(s), public plaza, park or greenway or not less than 75 percent of the lot with the following exceptions:
        - (1) Frontage is used for an existing utility corridor.
        - (2) The curvature of the road precludes efficient frontage.
        - (3) Where exceptions 2.2.1 and 2.2.2 preclude a building from being one foot from the property line, the design of the buildings shall seek a similar design intent through building and site design to frame the street/ally way and create an engaging pedestrian environment.
        - b) For commercial and non-residential buildings, at least 10 percent of the building frontage shall be within one foot of the property line with the following exceptions:
          - (1) Frontage is used for a place or an existing utility corridor.
          - (2) The curvature of the road precludes efficient frontage along the property line.
          - (3) Where exceptions 2.2.1 and 2.2.2 preclude a building from being one foot from the property line, the design of the buildings shall seek a similar design intent through building and site design to frame the street/ally way and create an engaging pedestrian environment.
    2. High Density Residential
      - a) All buildings shall have frontage on principal street(s), public plaza, park or greenway or not less than 75 percent of the lot with the following exceptions:
        - (1) Frontage is used for a place or an existing utility corridor.
        - (2) The curvature of the road precludes efficient frontage along the property line.
        - (3) Where exceptions 2.2.1 and 2.2.2 preclude a building from being one foot from the property line, the design of the buildings shall seek a similar design intent through building and site design to frame the street/ally way and create an engaging pedestrian environment.
    3. Medium Density Residential
      - a) All buildings shall have frontage on principal street(s), public plaza, park, greenway or alley or not less than 75 percent of the lot with the following exceptions:
        - (1) Frontage is used for an existing utility corridor.
        - (2) The curvature of the road precludes efficient frontage along the property line.
        - (3) Where exceptions 2.2.1 and 2.2.2 preclude a building from being one foot from the property line, the design of the buildings shall seek a similar design intent through building and site design to frame the street/ally way and create an engaging pedestrian environment.

**VI. Signs (See approved Conceptual Development Plan for Land Use Classifications)**

    - A. Definitions
      1. Sign means any structure, including but not limited to a device or display, other than buildings or landscaping, used primarily for visual communication for the purpose of or being the result of changing the subject thereof to the attention of a person, group of persons, or the public generally. The term "sign" includes but is not limited to any and all existing, printed, letters, numerals, pictorial representations, symbols, trademarks, logos, and patterns, whether affixed to a building, structure or otherwise displayed on a building, or separate from any building, including but not limited to a sign that is a public identification or commercial advertisement on any sign on which commercial advertising is prohibited.
      2. Sign face means the flat area contained within the limits of a sign, provided however, that the area of a sign containing back-lit neon signs or other signs with an internal angle of 45 degrees or less, attached to a single supporting structure, shall be the area of the larger supporting sign face. The area of a sign composed of numerous or multiple attached signs to a building or wall surface is the smallest rectangle which encloses the signs.
      3. Sign, building or structure means a sign, other than a real sign, which is supported by a building or wall, and is further defined as follows:
        - a) Sign, three means a sign-face building or wall sign which is supported by its supporting wall.
        - b) Sign, projecting means a building or wall sign other than a back sign.
        - c) Sign, directional means a sign for the purpose of traffic control which is located on private property.
        - d) Sign, directional means a sign containing the name of a building, complex or center and its main classification.
        - e) Sign, directional means a sign containing the name of a building, complex or center and its main classification.
        - f) Sign, directional means a sign containing the name of a building, complex or center and its main classification.
        - g) Sign, directional means any system of a sign space which delineates, points, guides or symbolically communicates information to change or direct an existing system (except of sign building structure, GDCO, street signs, traffic signs or other transportation signs, including but not limited to programmable microprocessor controlled electronic displays), and the projection of an image or message with these characteristics by any other means onto the sign face.
        - h) Sign face means all of the surface of a sign used or designed for the presentation of a single visual display, including letters, numbers and text, and including the supporting structure. Each separate surface of a sign used or designed for the presentation of a separate visual display shall constitute a separate sign face.
        - i) Sign, wall-mounted means a sign not attached to any building and is further defined as follows:
          - a) Sign, wall-mounted means a sign affixed to a structure built on grade, having a solid support base, constructed of brick, stone, concrete block or other durable masonry including the exterior of the building and extending from grade to the bottom of the sign face across the entire width of the sign face. The height of the sign face shall be no less than the height of the lot or 25 percent of the total sign height, whichever is greater, unless otherwise indicated in this chapter or non-municipal signs.
          - b) Sign, wall-mounted means a sign not attached to any building and is further defined as follows:
            - a) Sign, wall-mounted means a sign affixed to a structure built on grade, having a solid support base, constructed of brick, stone, concrete block or other durable masonry including the exterior of the building and extending from grade to the bottom of the sign face across the entire width of the sign face. The height of the sign face shall be no less than the height of the lot or 25 percent of the total sign height, whichever is greater, unless otherwise indicated in this chapter or non-municipal signs.
      - B. Sign, wall-mounted means a sign affixed to a structure built on grade, having a solid support base, constructed of brick, stone, concrete block or other durable masonry including the exterior of the building and extending from grade to the bottom of the sign face across the entire width of the sign face. The height of the sign face shall be no less than the height of the lot or 25 percent of the total sign height, whichever is greater, unless otherwise indicated in this chapter or non-municipal signs.
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      - E. Sign, wall-mounted means a sign affixed to a structure built on grade, having a solid support base, constructed of brick, stone, concrete block or other durable masonry including the exterior of the building and extending from grade to the bottom of the sign face across the entire width of the sign face. The height of the sign face shall be no less than the height of the lot or 25 percent of the total sign height, whichever is greater, unless otherwise indicated in this chapter or non-municipal signs.
      - F. Sign, wall-mounted means a sign affixed to a structure built on grade, having a solid support base, constructed of brick, stone, concrete block or other durable masonry including the exterior of the building and extending from grade to the bottom of the sign face across the entire width of the sign face. The height of the sign face shall be no less than the height of the lot or 25 percent of the total sign height, whichever is greater, unless otherwise indicated in this chapter or non-municipal signs.
      - G. Sign, wall-mounted means a sign affixed to a structure built on grade, having a solid support base, constructed of brick, stone, concrete block or other durable masonry including the exterior of the building and extending from grade to the bottom of the sign face across the entire width of the sign face. The height of the sign face shall be no less than the height of the lot or 25 percent of the total sign height, whichever is greater, unless otherwise indicated in this chapter or non-municipal signs.
      - H. Sign, wall-mounted means a sign affixed to a structure built on grade, having a solid support base, constructed of brick, stone, concrete block or other durable masonry including the exterior of the building and extending from grade to the bottom of the sign face across the entire width of the sign face. The height of the sign face shall be no less than the height of the lot or 25 percent of the total sign height, whichever is greater, unless otherwise indicated in this chapter or non-municipal signs.
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      - K. Sign, wall-mounted means a sign affixed to a structure built on grade, having a solid support base, constructed of brick, stone, concrete block or other durable masonry including the exterior of the building and extending from grade to the bottom of the sign face across the entire width of the sign face. The height of the sign face shall be no less than the height of the lot or 25 percent of the total sign height, whichever is greater, unless otherwise indicated in this chapter or non-municipal signs.
      - L. Sign, wall-mounted means a sign affixed to a structure built on grade, having a solid support base, constructed of brick, stone, concrete block or other durable masonry including the exterior of the building and extending from grade to the bottom of the sign face across the entire width of the sign face. The height of the sign face shall be no less than the height of the lot or 25 percent of the total sign height, whichever is greater, unless otherwise indicated in this chapter or non-municipal signs.
      - M. Sign, wall-mounted means a sign affixed to a structure built on grade, having a solid support base, constructed of brick, stone, concrete block or other durable masonry including the exterior of the building and extending from grade to the bottom of the sign face across the entire width of the sign face. The height of the sign face shall be no less than the height of the lot or 25 percent of the total sign height, whichever is greater, unless otherwise indicated in this chapter or non-municipal signs.
      - N. Sign, wall-mounted means a sign affixed to a structure built on grade, having a solid support base, constructed of brick, stone, concrete block or other durable masonry including the exterior of the building and extending from grade to the bottom of the sign face across the entire width of the sign face. The height of the sign face shall be no less than the height of the lot or 25 percent of the total sign height, whichever is greater, unless otherwise indicated in this chapter or non-municipal signs.
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      - P. Sign, wall-mounted means a sign affixed to a structure built on grade, having a solid support base, constructed of brick, stone, concrete block or other durable masonry including the exterior of the building and extending from grade to the bottom of the sign face across the entire width of the sign face. The height of the sign face shall be no less than the height of the lot or 25 percent of the total sign height, whichever is greater, unless otherwise indicated in this chapter or non-municipal signs.
      - Q. Sign, wall-mounted means a sign affixed to a structure built on grade, having a solid support base, constructed of brick, stone, concrete block or other durable masonry including the exterior of the building and extending from grade to the bottom of the sign face across the entire width of the sign face. The height of the sign face shall be no less than the height of the lot or 25 percent of the total sign height, whichever is greater, unless otherwise indicated in this chapter or non-municipal signs.
      - R. Sign, wall-mounted means a sign affixed to a structure built on grade, having a solid support base, constructed of brick, stone, concrete block or other durable masonry including the exterior of the building and extending from grade to the bottom of the sign face across the entire width of the sign face. The height of the sign face shall be no less than the height of the lot or 25 percent of the total sign height, whichever is greater, unless otherwise indicated in this chapter or non-municipal signs.
      - S. Sign, wall-mounted means a sign affixed to a structure built on grade, having a solid support base, constructed of brick, stone, concrete block or other durable masonry including the exterior of the building and extending from grade to the bottom of the sign face across the entire width of the sign face. The height of the sign face shall be no less than the height of the lot or 25 percent of the total sign height, whichever is greater, unless otherwise indicated in this chapter or non-municipal signs.
      - T. Sign, wall-mounted means a sign affixed to a structure built on grade, having a solid support base, constructed of brick, stone, concrete block or other durable masonry including the exterior of the building and extending from grade to the bottom of the sign face across the entire width of the sign face. The height of the sign face shall be no less than the height of the lot or 25 percent of the total sign height, whichever is greater, unless otherwise indicated in this chapter or non-municipal signs.
      - U. Sign, wall-mounted means a sign affixed to a structure built on grade, having a solid support base, constructed of brick, stone, concrete block or other durable masonry including the exterior of the building and extending from grade to the bottom of the sign face across the entire width of the sign face. The height of the sign face shall be no less than the height of the lot or 25 percent of the total sign height, whichever is greater, unless otherwise indicated in this chapter or non-municipal signs.
      - V. Sign, wall-mounted means a sign affixed to a structure built on grade, having a solid support base, constructed of brick, stone, concrete block or other durable masonry including the exterior of the building and extending from grade to the bottom of the sign face across the entire width of the sign face. The height of the sign face shall be no less than the height of the lot or 25 percent of the total sign height, whichever is greater, unless otherwise indicated in this chapter or non-municipal signs.
      - W. Sign, wall-mounted means a sign affixed to a structure built on grade, having a solid support base, constructed of brick, stone, concrete block or other durable masonry including the exterior of the building and extending from grade to the bottom of the sign face across the entire width of the sign face. The height of the sign face shall be no less than the height of the lot or 25 percent of the total sign height, whichever is greater, unless otherwise indicated in this chapter or non-municipal signs.
      - X. Sign, wall-mounted means a sign affixed to a structure built on grade, having a solid support base, constructed of brick, stone, concrete block or other durable masonry including the exterior of the building and extending from grade to the bottom of the sign face across the entire width of the sign face. The height of the sign face shall be no less than the height of the lot or 25 percent of the total sign height, whichever is greater, unless otherwise indicated in this chapter or non-municipal signs.
      - Y. Sign, wall-mounted means a sign affixed to a structure built on grade, having a solid support base, constructed of brick, stone, concrete block or other durable masonry including the exterior of the building and extending from grade to the bottom of the sign face across the entire width of the sign face. The height of the sign face shall be no less than the height of the lot or 25 percent of the total sign height, whichever is greater, unless otherwise indicated in this chapter or non-municipal signs.
      - Z. Sign, wall-mounted means a sign affixed to a structure built on grade, having a solid support base, constructed of brick, stone, concrete block or other durable masonry including the exterior of the building and extending from grade to the bottom of the sign face across the entire width of the sign face. The height of the sign face shall be no less than the height of the lot or 25 percent of the total sign height, whichever is greater, unless otherwise indicated in this chapter or non-municipal signs.

# Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa

## Sheet 2B of 20



15. Sign, on-premise advertising means a sign which identifies or directs attention to:
- A profession, business, service, industry, product, campaign or attraction manufactured, sold, offered or conducted upon the premises where such sign is located, and/or
  - The identity of or noncommercial views of an occupant of the premises.
17. Sign, on-premise a sign erected upon or above a roof or part of a building.
18. Sign, sandwich board means a sign with two faces that are adjacent at the top edge displayed at an angle, which is not permanently attached or secured.
19. Sign, service means a sign identifying restaurants and other service facilities.
20. Sign, temporary means a sign identified for a limited period of display.
21. Sign, type A means a sign which shall conform to all aspects with the following:
- It shall be either nonilluminated or indirectly lighted with nonflashing white light.
  - It shall have not more than two sign faces, each sign faces to be parallel.
22. Sign, type B means a sign which shall conform to all of the requirements for type A sign, except that type B signs may be indirectly illuminated.
23. Sign, type C includes type A and type B signs and shall conform to all aspects with the following:
- It may contain exposed pipes or duct gas lines or any combination thereof, provided that the exposed pipes does not exceed 11 inches in the best gas take does not exceed 20 inches. Where inside a building lamps or exposed lamps with a diffusing screen are used, no lamp shall exceed 22 watts.
  - Identification of illumination by individual lamps or by external reflectors is prohibited.
24. Sign, window means a sign within a building which is located and placed so as to enhance an interest for the sign to be viewed from a public right-of-way.
- B. Overhead Street Sign High Density Residential Signs**
1. Signs shall be in accordance with the following:
- Any combination of two or more type A, type B or type C Identification enable advertising signs per occupant of a building not to exceed one and one-half square feet in area per linear foot of building footage per occupant, and not to exceed a minimum of 100 square feet per occupant, shall be permitted. If building footage consists of 25 percent or less of the total lot footage, the occupant or occupants of that building shall be permitted, in lieu of the above, one square foot in area per linear foot of building per lot and not to exceed a minimum of 100 square feet per occupant. Such signs shall be permitted on public or private concrete attached to and supported by a building.
  - All building or wall signs shall project no more than seven feet from the building, unless such projection is over the street right-of-way, in which case such sign shall project no more than 16 inches. Such projecting signs shall be not more than ten feet from the ground in areas in which there is no pedestrian traffic and not less than 15 feet from the ground in areas in which there is vehicular traffic. Signs shall be not more than 28 square feet in area per occupant and shall not extend more than four feet above the roofline. Projecting signs may be anchored to the roof of building.
  - The permitted types of free-standing signs are noncommercial and sandwich board signs. Pole signs and portable signs other than sandwich board signs are not permitted.
  - One sandwich board sign shall be permitted per building front for identification and newspaper advertising. Such signs shall comply with signage and space requirements outlined in the city building code. Sandwich board signs are permitted upon the sidewalk within the public right-of-way, subject to the following:
    - Such signs are permitted only during the hours the business is open to the public, and must be located adjacent to the premises.
    - Such signs shall be placed no closer than two feet from the back of the street curb.
    - Such signs shall be no more than five square feet in area on each side, no more than five feet in height, and no more than three feet in width.
    - There shall be no electrical service to such signs.
  - One non-illuminated building or wall identification sign per occupant, not to exceed two square feet in area, shall be permitted on buildings where occupants have no occupant footage, and shall be located immediately adjacent to the principal entrance to said occupant's premises.
  - Two non-illuminated type B identification signs shall be permitted on building marquees. The total sign area per marquee shall not exceed 100 square feet.

- C. Medium Density Residential Signs**
- Signs shall be in accordance with the following:
- One sign per type A, type B or type C Identification enable advertising signs per occupant for each of the following uses: Single-family detached dwellings, supervised group residences, and family farms.
  - One sign per type A Identification sign not to exceed 24 square feet in area per sign face shall be permitted for each lot located for the following uses: churches, educational facilities, and similar places of worship; public and parochial schools (including boarding schools, nursery schools, and child care centers); city-owned facilities; and publicly owned parks, playgrounds, golf courses, and recreation areas.
  - One non-illuminated building board, indirectly or directly illuminated, not to exceed 24 square feet in area, shall be permitted per residential. Its only sign shall extend higher than eight feet above ground level.
  - One sign per type A Identification sign not to exceed one square foot in area shall be permitted for each of the two dwelling units of a two-family residence.
  - One sign per type B Identification enable advertising sign per lot per lot, not to exceed four square feet in area per sign face, shall be permitted for each multiple dwelling.
  - In addition to the signs permitted in subsections 1) through 4) of this section, a home occupation may be permitted one additional square foot of signage.
  - In lieu of the Identification sign and the non-illuminated building board, a church or school may combine into one sign type A or type B Identification sign and a non-illuminated building board. The sign may be indirectly or directly illuminated. It shall not exceed 72 square feet per sign face nor extend more than eight feet above the ground. It shall not be located in any required front or side yard.

**VI. Parking Requirements**

- A. Nonresidential areas**
- Surface parking spaces shall not exceed 2.75 spaces per 1000 GSF of commercial area.
  - Surface parking shall have an emphasis of trees, shrubs and other plantings placed around the perimeter of any parking area and within the parking lot to create a more attractive area. The landscape plan shall generally enhance the visual appearance of the building, parking area and any pedestrian areas. Design of surface parking landscape enhancements to be reviewed by the Planning & Zoning Commission and City Council.
  - Vehicular use areas with 45 or more one vehicle parking spaces shall be detailed below:
    - Interior landscaping shall include a minimum variety of one planting bed no smaller than 8' x 17' for every 15 interior spaces containing 1 necessary tree and 2 shrubs.
    - Required landscaping and planting beds may be eliminated but there may be no more than 25 spaces in a single row, or 50 spaces in a double row without an intervening planting bed.
    - Trees planted in Parking Lot Islands (Planting Islands) shall be a minimum of 1 1/2" caliper.
    - One necessary tree is required to increase shade and reduce "urban heat island" effect.
    - A minimum of one (1) necessary tree and three (3) shrubs for every ten (10) spaces exclusive of tree planting required in perimeter landscape area.
    - Corner islands beyond the 7' perimeter landscape sign may be omitted behind interior planting requirements except they are contiguous with perimeter landscape areas.
    - The 7' perimeter landscape area may be omitted behind interior planting requirements.
    - Perimeter landscape areas in excess of 7' may partially be omitted behind interior planting requirements.
    - Interior planting islands that are at least 11'x17' shall be spaced no farther than 20 parking spaces or 100' apart.
  - Vehicular use areas require perimeter landscape area as detailed below:
    - Vehicular use areas holding up to 28 cars require a 5' perimeter landscape area (7' car overhang, 5' landscape area) measured from the right-of-way line along parking lot facing public right-of-way. (Public land cannot be counted toward fulfillment of this obligation).
    - Two non-illuminated type B identification signs shall be permitted on building marquees. The total sign area per marquee shall not exceed 100 square feet.

- Vehicular use areas of 40 car stalls and over require a 7' perimeter landscape area (7' car overhang, 5' landscape area) measured from the right-of-way line along parking lot facing public right-of-way. (Public land cannot be counted toward fulfillment of this obligation).
  - Roof edge of concrete or stone shall be a minimum of 6" in height and a minimum of 6" in width, and shall be securely installed around the perimeter landscape area and be maintained in good condition.
  - The landscape area shall contain 1 tree and 10 shrubs (24" DBH maximum mature minimum height), and be no less than 10' tall per 40 linear feet of perimeter landscape area with proportion of a diverse mix of trees mostly of dense, dark, high-lower canopy thick, cultivated and/or native plants, shrubs, ground cover, or a combination of these materials, located at the lot line. Wood is not allowed except for an accent material. Chain link fences are not allowed. Walls shall be no taller than 30", and no steeper than 24".
  - Groundcover or low planting is required in the car overhang area where maintenance will be provided. Ornamental grass, or landscape treated mulch, is required.
  - Trees shall be a minimum of every 50' throughout the perimeter landscape area.
  - Trees shall be planted a minimum of 20' from any curb.
  - Unexcused tree spacing is acceptable with groupings of lighter spacing and larger openings between groupings.
  - The dogging or shearing of perimeter areas with parking trees is encouraged.
  - Trees planted in the perimeter landscape area are a separate requirement, and do not count toward other tree planting or the parking lot interior planting.
- 5. Parking lots adjacent to public streets shall be screened by walls, fences, hedges, landscaping or a combination thereof. Two paved surfaces shall be landscaped to meet the Minimum Landscape Standards for parking facilities C-1, C-2A, Residential Access**
- B. Street Designated as "Local" or "Collector" shall include parallel parking as depicted in the approved conceptual development plan.**
- C. Structured Parking**
- Facilities of three or more parallel parking (e.g., podium parking beneath commercial or residential areas) adjacent to any public right-of-way shall be substantially and aesthetically consistent with the remainder of the building they support.
- VI. Mass Transit Integration**
- Development shall accommodate long term transit connections within and between neighborhoods.
  - The Des Moines Area Regional Transit (DART) Authority shall review all development plans and design/landscape for transit stops and shelters.
  - Developers and DART shall work together to establish feasible transportation linkages on a case by case basis.
- VII. Public amenities**
- A. Streets and Pedestrian Facilities:**
- Development shall provide on-street bike lanes and off-street trails that connect to existing urban system.
  - Development shall include a Pedestrian Bridge connection to Gray's Lake over the Ramona River.
  - Pedestrian Bridge is anticipated to be located near the southeast corner of the identified PUD boundary and west of the existing pump station associated with the enhanced stormwater basin.
  - Development shall provide the inclusion of bicycle sharing services (e.g., B-Cycle), schedules along streets, to enhance open space areas, and between buildings shall maintain a minimum width of 10 feet.
- B. Public Open Space**
- Greenways
    - Greenways shall maintain an approximate width of 40' to 50'. Exceptions may occur where infrastructure requirements necessitate and will be reviewed with the Final Development Plan.
    - Greenways shall include landscaping.
    - Greenways shall include pedestrian pathways with a minimum width of 6'.
    - Greenways may include structural (e.g., Checkers, Chesses, Pergolas, etc.) and/or artistic elements where deemed appropriate by the developer and approved in the Final Development Plan.
  - Neighborhood Parks
    - Neighborhood Parks shall be generally located per the approved conceptual development plan.
    - Neighborhood Parks shall include landscaping.
    - Neighborhood Parks shall include pedestrian pathways with a minimum width of 6'.
    - Neighborhood Parks may include structural (e.g., Checkers, Chesses, Pergolas, etc.) and/or artistic elements where deemed appropriate by the developer.
  - Pedestrian Bridge Landing Plaza
    - The Pedestrian Bridge Landing Plaza shall generally be located as depicted on the approved conceptual development plan.
    - The Plaza shall be designed to accommodate pedestrian and bicycle traffic associated with daily use of the Pedestrian Bridge facility.
    - The Plaza shall be designed as an inviting public space that is feasible enough to accommodate neighborhood events.
  - Enhanced Stormwater Basin
    - The Enhanced Stormwater Basin (ESB) shall generally be located as depicted on the approved conceptual development plan on Sheet 2C.
    - The ESB shall be designed to include stormwater from the wetlands which it serves.
    - The ESB shall be designed to improve the quality of stormwater that flows through it.
    - The ESB shall be designed to be aesthetically pleasing.
    - The ESB shall be designed by the developer. The ESB design shall be approved by the City of Des Moines. ESB shall be constructed and owned by the City of Des Moines.

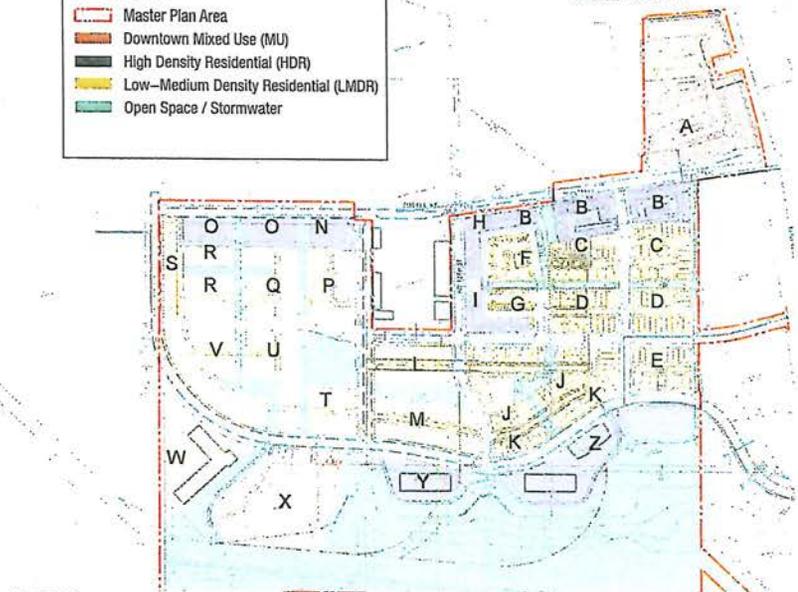
# Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa

## Sheet 3 of 20



**LEGEND**

- Master Plan Area
- Downtown Mixed Use (MU)
- High Density Residential (HDR)
- Low-Medium Density Residential (LMDR)
- Open Space / Stormwater



Principal Uses	Plan Subarea																											
	MU	HDR	MDR	MDR	MDR	MDR	MDR	HDR	MDR	MDR	MDR	MDR	HDR	HDR	MDR	MU	MU	HDR	HDR									
<b>Residential Use Category</b>																												
1-unit dwelling		X	X	X	X	X			X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
2-unit dwelling			X	X	X	X	X		X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
3-unit dwelling			X	X	X	X	X		X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Multi-unit dwelling (4+) units		X	X	X	X	X	X		X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Efficiency/Micro Units		X	X	X	X	X	X		X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Bed & Breakfast		X	X	X	X	X	X		X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Group Living		X	X	X	X	X	X		X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Group Living for Health Reasons		X	X	X	X	X	X		X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Hotel/Apartment Hotel		X	X	X	X	X	X		X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Residential Care, Large		X	X	X	X	X	X		X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Residential Care, Small		X	X	X	X	X	X		X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
<b>Civic &amp; Institutional Category</b>																												
Assembly, Neighborhood (<10,000 sf)		X	X						X					X	X										X	X	X	X
Assembly, General (≥10,000 sf)		X	X						X					X	X										X	X	X	X
Government/Higher Education/Hospital		X	X						X					X	X										X	X	X	X
Library/Museum		X	X						X					X	X										X	X	X	X
Police/Fire		X	X						X					X	X										X	X	X	X
Schools: Pre-K, elementary, intermediate		X	X	X	X	X	X		X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Transit Station		X	X	X	X	X	X		X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
<b>Open Space Category</b>																												
Community Garden		X	X	X	X	X	X		X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Intensive Park Uses		X	X	X	X	X	X		X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Park		X	X	X	X	X	X		X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Urban Farm		X	X	X	X	X	X		X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
<b>Retail Use Category</b>																												
Neighborhood Retail		X	X						X					X	X										X	X	X	X
General Retail		X	X						X					X	X										X	X	X	X
Beer/Wine/Liquor Sales		X	X						X					X	X										X	X	X	X
Convenience Store		X	X						X					X	X										X	X	X	X
<b>Service Use Category</b>																												
Neighborhood Service		X	X						X					X	X										X	X	X	X
General Service		X	X						X					X	X										X	X	X	X
Adult Day Care		X	X	X	X	X	X		X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Child Day Care (>12 children)		X	X						X					X	X										X	X	X	X
Community Service		X	X						X					X	X										X	X	X	X
Drinking Places		X	X						X					X	X										X	X	X	X
Eating Places		X	X						X					X	X										X	X	X	X
Entertainment Assembly		X	X	X	X	X	X		X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Private Club		X	X						X					X	X										X	X	X	X
Tattoo/Piercing Parlor		X	X						X					X	X										X	X	X	X
<b>Employment Use Category</b>																												
Office		X	X						X					X	X										X	X	X	X
Craftsman/Studio		X	X						X					X	X										X	X	X	X

X Permitted  
All uses not listed on this sheet are prohibited.

Accessory & Temporary Uses	Plan Subarea																											
	MU	HDR	MDR	MDR	MDR	MDR	MDR	HDR	MDR	MDR	MDR	MDR	HDR	HDR	MDR	MU	MU	HDR	HDR									
<b>Residential Accessory Use</b>																												
Accessory Dwelling Unit		X	X	X	X	X	X		X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Home Child Care		X	X	X	X	X	X		X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Home Occupation		X	X	X	X	X	X		X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Room Rental - Long-Term (≥ 3 months)		X	X	X	X	X	X		X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Room Rental - Short-Term (< 21 days)		X	X	X	X	X	X		X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
<b>Commercial Accessory Use</b>																												
Drive-Through Facility		X																										
Outdoor Café		X	X						X	X															X	X	X	X
Outdoor Display		X	X						X	X															X	X	X	X
Parking		X	X	X	X	X	X		X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
<b>Temporary Use Category</b>																												
Voting Place		X	X						X					X	X										X	X	X	X
Temporary Outdoor Event		X	X						X					X	X										X	X	X	X
Farmers Market		X	X						X					X	X										X	X	X	X

X Permitted  
All uses not listed on this sheet are prohibited.

PERMITTED LAND USES  
OCTOBER 19, 2022



Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
 Sheet 5 of 20



**GENERAL LEGEND**

	GROUND SURFACE CONTOUR
	SANITARY MANHOLE
	WATER VALVE BOX
	FIRE HYDRANT
	WATER CURB STOP
	WELL
	STORM SEWER MANHOLE
	STORM SEWER DOUBLE INTAKE
	FLARED END SECTION
	DECIDUOUS TREE
	DECIDUOUS SHRUB
	CONIFEROUS SHRUB
	ELECTRIC POWER POLE
	GUY ANCHOR
	STREET LIGHT
	POWER POLE W/ TRANSFORMER
	UTILITY POLE W/ LIGHT
	ELECTRIC BOX
	ELECTRIC TRANSFORMER
	ELECTRIC MANHOLE OR VAULT
	TRAFFIC SIGN
	TELEPHONE JUNCTION BOX
	TELEPHONE MANHOLE/VAULT
	TELEPHONE POLE
	GAS VALVE BOX
	CABLE TV JUNCTION BOX
	CABLE TV MANHOLE/VAULT
	MAIL BOX
	BENCHMARK
	SOIL BORING
	UNDERGROUND TV CABLE
	GAS MAIN
	FIBER OPTIC
	UNDERGROUND TELEPHONE
	OVERHEAD ELECTRIC
	UNDERGROUND ELECTRIC
	FIELD TILE
	SANITARY SEWER W/ SIZE
	STORM SEWER W/ SIZE
	WATER MAIN W/ SIZE

PLAN MAP:  
 EXISTING CONDITIONS  
 MAY 3, 2017

CIVIL DESIGN ADVANTAGE

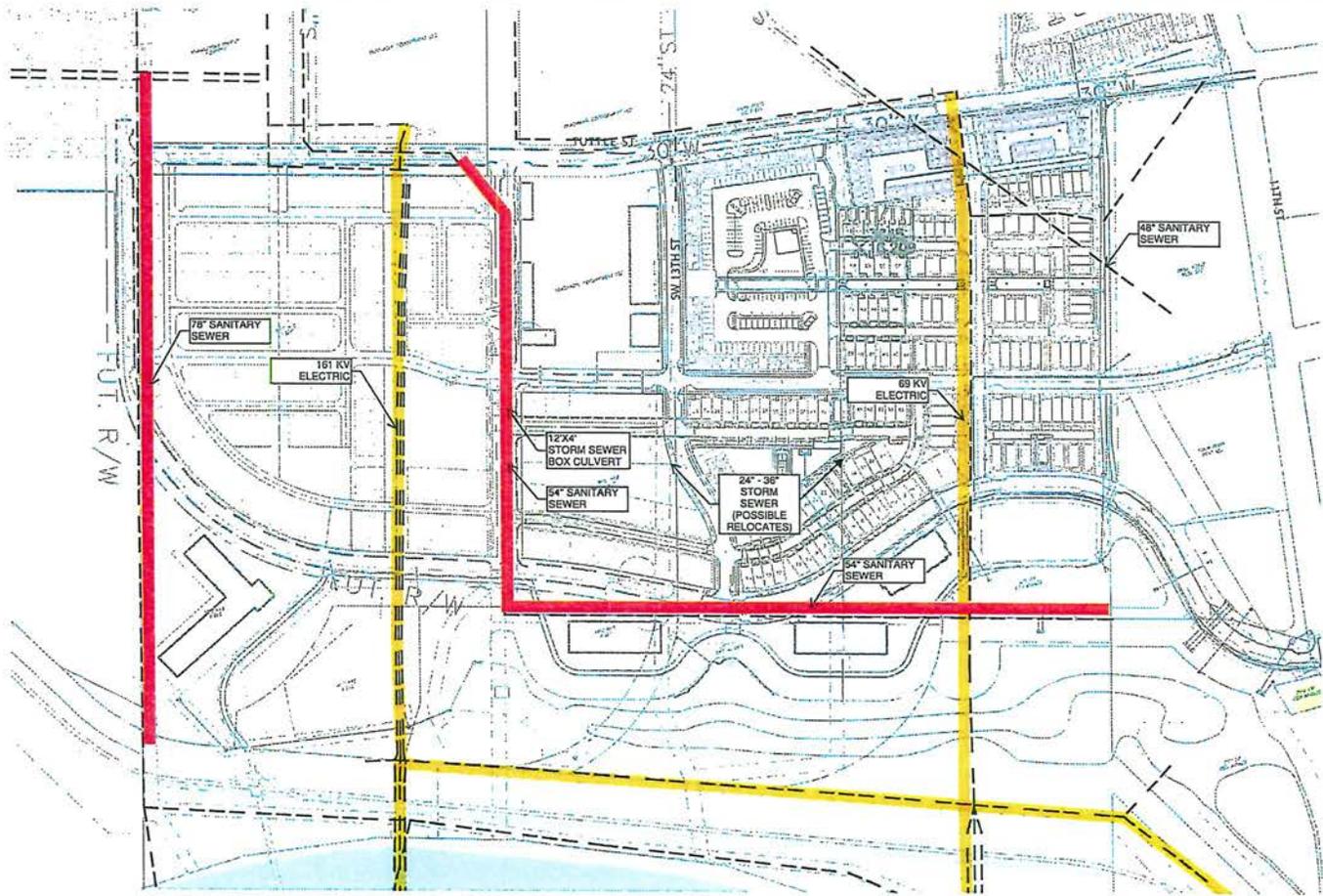
HUBBELL REALTY COMPANY

RDg...  
 PLANNING & DESIGN

N  
 SCALE  
 0 75 150 300'



Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
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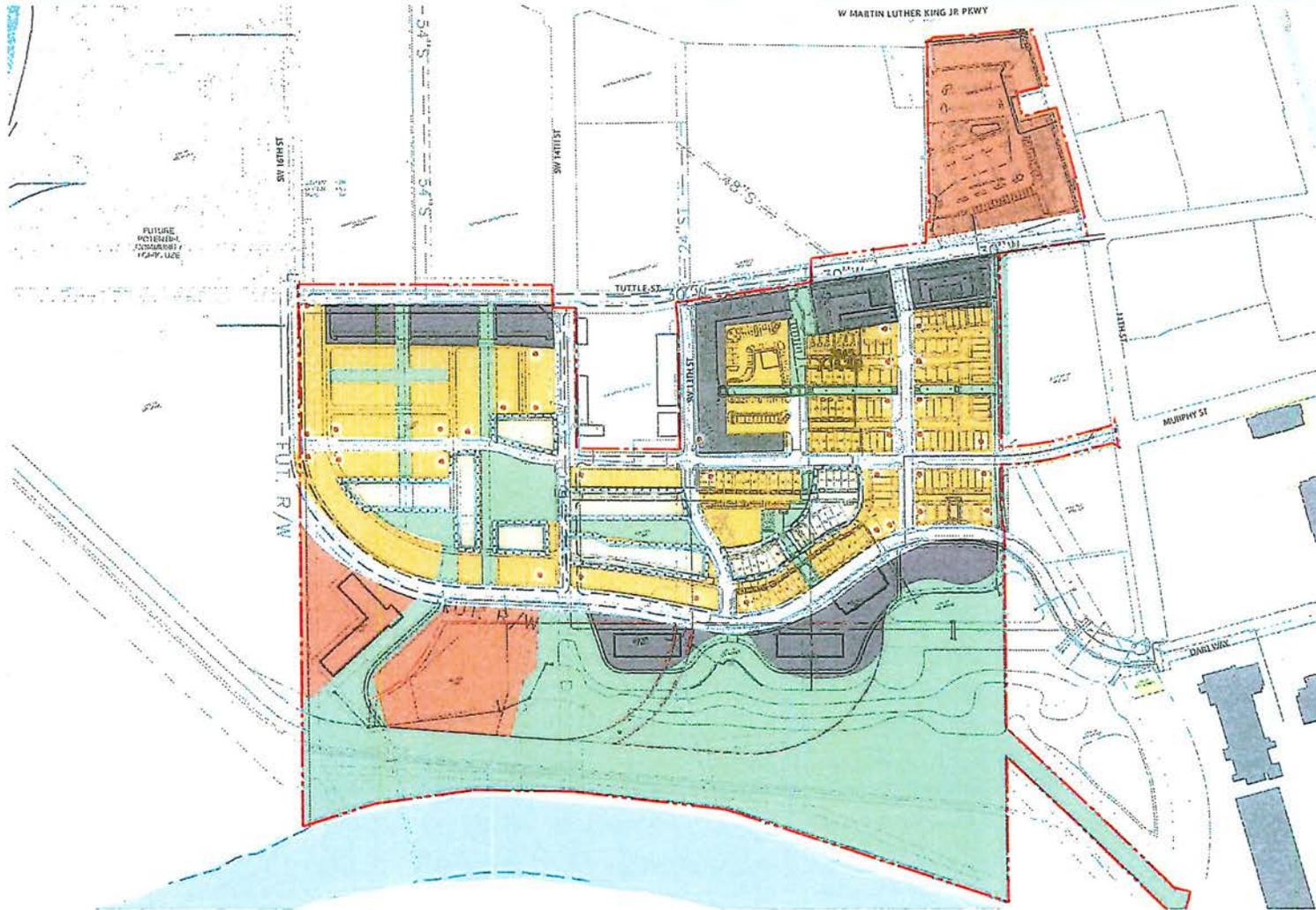
**LEGEND:**  
 ——— EXISTING SANITARY LINE TO REMAIN  
 ——— EXISTING OVERHEAD POWER LINES TO REMAIN

**NOTES:**  
 1. The conceptual plan takes into account significant existing infrastructure features that are currently infeasible to move based on economic factors. Unless economic conditions change, future development plans should respect and sensitively integrate the existing infrastructure highlighted in the adjacent diagram and attached legend.

EXISTING INFRASTRUCTURE CONSTRAINTS  
 OCTOBER 19, 2022



Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
 Sheet 8 of 20

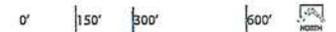


**NOTES:**  
 1. The master plan at left is for illustrative purposes only and is provided to show general, conceptual intent and character of the development. Individual parcels, building footprints, landscape design, open space design and parking design will be determined and designed as a part of future development plans for each phase.

**LEGEND**

- Master Plan Area
- Downtown Mixed Use (MU)
- High Density Residential (HDR)
- Low-Medium Density Residential (LMDR)
- Open Space / Stormwater
- 2 story minimum building height at corner lots where indicated with orange circle
- Detached Urban Townhomes Subarea  
 (Other uses permitted as per the Permitted Land Uses Chart)

CONCEPTUAL ILLUSTRATIVE MASTER PLAN  
 OCTOBER 19, 2022



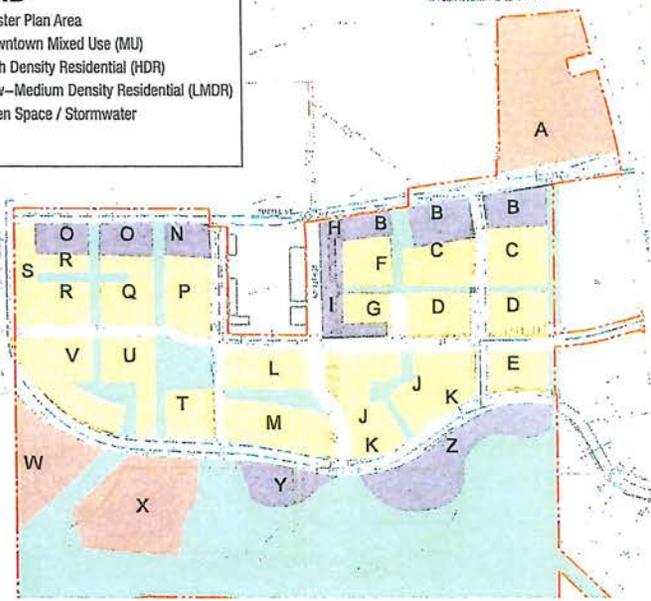
Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
 Sheet 9 of 20



GRAY'S STATION

**LEGEND**

- Master Plan Area
- Downtown Mixed Use (MU)
- High Density Residential (HDR)
- Low-Medium Density Residential (LMDR)
- Open Space / Stormwater



**Hubbell Gray's Station**  
 Master Plan Concept  
 Conceptual Land Use Summary & Estimated Residential Units

AREA NAME	LAND USE	ACRES	MINIMUM DENSITY (DU/AD)	MAXIMUM DENSITY (DU/AD)
A	Mixed Use	5.3	30	50
B	HDR	1.9	30	50
C	MDR	2.0	8	35
D	MDR	1.9	8	35
E	MDR	1.0	8	35
F	MDR	0.7	8	35
G	MDR	0.7	8	35
H	HDR	0.1	18	30
I	MDR	0.8	8	35
J	MDR	1.2	7	22
K	MDR	1.7	7	22
L	MDR	1.6	8	35
M	MDR	1.6	8	35
N	HDR	0.4	18	30
O	HDR	1.2	40	55
P	MDR	1.0	8	35
Q	MDR	1.2	8	35
R	MDR	1.1	8	35
S	MDR	0.7	8	35
T	MDR	0.7	8	35
U	MDR	1.1	8	35
V	MDR	2.0	8	35
W	Mixed Use	2.0	30	50
X	Mixed Use	3.5	30	50
Y	HDR	1.0	50	70
Z	HDR	2.7	50	70
<b>TOTAL</b>		<b>39.2</b>		

**Illustrative Estimate**

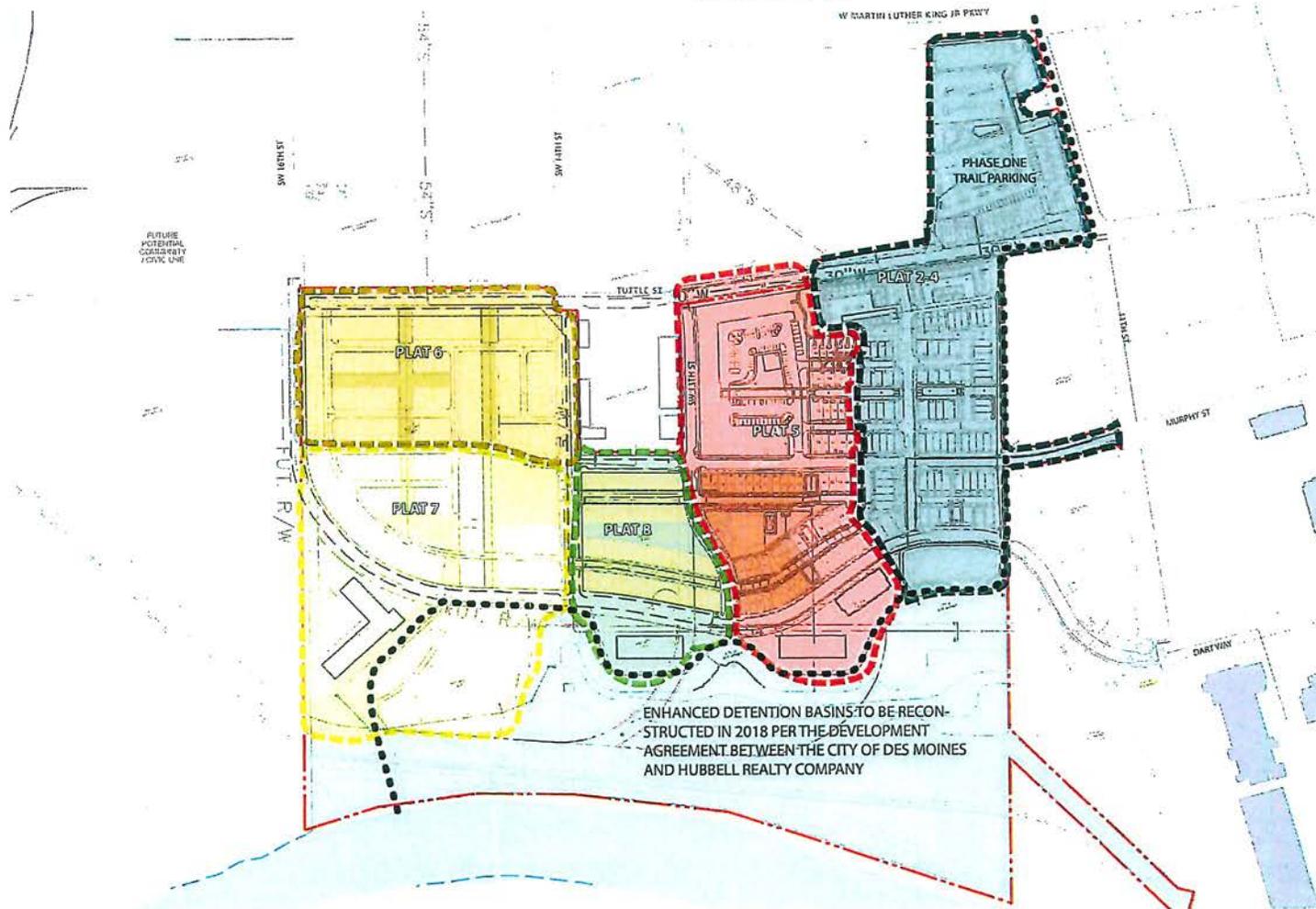
Illustrated Units	Illustrated Net Density	Assumed Multi-family residential floors	City Classification
193	37	4	High
79	41	3	High
34	17		High
34	18		High
16	16		Med
10	14		Med
11	16		Med
5	35	3	High
12	16		Med
13	11		Low-Med
22	13		Med
32	19		High
32	20		High
15	33	3	High
65	54	3	High
22	22		High
22	18		High
24	21		High
16	22		High
10	14		Med
18	16		Med
38	19		High
85	42	4	High
142	41	4	High
55	54	5	High
148	55	5	High
<b>TOTAL</b>	<b>1151</b>	<b>293</b>	

Gray's Station will achieve a minimum of 1,100 dwelling units.

**MINIMUM NET DENSITY (EXCLUDING SIGNIFICANT OPEN SPACE, ROADS) 28**

Notes: 1. The assumed number of multi-family units is based on an average 1000 sf dwelling unit.

Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
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NOTES:  
 Variables that will impact the anticipated phasing of the Gray's Station plan include the timing of:  
 1) Redevelopment of the DICO site, and  
 2) Infrastructure, improvements and street extensions completed by the City of Des Moines.

**LEGEND**

- Master Plan Area
- Phase One Trail - Conceptual Alignment

Anticipated Phasing:

- Plat 2-4
- Plat 5
- Plat 8
- Plat 6
- Plat 7



CONCEPTUAL PHASING DIAGRAM  
 OCTOBER 19, 2022

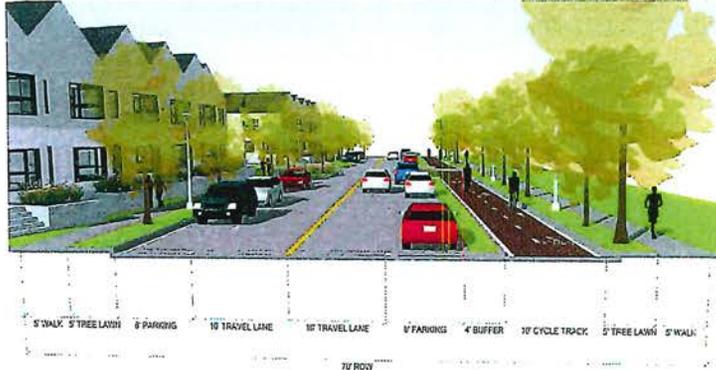


Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
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GRAY'S STATION

CONCEPTUAL STREET SECTION: SOUTHERN ROAD/16TH STREET (COLLECTOR STREET)



CONCEPTUAL STREET SECTIONS: TUTTLE STREET - CYCLE TRACK (COLLECTOR STREET)



CONCEPTUAL ALLEY SECTION



CONCEPTUAL STREET SECTION: LOCAL STREET AT 14TH STREET



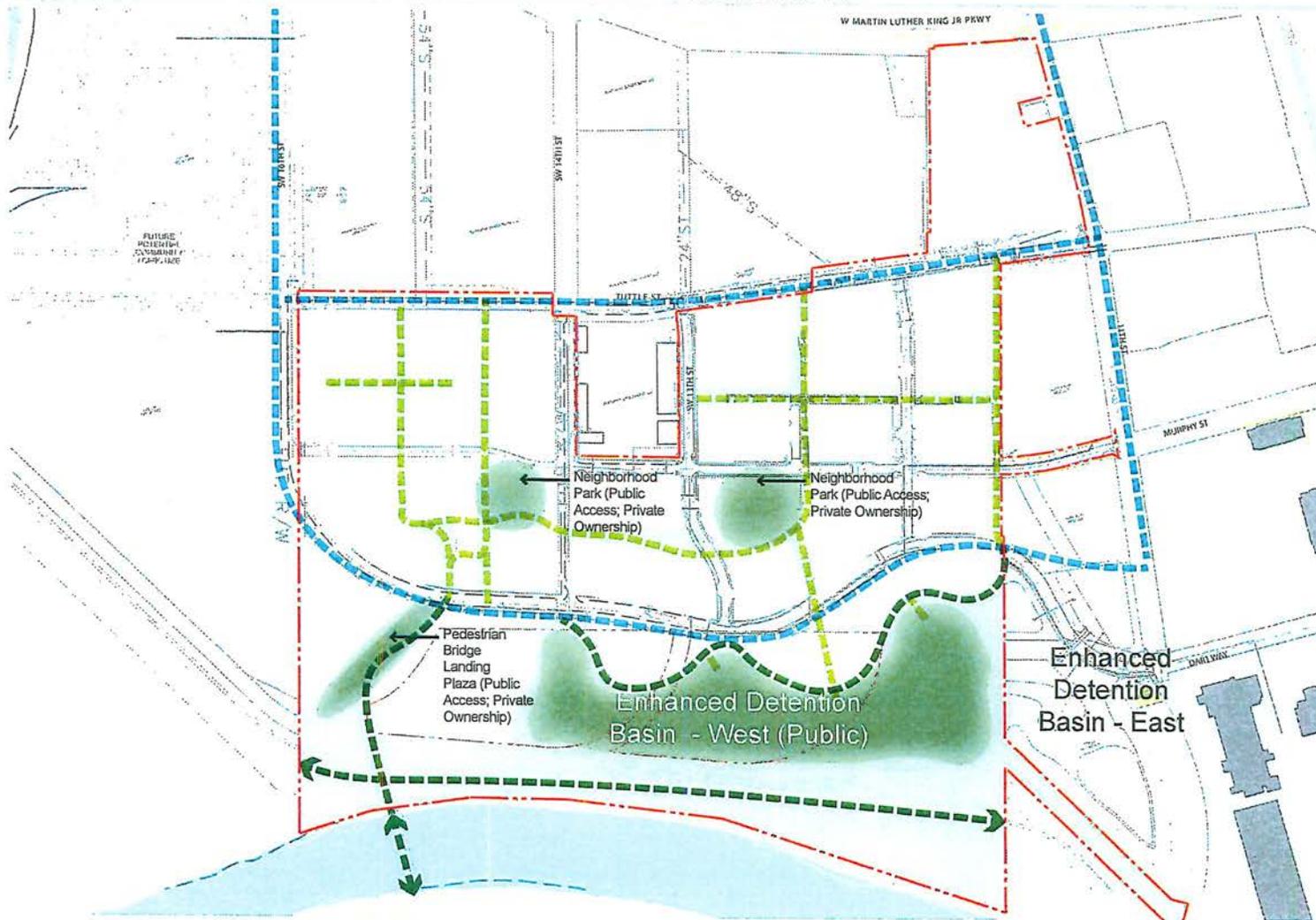
CONCEPTUAL STREET SECTION: LOCAL STREETS (EXCLUDING 14TH STREET)



NOTE: BUILDINGS SHOWN IN ALL SECTIONS FOR GENERAL MASSING ONLY. ARCHITECTURAL DESIGN IS YET TO BE DETERMINED. FINAL DEVELOPMENT PLANS TO BE REVIEWED AND

CONCEPTUAL STREET AND ALLEY SECTIONS  
OCTOBER 19, 2022

Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
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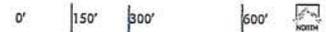


**LEGEND**

- Master Plan Area
- Proposed Public or Public Access Area
- Proposed Bicycle/Pedestrian Route – Primary
- Proposed Greenways
- Proposed On-Street Bike Facility
- Existing Regional Trail
- Existing Local Trail

NOTE: See Sheet 2A for land dedication and maintenance plans for open space areas.

CONCEPTUAL OPEN SPACE PLAN  
 OCTOBER 19, 2022



Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
Sheet 14 of 20



NOTE: BUILDINGS SHOWN FOR GENERAL MASSING ONLY. ARCHITECTURAL DESIGN IS YET TO BE DETERMINED. SUBJECT TO INDIVIDUAL FINAL DEVELOPMENT PLANS TO BE REVIEWED AND APPROVED BY THE PLANNING & ZONING COMMISSION AND CITY COUNCIL.

CONCEPTUAL SECTION ELEVATION THROUGH ENHANCED DETENTION BASIN  
OCTOBER 19, 2022



# Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa

## Sheet 15 of 20



GRAY'S STATION

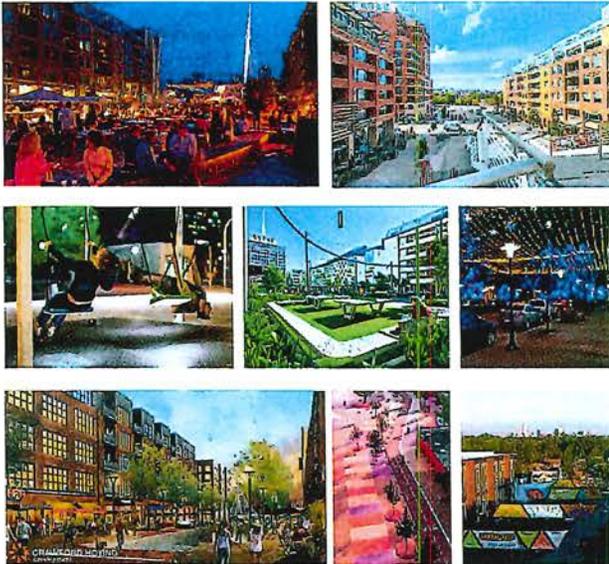
### OPEN SPACE CHARACTER

The open space system at Gray's Station is intended to support a diverse, urban population with a focus on wellness; opportunities for social and physical connections; and areas for connections to nature and solitude.

### PEDESTRIAN BRIDGE LANDING PLAZA

The following images and conceptual programming represent the intended character and feel of the plaza area anticipated in the Gray's Station neighborhood, but detailed programming and design of the park systems will be completed during later phases of the development. Final Development Plans to be reviewed by the Planning & Zoning Commission and by City Council.

Gathering • People-Watching • Outdoor Dining • Festival Space • Bike Connectivity



### NEIGHBORHOOD PARKS

The following images and conceptual programming represent the intended character and feel of the neighborhood parks anticipated in the Gray's Station neighborhood, but detailed programming and design of the park systems will be completed during later phases of the development. Final Development Plans to be reviewed by the Planning & Zoning Commission and by City Council.

Dog Walking • Playing • Learning • Picnicking • Gathering • Community Gardening • Community Space



### GREENWAYS

The following images and conceptual programming represent the intended character and feel of the greenways anticipated in the Gray's Station neighborhood, but detailed programming and design of the park systems will be completed during later phases of the development. Final Development Plans to be reviewed by the Planning & Zoning Commission and by City Council.

Connecting • Dog Walking • Playing • Biking • Neighborhood Gatherings



### ENHANCED STORMWATER DETENTION BASINS

The following images and conceptual programming represent the intended character and feel of the enhanced regional stormwater detention basins anticipated in the Gray's Station neighborhood, but detailed programming and design of the park systems will be completed during later phases of the development. Final Development Plans to be reviewed by the Planning & Zoning Commission and by City Council.

Restoring • Connecting • Learning • Unpaved Trails • Signage • Seating

The proposed enhanced detention basins will provide an attractive, educational water feature with public access and improved water quality within the Raccoon River. It is proposed to have sufficient volume to meet the water quality volume (WQv) requirements for the proposed Gray's Station development and have enough volume to address the 100-year storm event for the upstream watershed areas under full buildout conditions.

- The East Detention Basin area, formerly known as DART pond, is off-site, but is being planned for improvements concurrently with this redevelopment area. WQv requirements for areas within the proposed development will be met through permanent pool storage in the pool and marsh zones of the basin.
- The West Detention Basin area has sufficient volume to meet WQv requirements for areas within the proposed development through permanent pool storage in the pool and marsh zones of the basin.
- Permanent and well-maintained temporary access will be provided to the stormwater pump station at all times and coordinated with the City of Des Moines to ensure operation and maintenance of the station.
- A new security fence will be constructed around the stormwater pump station.

Though its primary purpose is to improve water quality, the enhanced detention basins also serve as a place to enhance the health of the community and to offer educational opportunities for the larger community. At a Master Plan level, the area features paved and unpaved trails, wetland overlook areas, and seating areas. The proposed inclusion of interpretive signage and art would allow people of all ages to understand the role of the area for the region. The use of micro-grading for the basins allows the area to be carefully shaped. This shaping breaks down the scale of the regional detention, creates places that are enjoyable, and removes the utilitarian feel of the existing basins.



PROPOSED OPEN SPACE CHARACTER IMAGERY  
OCTOBER 19, 2022

# Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa

## Sheet 16 of 20



GRAY'S STATION

### PRECEDENT HOUSING CHARACTER IMAGERY

As a large neighborhood, the proposed housing character for Gray's Station is intended to be diverse and urban in form/character. The following images represent the intended character and feel of the multi-family and mixed-use residential housing types. The design of the buildings including architectural details, materials, final massing/heights and colors will be completed during later phases of the development. Final Development Plans to be reviewed by the Planning & Zoning Commission and by City Council.

Each character image represents varied features as represented by the following categories that correspond to the colors under each image:

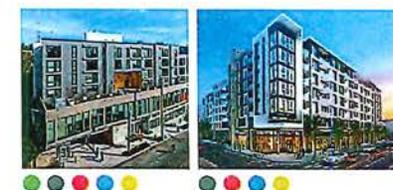
- Relationship to exterior open space (plaza, park, roof deck, etc.)
- Relationship to street
- Relationship to alley
- Building articulation/massing
- Architectural character
- Landscape character

### DOWNTOWN MIXED USE (NE CORNER)



### RESIDENTIAL

### MULTI-FAMILY / MEDIUM-HIGH DENSITY



### DOWNTOWN MIXED USE (BRIDGE LANDING)



PROPOSED BUILDING CHARACTER IMAGERY  
OCTOBER 19, 2022



Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
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GRAY'S STATION

**PRECEDENT HOUSING CHARACTER IMAGERY**

As a large neighborhood, the proposed housing character for Gray's Station is intended to be diverse and urban in form/character. The following images represent the intended character and feel of the medium density housing types. The design of the buildings including architectural details, materials, final massing/heights and colors will be completed during later phases of the development. Final Development Plans to be reviewed by the Planning & Zoning Commission and by City Council.

Each character image represents varied features as represented by the following categories that correspond to the colors under each image:

- Relationship to exterior open space (plaza, park, roof deck, etc.)
- Relationship to street
- Relationship to alley
- Building articulation/massing
- Architectural character
- Landscape character

**MEDIUM DENSITY RESIDENTIAL**



PROPOSED BUILDING CHARACTER IMAGERY  
 OCTOBER 19, 2022

Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
Sheet 18 of 20



DOWNTOWN MIXED USE - PROTOTYPICAL BUILDING

1. Primary facades of mixed-use and commercial structures shall be primarily parallel to the public right-of-way except in the subareas of W and X.
2. Buildings shall have entrances oriented toward primary street(s) and public plaza(s).
3. At least one building entrance for residential uses should directly access the street or public plaza when a residential use is located above street-level retail or commercial uses.
4. All buildings should have frontage on principal street(s), public plaza, park or greenway of not less than 70 percent of the lot (with exceptions as outlined in the Design Guidelines).
5. For commercial and mixed-use buildings, at least 70 percent of the building frontage should be within one foot of the property line (with exceptions as outlined in the Design Guidelines).
6. Facades of above grade structured parking (e.g., podium parking beneath commercial or residential uses) adjacent to any public right-of-way shall be architecturally and aesthetically consistent with the remainder of the building they support.
7. Minimum height for all Downtown Mixed Use buildings shall be four stories.
8. All Downtown Mixed Use buildings shall have a parapet or flat roof. Tower elements are allowed.

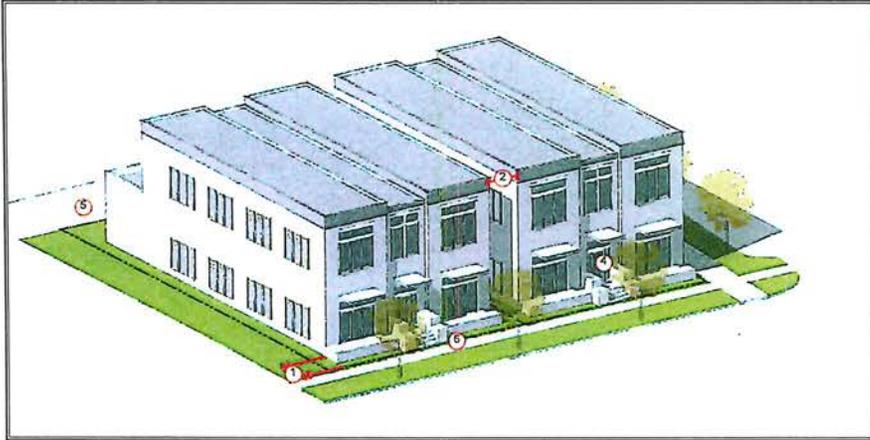


HIGH DENSITY RESIDENTIAL - PROTOTYPICAL BUILDING

1. For residential buildings, a maximum setback of 15 feet from the public right-of-way is permitted unless constrained by utility easements.
2. Buildings shall have entrances oriented toward primary street(s) and public plaza(s).
3. All buildings should have frontage on principal street(s), public plaza, park or greenway of not less than 70 percent of the lot (with exceptions as outlined in the Design Guidelines).
4. Facades of above grade structured parking (e.g., podium parking beneath commercial or residential uses) adjacent to any public right-of-way shall be architecturally and aesthetically consistent with the remainder of the building they support.
5. Minimum height for all uses shall be as follows:
  - a) Three stories for Subareas B, H, N, and O
  - b) Eight stories for Subareas Y and Z
6. All Downtown Mixed Use buildings shall have a parapet or flat roof. Tower elements are allowed.

PROPOSED PROTOTYPICAL FORMS  
OCTOBER 19, 2022





MEDIUM DENSITY RESIDENTIAL - PROTOTYPICAL MULTI-UNIT DWELLING UNIT

1. For all residential buildings, a maximum setback of 15 feet from the public right-of-way is permitted unless constrained by utility easements.
2. Attached and Detached residential buildings shall have side-yard building separation(s) (i.e., building face to building face) that are at minimum 5 feet and at maximum 10 feet except where walking paths occur between buildings.
  - a. Where walking paths occur between buildings the maximum width between buildings shall increase above the maximum separation by the width of the walking path.
3. Greenways shall include pedestrian pathways with a minimum width of 5'
4. Buildings shall have entrances oriented toward primary street(s), public plaza(s), parks, greenways or alleys.
5. All garages shall be oriented to an alley.
6. All buildings should have frontage on principal street(s), public plaza, park, greenways or alleys of not less than 70 percent of the lot (with exceptions as outlined in the Design Guidelines).



MEDIUM DENSITY RESIDENTIAL - PROTOTYPICAL DETACHED FORM (WITH FOURPLEX)

1. For all residential buildings, a maximum setback of 15 feet from the public right-of-way is permitted unless constrained by utility easements.
2. Attached and Detached residential buildings shall have side-yard building separation(s) (i.e., building face to building face) that are at minimum 5 feet and at maximum 10 feet except where walking paths occur between buildings.
  - a. Where walking paths occur between buildings the maximum width between buildings shall increase above the maximum separation by the width of the walking path.
3. Greenways shall include pedestrian pathways with a minimum width of 5'
4. Buildings shall have entrances oriented toward primary street(s), public plaza(s), parks, greenways or alleys.
5. All garages shall be oriented to an alley.
6. Accessory Dwelling Units shall be allowed.
7. All buildings should have frontage on principal street(s), public plaza, park, greenways or alleys of not less than 70 percent of the lot (with exceptions as outlined in the Design Guidelines).

Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
Sheet 20 of 20

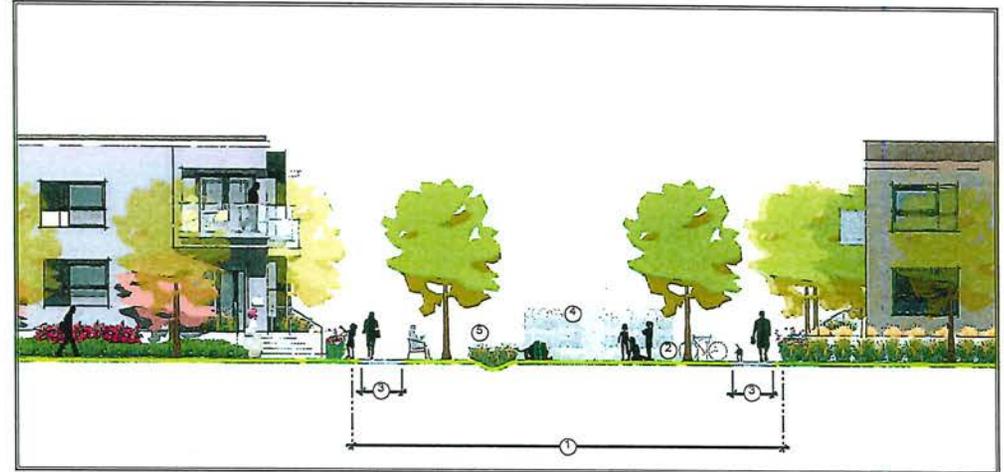


GRAY'S STATION



MEDIUM DENSITY RESIDENTIAL - PROTOTYPICAL ATTACHED FORM

1. For all residential buildings, a maximum setback of 15 feet from the public right-of-way is permitted unless constrained by utility easements.
2. Attached and Detached residential buildings shall have side-yard building separation(s) (i.e., building face to building face) that are at minimum 5 feet and at maximum 10 feet except where walking paths occur between buildings.
  - a. Where walking paths occur between buildings the maximum width between buildings shall increase above the maximum separation by the width of the walking path.
3. Greenways shall include pedestrian pathways with a minimum width of 5'
4. Buildings shall have entrances oriented toward primary street(s), public plaza(s), parks, greenways or alleys.
5. All garages shall be oriented to an alley.
6. Accessory Dwelling Units shall be allowed.
7. All buildings should have frontage on principal street(s), public plaza, park, greenways or alleys of not less than 70 percent of the lot (with exceptions as outlined in the Design Guidelines).



GREENWAY - PROTOTYPICAL FORM

Green connections are encouraged to minimize impact of storm water, provide urban pathways for wildlife, and promote recreation, wellness, and pedestrian scale mobility.

1. Greenways shall maintain an approximate width of 40' to 50'. Exceptions may occur where infrastructure requirements necessitate and will be reviewed with the Final Development Plan.
2. Greenways shall include landscaping.
3. Greenways shall include pedestrian pathways with a minimum width of 5'. Pathway location may vary depending on context.
4. Greenways may include structural (e.g., Pergola, Shade Structure, Community Garden Features, etc.) and/or artistic elements where deemed appropriate by the developer.
5. Greenways may include stormwater quality areas, where appropriate.

PROPOSED PROTOTYPICAL FORMS  
OCTOBER 19, 2022

# SITE DEVELOPMENT PLAN FOR: GRAY'S STATION TELUS

## DES MOINES, IOWA

### VICINITY - MAP

NOT TO SCALE



### OWNER

HIC INC I LLC  
6900 WESTON PKWY  
WEST DES MOINES, IA 50266

### APPLICANT

HUBBELL REALTY COMPANY  
CONTACT: KRIS SADDORIS  
6900 WESTON PKWY  
WEST DES MOINES, IA 50266  
PH: (515) 243-3228

### ENGINEER

CIVIL DESIGN ADVANTAGE  
4121 NW URBANDALE DRIVE  
URBANDALE, IA 50322  
CONTACT: RYAN HARDISTY  
EMAIL: RYANH@CDA-ENG.COM  
PH: (515) 369-4400

### SURVEYOR

CIVIL DESIGN ADVANTAGE, LLC  
4121 NW URBANDALE DRIVE  
URBANDALE, IA 50322  
CONTACT: CHARLIE MCGLOTHLEN  
EMAIL: CHARLEM@CDA-ENG.COM  
PH: (515) 369-4400

### ARCHITECT

SIMONSON & ASSOCIATES, INC.  
1717 INGERSOLL AVE SUITE #117  
DES MOINES, IA 50309  
CONTACT: STEPHANIE POOLE  
EMAIL: SPOOLE@SIMONSONASSOC.COM  
PH: (515) 440-5928

### SUBMITTAL DATES

FIRST SUBMITTAL: 10/19/2022  
SECOND SUBMITTAL: 01/04/2023

### ZONING

GRAY'S STATION PUD - ZON2017-00087

### EXISTING/ PROPOSED USE

EXISTING: UNDEVELOPED  
PROPOSED: HOUSEHOLD LIVING - 8+ HOUSEHOLDS PER LOT

### DEVELOPMENT SUMMARY

TOTAL SITE AREA: 3.48 ACRES (151,395 SF)  
EXISTING IMPERVIOUS AREA: 0.00 ACRES (0 SF)  
OPEN SPACE CALCULATION:  
TOTAL SITE: 151,395 SF (3.48 AC.)  
BUILDINGS: 47,725 SF  
PARKING AREAS: 66,172 SF  
SIDEWALK: 14,844 SF  
OPEN SPACE PROVIDED: 22,854 SF (15.0%)

UNITS: 84 UNITS (24.14 UNITS PER ACRE)

PARKING:  
STANDARD: 48 SPACES  
GARAGE: 84 SPACES  
ADA: 5 SPACES  
TOTAL: 137 SPACES

### DATE OF SURVEY

08/05/2022

### BENCHMARKS

- CITY BENCHMARK #725: BRASS CAP IN THE NORTHEAST TRAFFIC SIGNAL BASE AT THE SOUTHWEST CORNER OF MARTIN LUTHER KING DRIVE AND SOUTHWEST 11TH STREET. ELEVATION=28.61
- CITY BENCHMARK #5581: BRASS TABLET IN THE CONCRETE WALL 43.0 FEET WEST OF THE EAST FACE NEAR THE SANITARY SEWER PUMP STATION. ELEVATION=25.48

### LEGAL DESCRIPTION

LOT 87, GRAY'S STATION PLAT 5, AN OFFICIAL PLAT IN THE CITY OF DES MOINES, POLK COUNTY, IOWA AND CONTAINING 3.48 ACRES (151,395 SF).

### TAX ABATEMENT - SUSTAINABILITY

- BINE RACKS FOR PUBLIC USE THAT PROVIDE A MINIMUM OF 10% OF THE AUTOMOBILE PARKING PROVIDED.
- 50% INCREASE IN REQUIRED LANDSCAPING.
- PRIMARY ENTRY WITHIN 1/4 MILE OF A DART TRANSIT STOP.
- REDEVELOPMENT OF A PREVIOUSLY DEVELOPED SITE.

### INDEX OF SHEETS

NO.	DESCRIPTION
CO.1	COVER SHEET
C1.0-1.2	DIMENSION PLAN
C2.0-2.6	GRADING PLAN
C3.0-3.2	UTILITY PLAN
C4.0	DETAILS
L02.02 & L02.04	RDG LANDSCAPE PLAN - SITE LAYOUT PLAN
L04.02 & L04.04	RDG LANDSCAPE PLAN - SITE PLANTING PLAN
L05.01	RDG LANDSCAPE PLAN - DETAILS

### GENERAL LEGEND

PROPOSED	EXISTING
PROPERTY BOUNDARY	SANITARY MANHOLE
SECTION LINE	WATER VALVE BOX
CENTER LINE	FIRE HYDRANT
RIGHT OF WAY	WATER CURB STOP
BUILDING SETBACK	WELL
PERMANENT EASEMENT	STORM SEWER MANHOLE
TEMPORARY EASEMENT	STORM SEWER SINGLE INTAKE
TYPE SW-501 STORM INTAKE	STORM SEWER DOUBLE INTAKE
TYPE SW-502 STORM INTAKE	FLARED END SECTION
TYPE SW-503 STORM INTAKE	DECIDUOUS TREE
TYPE SW-505 STORM INTAKE	CONIFEROUS TREE
TYPE SW-506 STORM INTAKE	DECIDUOUS SHRUB
TYPE SW-512 STORM INTAKE	CONIFEROUS SHRUB
TYPE SW-513 STORM INTAKE	ELECTRIC POWER POLE
TYPE SW-401 STORM MANHOLE	GUY ANCHOR
TYPE SW-402 STORM MANHOLE	STREET LIGHT
FLARED END SECTION	POWER POLE W/ TRANSFORMER
TYPE SW-301 SANITARY MANHOLE	UTILITY POLE W/ LIGHT
STORM/SANITARY CLEANOUT	ELECTRIC BOX
WATER VALVE	ELECTRIC TRANSFORMER
FIRE HYDRANT ASSEMBLY	ELECTRIC MANHOLE OR VAULT
DETECTABLE WARNING PANEL	TRAFFIC SIGN
WATER CURB STOP	TELEPHONE JUNCTION BOX
SANITARY SEWER	TELEPHONE MANHOLE/VAULT
SANITARY SERVICE	TELEPHONE POLE
STORM SEWER	GAS VALVE BOX
STORM SERVICE	CABLE TV JUNCTION BOX
WATERMAN WITH SIZE	CABLE TV MANHOLE/VAULT
WATER SERVICE	MAIL BOX
SAWCUT (FULL DEPTH)	BENCHMARK
SILT FENCE	SOIL BORING
USE AS CONSTRUCTED	UNDERGROUND TV CABLE
MINIMUM PROTECTION ELEVATION	GAS MAIN
	FIBER OPTIC
	UNDERGROUND TELEPHONE
	OVERHEAD ELECTRIC
	UNDERGROUND ELECTRIC
	FIELD TILE
	SANITARY SEWER W/ SIZE
	STORM SEWER W/ SIZE
	WATER MAIN W/ SIZE



### UTILITY WARNING

ANY UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY AND RECORDS OBTAINED BY THIS SURVEYOR. THE SURVEYOR MAKES NO GUARANTEE THAT THE UTILITIES SHOWN COMPRISE ALL THE UTILITIES IN THE AREA. EITHER IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UTILITIES SHOWN ARE IN THE EXACT LOCATION SHOWN.



**CIVIL DESIGN ADVANTAGE**

4121 NW URBANDALE DRIVE, URBANDALE, IA 50322  
PH: (515) 369-4400  
PROJECT NO. 2205.350

**SITE PLAN APPROVAL:**

APPROVED  APPROVED WITH CONDITIONS - SEE EXHIBIT "A"

IN ACCORDANCE WITH SECTION 135-9, 2021 DES MOINES MUNICIPAL CODE, AS AMENDED.

NO CHANGES TO THIS PLAN UNLESS APPROVED IN WRITING FROM THE DEVELOPER SERVICES DIRECTOR.

DEVELOPMENT SERVICES DIRECTOR: \_\_\_\_\_ DATE: \_\_\_\_\_

THE PROJECT REQUIRES AN IOWA NPDES PERMIT #2 AND CITY OF DES MOINES GRADING PERMIT. CIVIL DESIGN ADVANTAGE WILL PROVIDE THE PERMITS AND THE INITIAL STORM WATER POLLUTION PREVENTION PLAN (SWPPP) FOR THE CONTRACTORS USE DURING CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR UPDATING THE SWPPP THROUGHOUT CONSTRUCTION AND MEETING LOCAL, STATE AND FEDERAL REQUIREMENTS.

ALL CONSTRUCTION MATERIALS, DUMPSTERS, DETACHED TRAILERS OR SIMILAR ITEMS ARE PROHIBITED ON PUBLIC STREETS OR WITHIN THE PUBLIC R.O.W.

THE 2022 EDITION OF THE SDAS STANDARD SPECIFICATIONS, THE PUBLIC RIGHTS-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG) AND ALL CITY SUPPLEMENTALS, IF APPLICABLE, SHALL APPLY TO ALL WORK ON THIS PROJECT UNLESS OTHERWISE NOTED.

I HEREBY CERTIFY THAT THIS ENGINEERING DOCUMENT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF IOWA.

DATE: \_\_\_\_\_

STAMP: CIVIL DESIGN ADVANTAGE, INC. 2021

EXPIRES: December 31, 2023

PAGES OR SHEETS COVERED BY THIS SEAL: \_\_\_\_\_

ALL C SERIES SHEETS

MADE IN URBANDALE, IOWA BY CIVIL DESIGN ADVANTAGE, INC. 4121 NW URBANDALE DRIVE, URBANDALE, IA 50322  
 515-369-4400



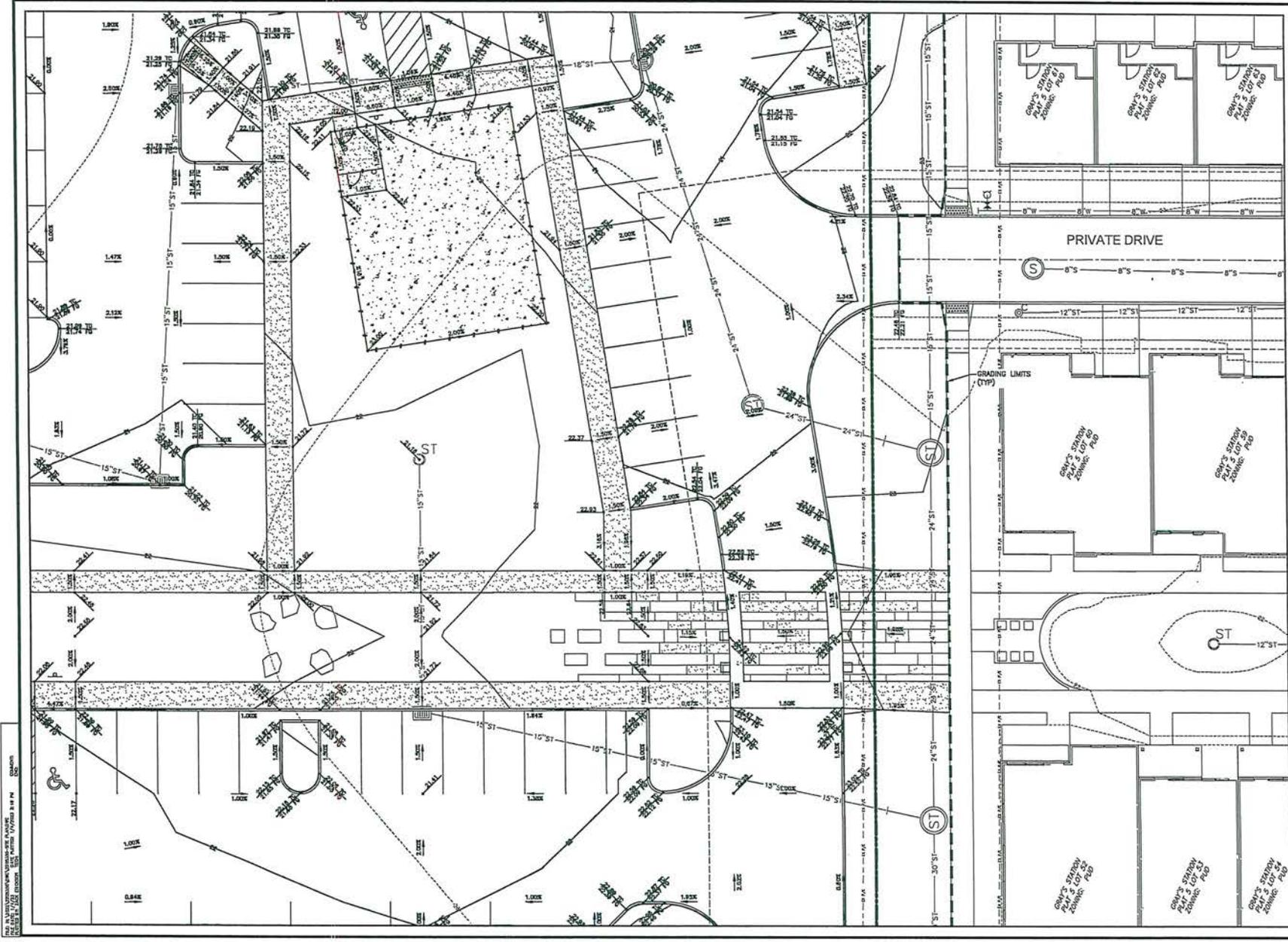












FILE: N:\2025\CONTRACTS\2500000000\2500000000.DWG  
 DATE: 11/11/2025 10:00:00 AM  
 USER: JEFFREY.P.  
 PLOT: 11/11/2025 10:00:00 AM

REVISIONS	DATE	UNISOFT

4124 NW URBANDALE DRIVE  
 URBANDALE IA 50322  
 PHONE: (515) 385-4400



**GRAY'S STATION TELUS**  
**GRADING PLAN**

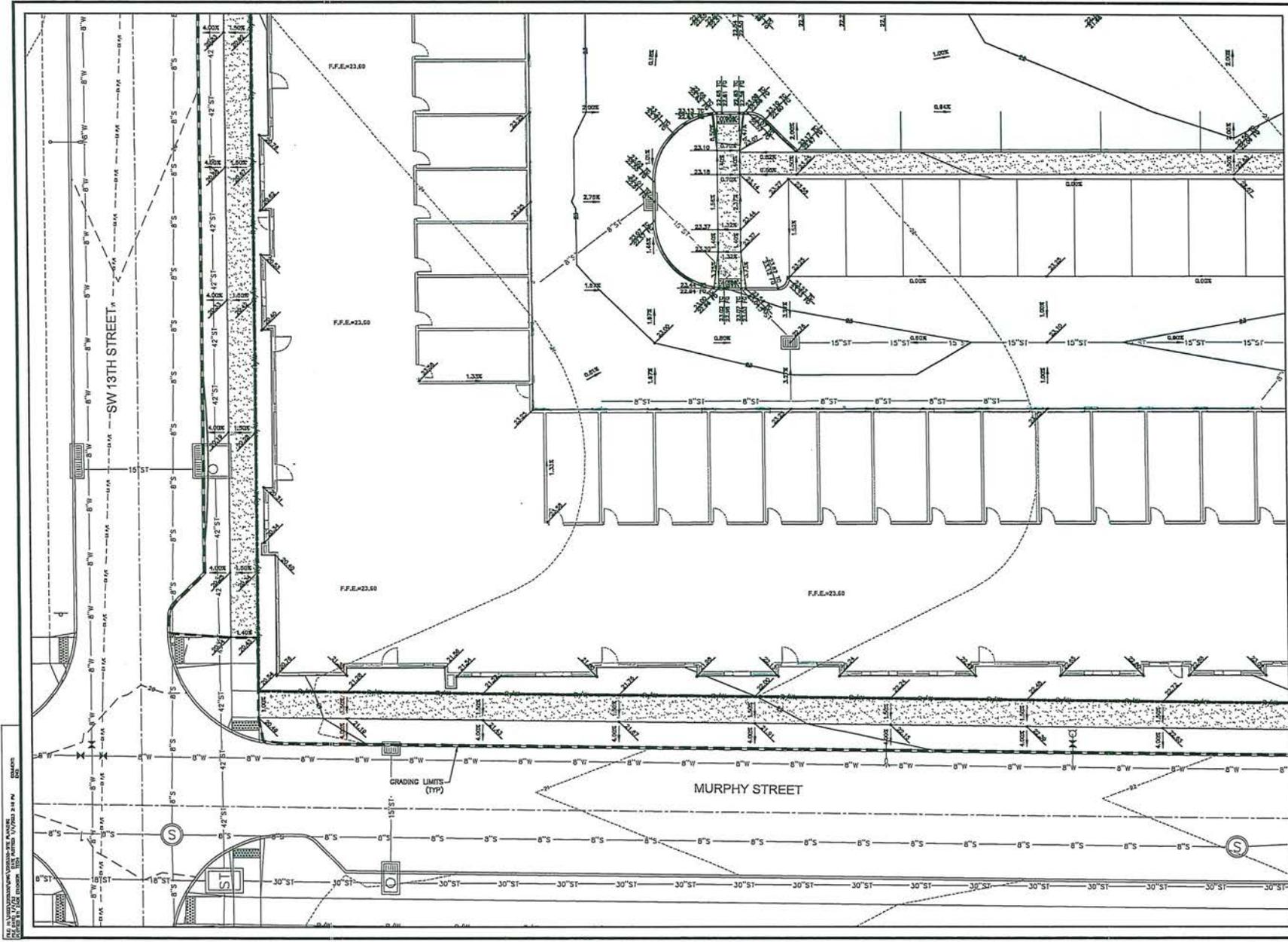
DES MOINES, IOWA  
 ENGINEER:

SECOND SUBMITTAL  
 FIRST SUBMITTAL

TECH:  
 ENGINEER:

CIVIL DESIGN ADVANTAGE

C2.4  
 2205.350

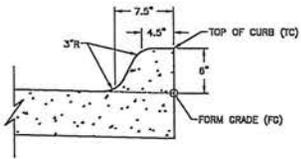


DATE	DESIGNER	PROJECT	TECH
		4121 NW URBANDALE DRIVE URBANDALE, IA 50322 PHONE: (515) 365-4400	
		ENGINEER	
		CIVIL DESIGN ADVANTAGE	
		DES MOINES, IOWA	
		GRAY'S STATION TELUS GRADING PLAN	
		C2.5	
		2205.360	

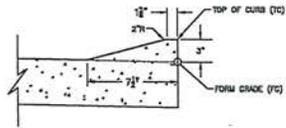




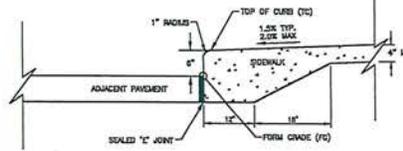




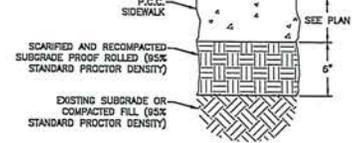
1 6" INTEGRAL CURB  
NOT TO SCALE



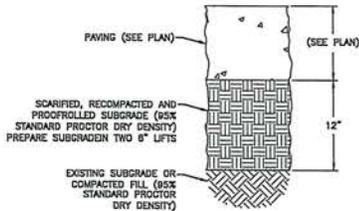
2 3" INTEGRAL ROLLED CURB  
NOT TO SCALE



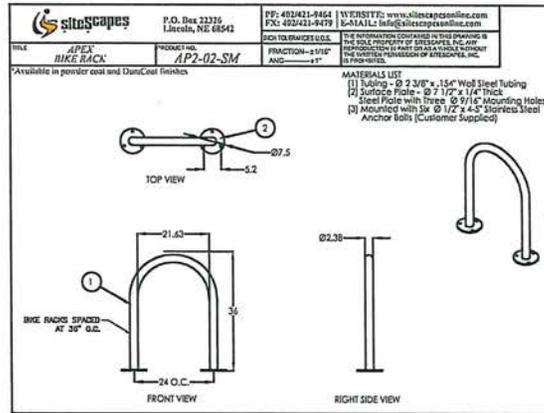
3 CLASS 'A' INTEGRAL CURB AND SIDEWALK  
NOT TO SCALE



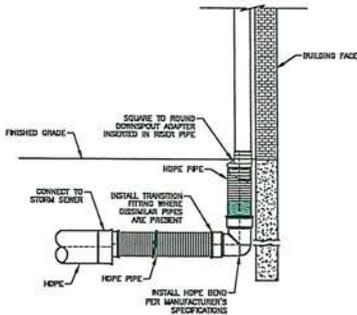
4 SIDEWALK  
NOT TO SCALE



5 12" SUBGRADE PREPARATION  
NOT TO SCALE



6 BIKE RACK  
NOT TO SCALE



7 ROOF DRAIN CONNECTION WITH TEE  
NOT TO SCALE

DATE	
REVISIONS	
RECORD SUBMITTAL	
PRINT SUBMITTAL	
DATE	05/19/2023

4121 NW URBANDALE DRIVE  
URBANDALE, IA 50322  
PHONE: (515) 369-4400

TECH:

ENGINEER:



GRAY'S STATION TELUS  
DETAILS

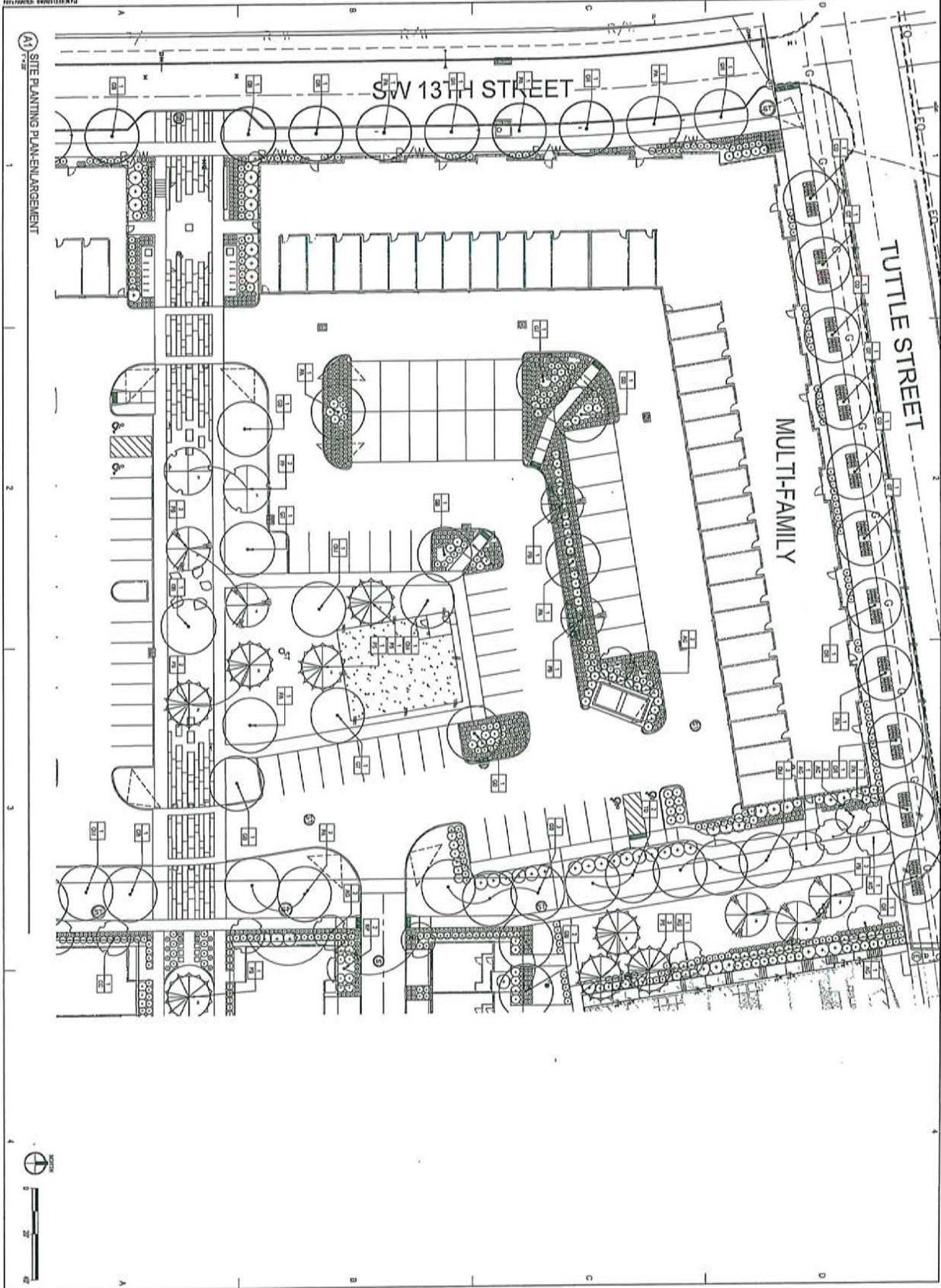
CIVIL DESIGN ADVANTAGE

C4.0

2205.350

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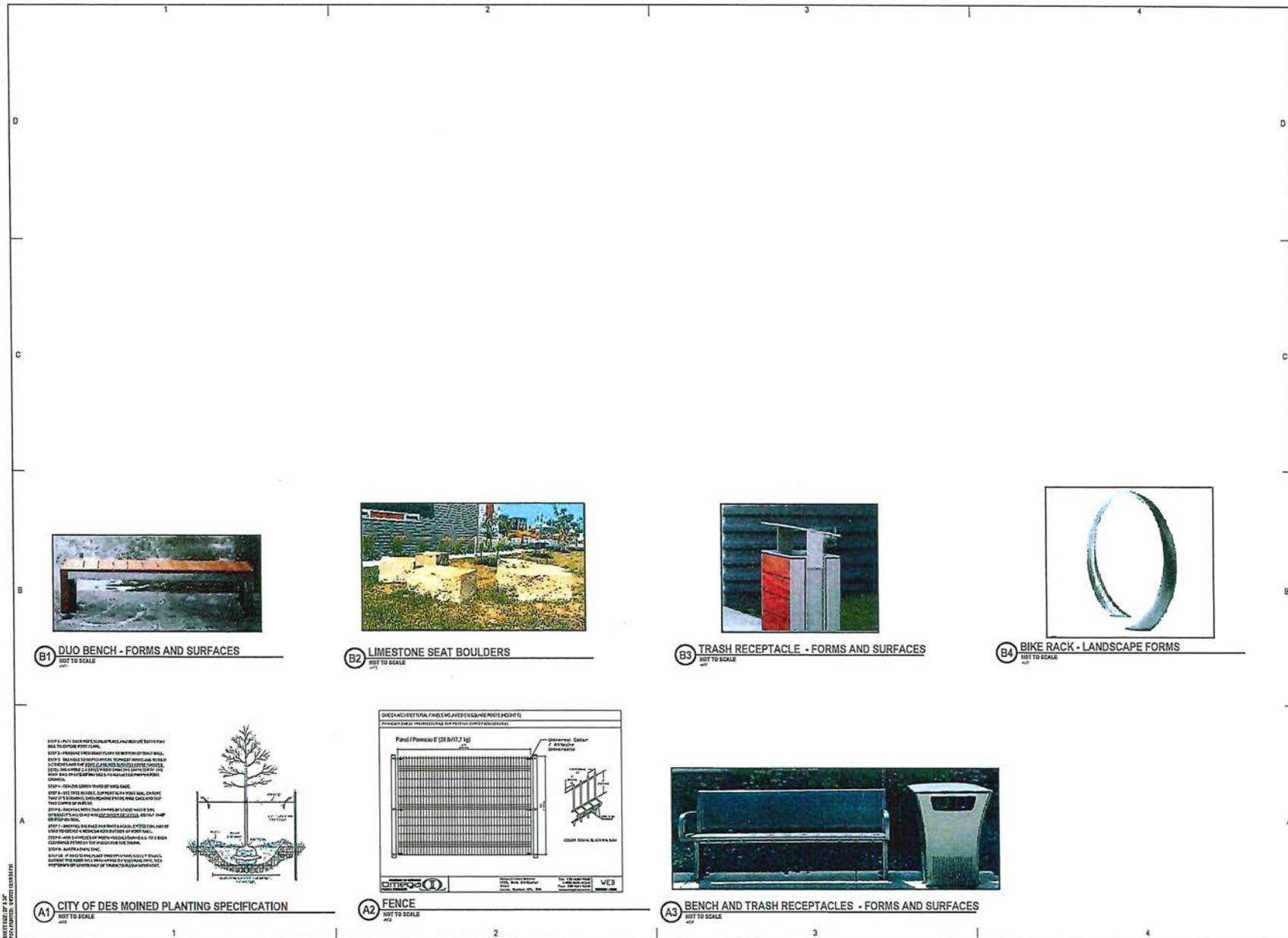
(A) SITE PLANTING PLAN-ENLARGEMENT



<p><b>GRAY'S STATION PLAT 5</b></p>	<p>PRELIMINARY - NOT FOR CONSTRUCTION</p>	<p>DESIGN DEVELOPMENT</p>
<p style="writing-mode: vertical-rl; transform: rotate(180deg);">L04.02</p> <p style="writing-mode: vertical-rl; transform: rotate(180deg);">SITE PLANTING PLAN</p>	<p>HUBBELL REALTY</p>	<p><b>ARCHITECT</b>                  RDG Planning &amp; Design                  200 Paving &amp; Grp.                  201 South Park                  Oak Brook, IL 60110                  630.585.1144</p> <p><b>LANDSCAPE ARCHITECT</b>                  RDG Planning &amp; Design                  200 Paving &amp; Grp.                  201 South Park                  Oak Brook, IL 60110                  630.585.1144</p>







**B1 DUO BENCH - FORMS AND SURFACES**  
NOT TO SCALE



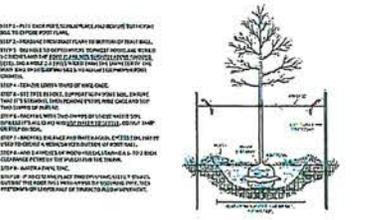
**B2 LIMESTONE SEAT BOULDERS**  
NOT TO SCALE



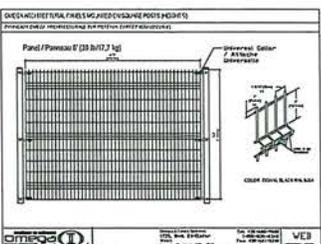
**B3 TRASH RECEPTACLE - FORMS AND SURFACES**  
NOT TO SCALE



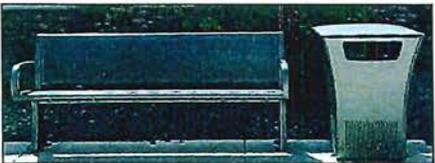
**B4 BIKE RACK - LANDSCAPE FORMS**  
NOT TO SCALE



**A1 CITY OF DES MOINES PLANTING SPECIFICATION**  
NOT TO SCALE



**A2 FENCE**  
NOT TO SCALE



**A3 BENCH AND TRASH RECEPTACLES - FORMS AND SURFACES**  
NOT TO SCALE

GRAY'S STATION PLAT 5

PRELIMINARY - NOT FOR CONSTRUCTION

DESIGN DEVELOPMENT

KEY PLAN

ARCHITECT  
R.D.G. PLANNING & DESIGN  
1000 EAST 15TH AVENUE  
DES MOINES, IA 50319

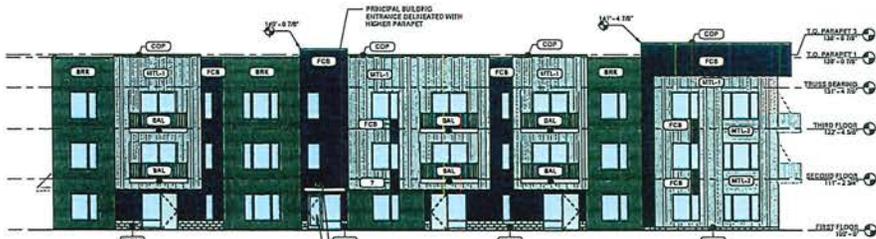
LANDSCAPE ARCHITECT  
HUBBELL REALTY  
1000 EAST 15TH AVENUE  
DES MOINES, IA 50319

**RDG...**  
PLANNING • DESIGN

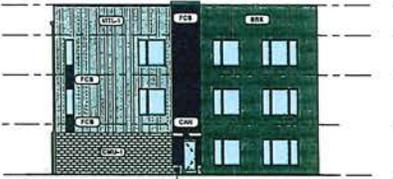
DATE	DESCRIPTION	BY	CHKD

PROJECT: HUBBELL REALTY 1000 EAST 15TH AVENUE, DES MOINES, IA 50319  
 DESIGNER: R.D.G. PLANNING & DESIGN  
 LANDSCAPE ARCHITECT: HUBBELL REALTY  
 DATE: 10/15/2015  
 SCALE: AS SHOWN  
 DRAWN BY: J. HUBBELL  
 CHECKED BY: J. HUBBELL  
 PROJECT NO.: 15-001  
 SHEET NO.: 5 OF 5

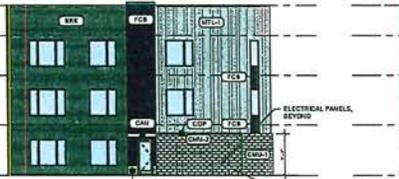




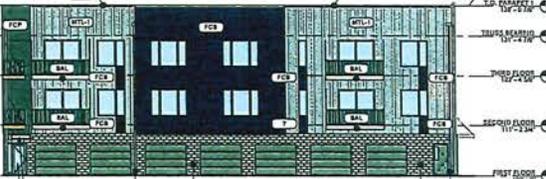
1 BUILDING #2 SW 13TH STREET  
SCALE 3/8" = 1'-0"



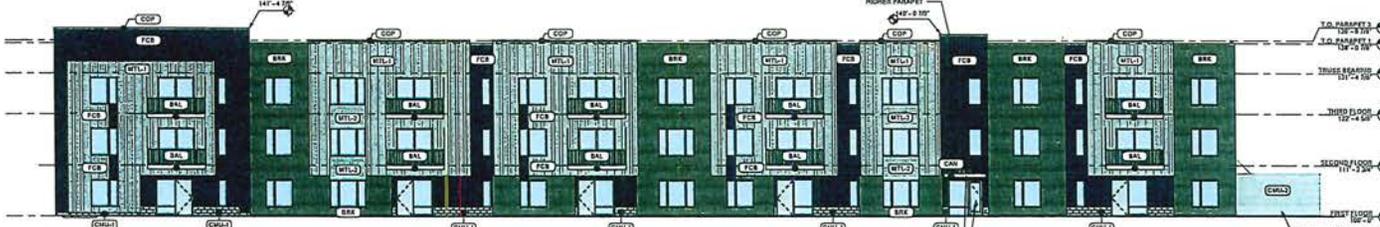
6 BUILDING #2 - SIDE (GREEN BELT)  
SCALE 3/8" = 1'-0"



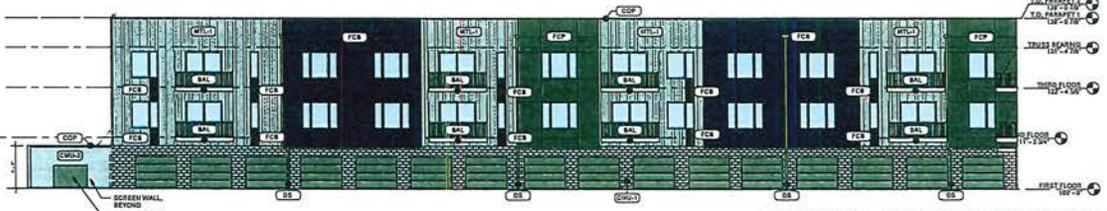
4 BUILDING #2 SIDE (EAST END)  
SCALE 3/8" = 1'-0"



3 BUILDING #2 - INTERNAL ELEVATION (EAST)  
SCALE 3/8" = 1'-0"



2 BUILDING #2 - MURPHY STREET  
SCALE 3/8" = 1'-0"



5 BUILDING #2 - INTERNAL ELEVATION (SOUTH)  
SCALE 3/8" = 1'-0"

EXTERIOR MATERIAL LEGEND	
MAT. LABEL	MATERIAL DESCRIPTION
BAL	PERFORATED BALCONY CLEAR ANODIZED ALUMINUM
BK	FULL DEPTH MODULAR BRICK, NATURAL, MORTAR COLOR - GRAY
CAN	PERFORATED CANOPY, HAPES, OR EQUAL, COLOR, CLEAR ANODIZED
CAN1	PERFORATED CANOPY, HAPES, OR EQUAL, VERTICAL, NATURAL
CH2	PERFORATED CHAIR, HAPES, OR EQUAL, NATURAL WITH NATURAL MORTAR
CH3	PERFORATED CHAIR, HAPES, OR EQUAL, NATURAL WITH NATURAL MORTAR
CH4	PERFORATED CHAIR, HAPES, OR EQUAL, NATURAL WITH NATURAL MORTAR
CH5	PERFORATED CHAIR, HAPES, OR EQUAL, NATURAL WITH NATURAL MORTAR
CH6	PERFORATED CHAIR, HAPES, OR EQUAL, NATURAL WITH NATURAL MORTAR
CH7	PERFORATED CHAIR, HAPES, OR EQUAL, NATURAL WITH NATURAL MORTAR
CH8	PERFORATED CHAIR, HAPES, OR EQUAL, NATURAL WITH NATURAL MORTAR
CH9	PERFORATED CHAIR, HAPES, OR EQUAL, NATURAL WITH NATURAL MORTAR
CH10	PERFORATED CHAIR, HAPES, OR EQUAL, NATURAL WITH NATURAL MORTAR
CH11	PERFORATED CHAIR, HAPES, OR EQUAL, NATURAL WITH NATURAL MORTAR
CH12	PERFORATED CHAIR, HAPES, OR EQUAL, NATURAL WITH NATURAL MORTAR
CH13	PERFORATED CHAIR, HAPES, OR EQUAL, NATURAL WITH NATURAL MORTAR
CH14	PERFORATED CHAIR, HAPES, OR EQUAL, NATURAL WITH NATURAL MORTAR
CH15	PERFORATED CHAIR, HAPES, OR EQUAL, NATURAL WITH NATURAL MORTAR
CH16	PERFORATED CHAIR, HAPES, OR EQUAL, NATURAL WITH NATURAL MORTAR
CH17	PERFORATED CHAIR, HAPES, OR EQUAL, NATURAL WITH NATURAL MORTAR
CH18	PERFORATED CHAIR, HAPES, OR EQUAL, NATURAL WITH NATURAL MORTAR
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CH99	PERFORATED CHAIR, HAPES, OR EQUAL, NATURAL WITH NATURAL MORTAR
CH100	PERFORATED CHAIR, HAPES, OR EQUAL, NATURAL WITH NATURAL MORTAR

- ### GENERAL EXTERIOR ELEVATION NOTES
- REFER TO ADDITIONAL NOTES AND REQUIREMENTS ON ALL OTHER DOCUMENTS AND OTHER OCCUPANCY DOCUMENTS.
  - ALL CORNER RINGS & SERVICE LINES SHALL BE GROUPED TOGETHER IN A NEAT & ORDERLY MANNER AND SHOULD PENETRATE THE WALL AT THE LOWEST POINT AND ARE NOT TO RUN OVER THE TOP OF PARAPET. ALL OTHER CORNER & JUNCTION COVERS FOR EXTERIOR FIXTURES & EQUIPMENT SHALL BE FULLY COVERED WITH THE BUILDING CONSTRUCTION.
  - ALL HORIZONTAL TRANSITIONS BETWEEN DIFFERENT MATERIALS SHALL BE FINISHED CONTINUOUSLY FROM TOP BEHIND UPPER MATERIAL AND EXTEND OVER TO EXTERIOR FACE OF LOWER MATERIAL WITH DRIP EDGE.
  - ANY RETURN OF SLAB ELEVATIONS NOT SHOWN SHALL BE EQUAL AS MATERIAL AND MANNER TO ADJACENT CONDITIONS OR OTHER SIMILAR CONSIDERATIONS.
  - NO EXPOSED MASONRY/VERTICAL CORES, MASONRY SETBACKS AT WINDOW HEADINGS ARE REQUIRED IF FRAMING IS NOT SET ENOUGH TO PROVIDE CLOSURE AT THE BACKS OF MASONRY FOR AN EFFECTIVE SEALED JOINT.
  - MATERIALS ABOVE CANOPIES & FININGS TO BE CONTINUED DOWN BEHIND THEM UNLESS NOTED OTHERWISE.
  - ANY EXPOSED ROOFING MEMBERS ARE VISIBLE FROM A PUBLIC RIGHT AWAY MUST MATCH THE PRIMARY WALL MATERIAL COLOR.
  - INSTALL VERTICAL EXPANSION JOINTS PER STRUCTURAL OR SIMILAR AT COLUMN/SHIELD LINES AND AT THE HEAD OF DOOR JAMB/COLUMNS OR SIMILAR AT OTHER JOINTS OR WHERE REQUIRED PER STANDARD MATERIAL PRACTICES TO REDUCE STRESS CRACKING. NO CONTROL JOINTS/EXPANSION JOINT AT MAIN STRUCTURAL BEARING LOCATIONS. VERIFY JOINTS WITH ARCHITECT.
  - CURE & SEAL ALL TRANSITION/CONTROL/EXPANSION AT ALL EXTERIOR MATERIALS.
  - PROVIDE CAST REGLETS JUST IN FROM THE OUTER EDGE OR UNDERLAGE OF FACE MATERIAL OR DRIP EDGE AT ALL OVERHANGS & SOFFITS.
  - ISOLATE AND SEAL TO ENSURE ADEQUATE AIR INFILTRATION BARRIER AROUND ALL EXTERIOR PENETRATIONS TO PREVENT WATER AND SPERMITE PIPE PENETRATION.
  - WHERE EXTERIOR FINISH ADJACENT TO BUILDING, TOP OF FINISH TO BE 1/2" LOWER THAN INTERIOR FINISH FLOOR, ELONG CONTINUOUSLY AWAY FROM BUILDING A MINIMUM OF 3" WHERE STOP 48" AWAY FROM DOOR 1/2" MINIMUM MAX. SLOPE, AFTER 48" AWAY FROM DOOR 1/2" MINIMUM MAXIMUM SLOPE TO PUBLIC WALK, REFER TO CODE.
  - WHERE NO EXTERIOR FINISH ADJACENT TO BUILDING, TOP OF FINISHED UNDERLAYS & MINIMUM OF 1/2" BELOW INTERIOR FINISH FLOOR AND 1/2" BELOW EXTERIOR WALL FLASHING/WATERFALL, ELONG CONTINUOUSLY AWAY FROM BUILDING A MINIMUM OF 3" (REFER TO CODE).
  - ADDRESS SHALL BE FINISHED ON STREET SIDE OF BUILDING AND ON MONUMENT SIDE, NUMBERS & WINDOW & SIGN CONTRACTING COLOR NUMBERS WITH MINIMUM 1/2" EXTENSION.
  - IF ANY SIGNAGE OR SIGN BASE IS SHOWN IT IS FOR REFERENCE ONLY. FINAL SIGNAGE AND/OR ALLOWANCES SHALL BE DETERMINED WITH APPROVED SIGNAGE PERMITS.

ACTUAL DETAILS & DIMENSIONS MAY VARY AND ARE SUBJECT TO CHANGE. ALL DRAWINGS ARE SUBJECT TO AUTHORITY HAVING JURISDICTION APPROVAL. SIGNAGE SHOWN DOES NOT REPRESENT ALLOWABLE SIGNAGE PERMITS/REGULATIONS OF ANY JURISDICTION FOR CURRENT CITY ORDINANCE BY USHERS.

- ### MISC EXTERIOR MATERIAL FINISHES
- TYPICAL USES:
- EXPOSED CONCRETE: GRIND SMOOTH & SUBSEAL WITH CEMENT SLURRY.
  - EXPOSED BRICK: GALVANIZED & PAINTED TO MATCH ADJACENT MATERIAL ABOVE.
  - METAL CANOPIES & RAILINGS: GALVANIZED & PAINTED WHITE.
  - FAUCETS & SOFFITS: METAL, PREFINISHED TO MATCH ADJACENT ROOF COLOR.
  - CLIMBERS, CUTTERS & DOWNPOUTS: METAL, PREFINISHED TO MATCH ADJ. MATERIAL.
  - CHIMNEYS & GUTTERS: COLOR MATCHED TO ADJACENT MATERIAL ABOVE MATERIAL.
  - OVERHEAD PANEL ROOFING: METAL, PRE-FINISHED TO MATCH ADJACENT MATERIAL.
  - LOGGERS: METAL, PRE-FINISHED TO MATCH GRATE.
  - LIGHT FIXTURES: METAL, PRE-FINISHED HOUSEHOLD TO MATCH ADJ. OPENING FRAMING.
  - MECHANICAL EQUIPMENT: SCREENED BY PARAPET.
  - EXTERIOR FINISH EQUIPMENT: IF ALLOWED, PAINTED TO MATCH ADJACENT MATERIAL.
  - TRIM SIZES, TYP. 1/2":
    - VERTICAL & SQUARE 5/8"
    - HORIZONTAL 5/8"

MATERIAL CALCULATIONS (BODY OF DEE HOUSE)			SIDE ELEVATION (EAST END) 2,843.81 SF TOTAL			SIDE ELEVATION (WEST END) 2,843.81 SF TOTAL		
	GLASS =	W/O FENESTRATIONS: 1,684.81 SF		GLASS =	W/O FENESTRATIONS: 1,684.81 SF		GLASS =	W/O FENESTRATIONS: 1,684.81 SF
GLASS =	215.23 SF	18.2%	GLASS =	215.23 SF	33.0%	GLASS =	215.23 SF	33.0%
BRICK =	61.23 SF	2.2%	BRICK =	205.24 SF	11.0%	BRICK =	205.24 SF	11.0%
FIBER CEMENT =	283.24 SF	10.0%	FIBER CEMENT =	205.24 SF	11.0%	FIBER CEMENT =	205.24 SF	11.0%
METAL PANEL =	13.23 SF	0.5%	METAL PANEL =	205.24 SF	11.0%	METAL PANEL =	205.24 SF	11.0%
CMU =	248.19 SF	8.8%	CMU =	248.19 SF	12.5%	CMU =	248.19 SF	12.5%
<b>8/11TH STREET (STREET FACED) 4,831.41 SF TOTAL</b>			<b>W/O FENESTRATIONS: 4,751.41 SF</b>			<b>MURPHY STREET (STREET FACED) 4,841.41 SF TOTAL</b>		
GLASS =	1,474.20 SF	30.5%	GLASS =	2,388.71 SF	24.0%	GLASS =	2,388.71 SF	24.0%
BRICK =	1,088.24 SF	22.5%	BRICK =	1,852.81 SF	17.0%	BRICK =	1,852.81 SF	17.0%
FIBER CEMENT =	1,115.20 SF	23.0%	FIBER CEMENT =	1,852.81 SF	17.0%	FIBER CEMENT =	1,852.81 SF	17.0%
METAL PANEL =	1,970.15 SF	40.8%	METAL PANEL =	2,372.10 SF	25.0%	METAL PANEL =	2,372.10 SF	25.0%
CMU =	94.23 SF	2.0%	CMU =	133.42 SF	0.1%	CMU =	133.42 SF	0.1%
<b>INTERNAL ELEVATION (EAST) 3,331.00 SF TOTAL</b>			<b>W/O FENESTRATIONS: 2,222.72 SF</b>			<b>INTERNAL ELEVATION (SOUTH) 4,831.41 SF TOTAL</b>		
GLASS =	1,108.78 SF	33.0%	GLASS =	2,388.79 SF	33.0%	GLASS =	2,388.79 SF	33.0%
BRICK =	8.51 SF	0.3%	BRICK =	8.51 SF	0.3%	BRICK =	8.51 SF	0.3%
FIBER CEMENT =	61.23 SF	1.8%	FIBER CEMENT =	1,684.10 SF	34.0%	FIBER CEMENT =	1,684.10 SF	34.0%
METAL PANEL =	1,920.67 SF	57.6%	METAL PANEL =	1,684.10 SF	34.0%	METAL PANEL =	1,684.10 SF	34.0%
CMU =	424.28 SF	12.6%	CMU =	807.40 SF	13.0%	CMU =	807.40 SF	13.0%
<b>WINDOW RECESSES BY MATERIAL:</b>								
BRICK:	5.50"		GLASS:	5.50"		GLASS:	5.50"	
FIBER CEMENT:	5.50"		BRICK:	5.50"		BRICK:	5.50"	
METAL PANEL:	5.50"		FIBER CEMENT:	5.50"		FIBER CEMENT:	5.50"	
CMU:	5.50"		METAL PANEL:	5.50"		METAL PANEL:	5.50"	

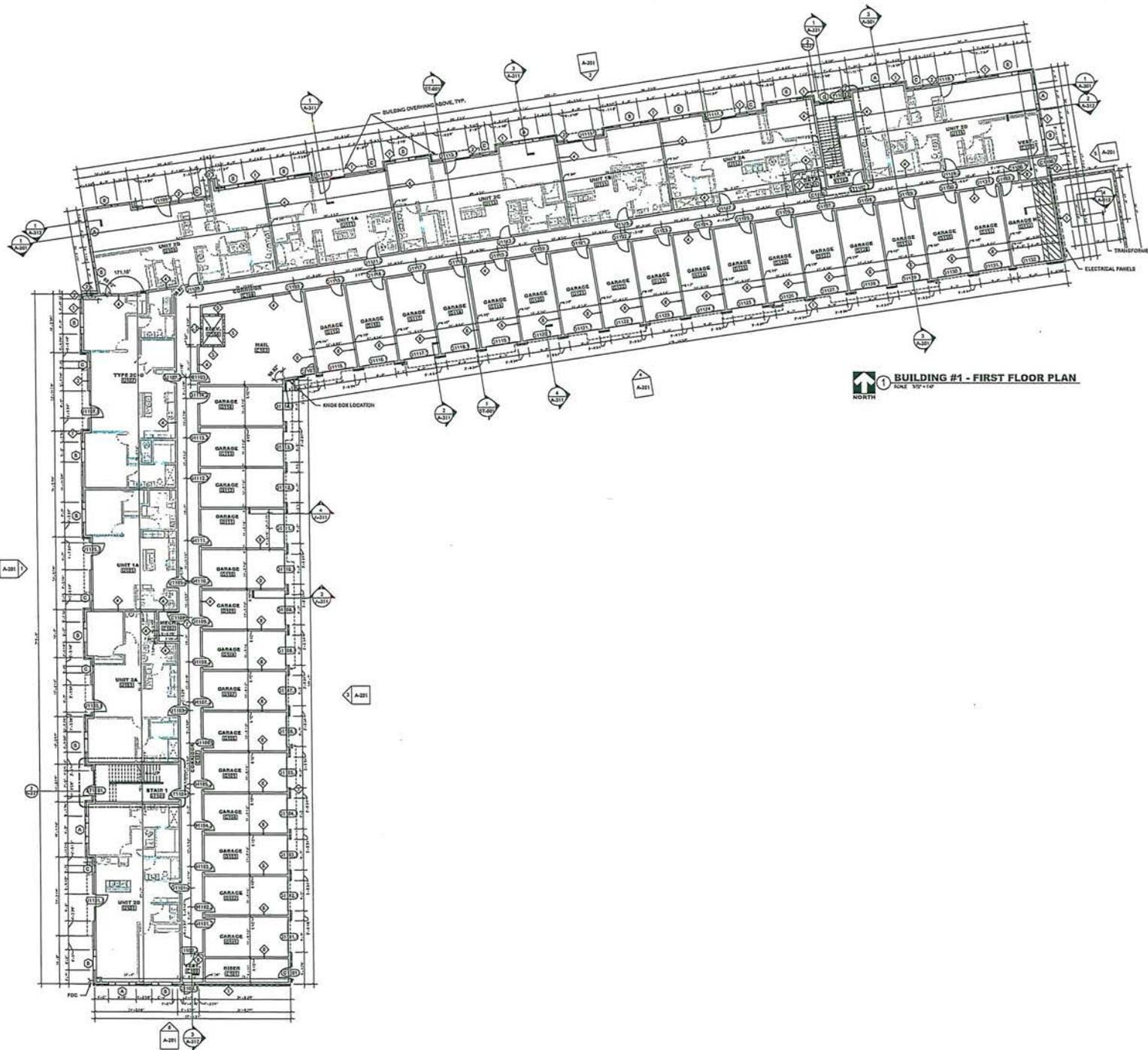


**TELUS CONDOS**  
DES MOINES, IA

NO. 101  
PH. 563.261.1111  
21102  
SP

BLDG #2 - EXTERIOR ELEVATIONS COLORED.

M-202



**BUILDING #1 - FIRST FLOOR PLAN**  
SCALE: 1/8" = 1'-0"

**GENERAL FLOOR PLAN NOTES**

1. REFER TO ADDITIONAL NOTES AND REQUIREMENTS ON ALL OTHER DOCUMENTS AND OTHER DISCIPLINE DOCUMENTS.
2. ALL SITE INFO, FURNITURE, FEATURES AND EQUIPMENT SHOWN SHOULD BE CONSIDERED CONCEPTUAL AND MAY HAVE TO BE PROVIDED FOR COORDINATION PURPOSES ONLY. REFER TO FINAL FIRE SUPPRESSION, PLUMBING, MECHANICAL, ELECTRICAL, COMMUNICATIONS, ELECTRONIC SAFETY & SECURITY DOCUMENTS AND CONSULTANTS FOR SPECIFIC DESIGN INFORMATION & REQUIREMENTS.
3. DIMENSIONS ARE TO FACE OF FINISH MATERIAL OR CENTER OF STRUCTURE UNLESS OTHERWISE SPECIFIED.
4. ALL CONCRETE, PIPE, WIRING, HOV/DM, ETC. SHOULD BE HELD AS TIGHT AS POSSIBLE TO THE ROOF OR FLOOR JOCK OR SHOULD BE KEPT WITHIN INTERIOR WALL CENTER. NOTING SHALL BE SECTION THROUGH EDGE OF MAIN BUILDING FRAMES. NOTING SHALL BE EXPRESSED IN FINISHED SPACES. NOTING SHALL PREVENT A SMOOTH COURTEOUS FINISH WALL OR CEILING AS NOTED ON THE DRAWINGS AND NO FINISHING RUN IN EXTERIOR WALLS UNLESS PRIOR APPROVAL IS GRANTED BY THE ARCHITECT OR OWNER. PLACEMENT COORDINATION RETIRED TRADES IS REQUIRED.
5. RETIAL VERTICAL EXPANSION JOINT REMAIN AT COLUMN/STAIR LINES AND AT THE HEAD OF DOOR JAMB/SPRINGS OR AS OTHER NOTES OR CHANGE SHEETS INDICATED PER EXISTING MATERIAL. PRACTICE TO REDUCE STRESS CARCASSING. NO CONTROL JOINTS/EXPANSION JOINT IN LOAD BEARING AREAS. STAY AWAY 2'-0" MIN.
6. CHECK & SEAL ALL CONTROL-EXPANSION SCAWOUT JOINTS AT ALL INTERIOR & EXTERIOR MASONRY & CONCRETE.
7. PROVIDE LOWER LEVEL AND/OR UNDER-FLOOR DRAINAGE SYSTEM TO REDUCE FLOODING RISK AND PROTECT AGAINST NEGATIVE DRAINAGE. PROVIDE PROTECT AGAINST DRAINAGE SYSTEM TO REDUCE LATERAL PRESSURES AND FOUNDATION WALL DRAINAGE SYSTEM TO REDUCE LATERAL PRESSURES AND APPROPRIATE BELLS.
8. PROVIDE POSITIVE DRAINAGE OF SURFACE WATER AWAY FROM BUILDING WITHOUT PONDING OF WATER ADJACENT TO BUILDING OR ON PAVEMENTS.

**BUILDING #1 UNIT MIX:**

**FIRST FLOOR**

UNIT 1A	2
UNIT 1B	2
UNIT 2A	1
UNIT 2B	1
UNIT 2C	1
UNIT 2D	1
UNIT 2E	1
UNIT 2F	1
UNIT 2G	1
UNIT 2H	1
UNIT 2I	1
UNIT 2J	1
UNIT 2K	1
UNIT 2L	1
UNIT 2M	1
UNIT 2N	1
UNIT 2O	1
UNIT 2P	1
UNIT 2Q	1
UNIT 2R	1
UNIT 2S	1
UNIT 2T	1
UNIT 2U	1
UNIT 2V	1
UNIT 2W	1
UNIT 2X	1
UNIT 2Y	1
UNIT 2Z	1
TOTAL	18 (31 REG GARAGES, 1 ADA GARAGES)

**SECOND FLOOR**

UNIT 1A	4
UNIT 1B	2
UNIT 2A	2
UNIT 2B	2
UNIT 2C	2
UNIT 2D	2
UNIT 2E	2
UNIT 2F	2
UNIT 2G	2
UNIT 2H	2
UNIT 2I	2
UNIT 2J	2
UNIT 2K	2
UNIT 2L	2
UNIT 2M	2
UNIT 2N	2
UNIT 2O	2
UNIT 2P	2
UNIT 2Q	2
UNIT 2R	2
UNIT 2S	2
UNIT 2T	2
UNIT 2U	2
UNIT 2V	2
UNIT 2W	2
UNIT 2X	2
UNIT 2Y	2
UNIT 2Z	2
TOTAL	18

**THIRD FLOOR**

UNIT 1A	4
UNIT 1B	2
UNIT 2A	2
UNIT 2B	2
UNIT 2C	2
UNIT 2D	2
UNIT 2E	2
UNIT 2F	2
UNIT 2G	2
UNIT 2H	2
UNIT 2I	2
UNIT 2J	2
UNIT 2K	2
UNIT 2L	2
UNIT 2M	2
UNIT 2N	2
UNIT 2O	2
UNIT 2P	2
UNIT 2Q	2
UNIT 2R	2
UNIT 2S	2
UNIT 2T	2
UNIT 2U	2
UNIT 2V	2
UNIT 2W	2
UNIT 2X	2
UNIT 2Y	2
UNIT 2Z	2
TOTAL	18

**TOTAL**

UNIT 1A	15	17 1-BEDROOMS + 20K
UNIT 1B	4	
UNIT 2A	4	
UNIT 2B	2	
UNIT 2C	2	
UNIT 2D	2	
UNIT 2E	2	
UNIT 2F	2	
UNIT 2G	2	
UNIT 2H	2	
UNIT 2I	2	
UNIT 2J	2	
UNIT 2K	2	
UNIT 2L	2	
UNIT 2M	2	
UNIT 2N	2	
UNIT 2O	2	
UNIT 2P	2	
UNIT 2Q	2	
UNIT 2R	2	
UNIT 2S	2	
UNIT 2T	2	
UNIT 2U	2	
UNIT 2V	2	
UNIT 2W	2	
UNIT 2X	2	
UNIT 2Y	2	
UNIT 2Z	2	
TOTAL	44 (22 GARAGES)	

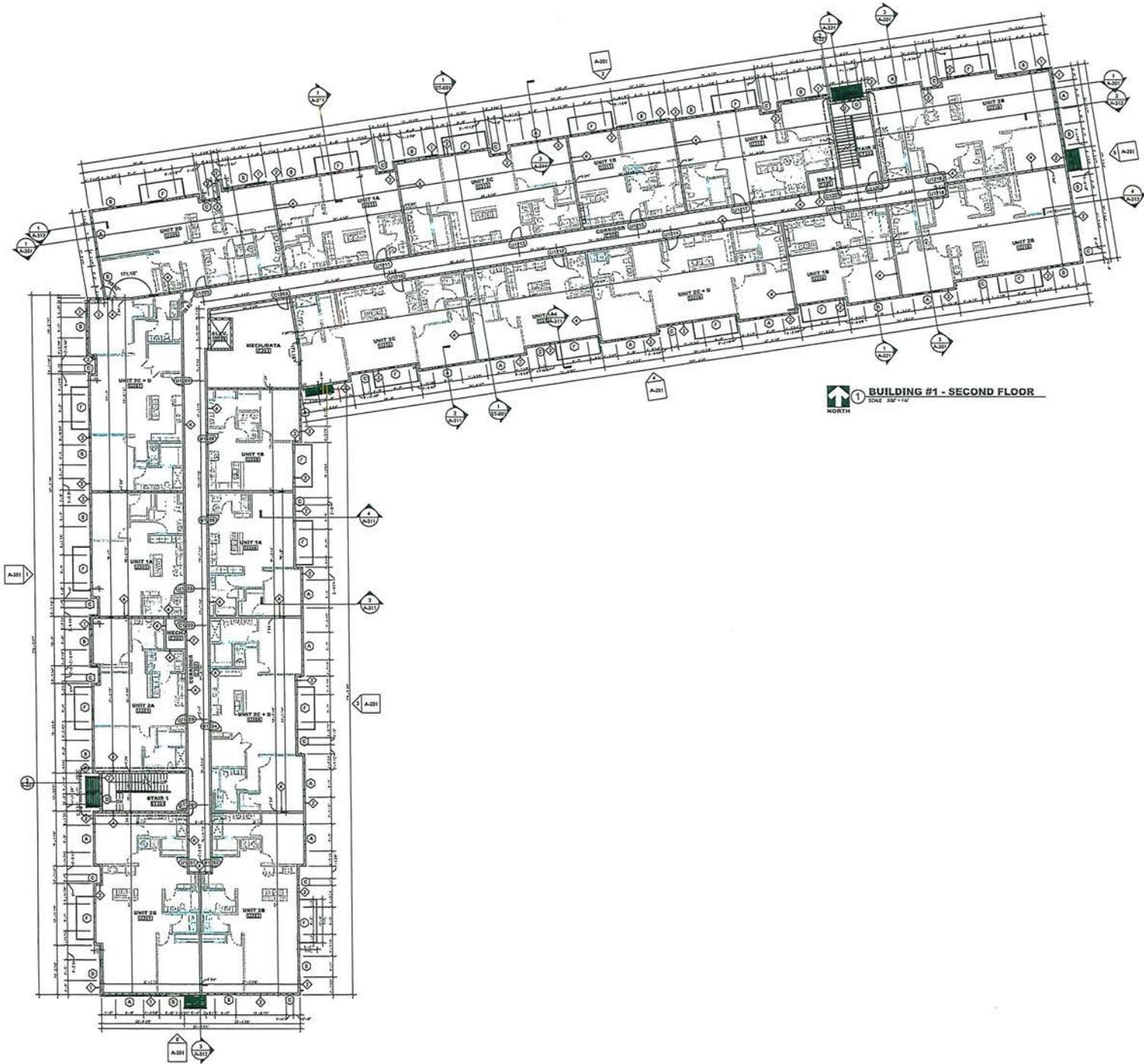
**SIMONSON**  
ARCHITECTS  
1117 13TH AVE. S.W.  
DES MOINES, IA 50315  
515.281.1111  
WWW.SIMONSONARCHITECTS.COM

**Hubbell**  
HUBBELL REALTY COMPANY

**TELUS CONDOS**  
DES MOINES, IA

NO. OF SHEETS	18
SHEET NO.	18 OF 18
DATE	10/20/2017
DESIGNED BY	JAMES E. HUBBELL
CHECKED BY	JAMES E. HUBBELL
DATE	10/20/2017

Job No. 21182  
Sheet Title: BUILDING #1 - FIRST FLOOR PLAN  
Date: 10/20/2017  
Scale: 1/8" = 1'-0"



**BUILDING #1 - SECOND FLOOR**  
 NORTH  
 SCALE 3/8" = 1'-0"

**GENERAL FLOOR PLAN NOTES**

1. REFER TO ADDITIONAL NOTES AND REQUIREMENTS ON ALL OTHER DOCUMENTS AND OTHER DISCIPLINE DOCUMENTS.
2. ALL SIZE INFO, FURNITURE, FIXTURES AND EQUIPMENT SHOWN SHOULD BE CONSIDERED CONCEPTUAL AND MAY HAVE BE PROVIDED FOR COORDINATION PURPOSES ONLY. REFER TO CIVIL, FIRE SUPPRESSION, PLUMBING, MECHANICAL, ELECTRICAL, COMMUNICATIONS, ELECTRONIC SAFETY & SECURITY DOCUMENTS AND CONSULTANTS FOR SPECIFIC DESIGN INFORMATION & REQUIREMENTS.
3. DIMENSIONS ARE TO FACE OF FINISH MATERIAL OR CENTER OF STRUCTURE UNLESS OTHERWISE SPECIFIED.
4. ALL CONDUITS, PIPES, WIRING, ROUGH-IN, ETC. SHOULD BE WELDED AS TIGHT AS POSSIBLE TO THE ROOF OR FLOOR DECK OR SHOULD BE KEPT WITHIN INTERIOR WALL CAVITIES. NOTHING SHALL BE BELOW THE BOTTOM EDGE OF ANY BUILDING FRAME. NOTHING SHALL BE EXPOSED IN FINISHED SPACES. NOTHING SHALL PREVENT A SMOOTH CONTINUOUS FINISHED WALL OR CEILING AS NOTED BY THE ARCHITECT AND NO PIPING OR AIR EXTERIOR WALLS UNLESS PRIOR APPROVAL BETWEEN TRADES IS REQUIRED.
5. INSTALL VERTICAL EXPANSION JOINTS WHERE AT COLLIMATED LINES AND AT THE HEAD OF DOOR JAMBSPENETRAE OR AS OTHERWISE NOTED OR WHERE REQUIRED FOR STANDARD MATERIAL PRACTICE TO REDUCE STRESS CRACKING. NO CONTROL JOINT/EXPANSION JOINT IN LOAD BEARING AREAS. STAY AWAY 2'-0" MIN.
6. CHECK & SEAL ALL CONTROL-EXPANSION, SAWCUT JOINTS AT ALL INTERIORS & EXTERIOR HARDWARE & CONCRETE.
7. PROVIDE LOWER LEVEL DRAIN UNDER FLOOR DRAINAGE SYSTEM TO REDUCE HYDROSTATIC UPLIFT AND PROTECT AGAINST MOISTURE SEEPAGE. PROVIDE PROTECTIVE WALL DRAINAGE SYSTEM TO REMOVE LATERAL PRESSURES AND PROTECT AGAINST MOISTURE SEEPAGE. BOTH SYSTEMS SHALL OUTLET TO APPROPRIATE RELIEF.
8. PROVIDE POSITIVE DRAINAGE OF SURFACE WATER AWAY FROM BUILDING WITHOUT POOLING OF WATER ADJACENT TO BUILDING OR ON PAVEMENTS.

**BUILDING #1 UNIT MIX:**

**FINAL FLOOR**

UNIT 1A	2
UNIT 1B	2
UNIT 2A	1
UNIT 2B	1
UNIT 2C	1
UNIT 2D	1
TOTAL	10 (01 REG GARAGES, 1 ADA GARAGES)

**SECOND FLOOR**

UNIT 1A	4
UNIT 1B	4
UNIT 2A	2
UNIT 2B	2
UNIT 2C	1
UNIT 2D	1
UNIT 2E	1
TOTAL	18

**THIRD FLOOR**

UNIT 1A	4
UNIT 1B	4
UNIT 2A	4
UNIT 2B	4
UNIT 2C	2
UNIT 2D	2
UNIT 2E	2
TOTAL	18

**TOTALS**

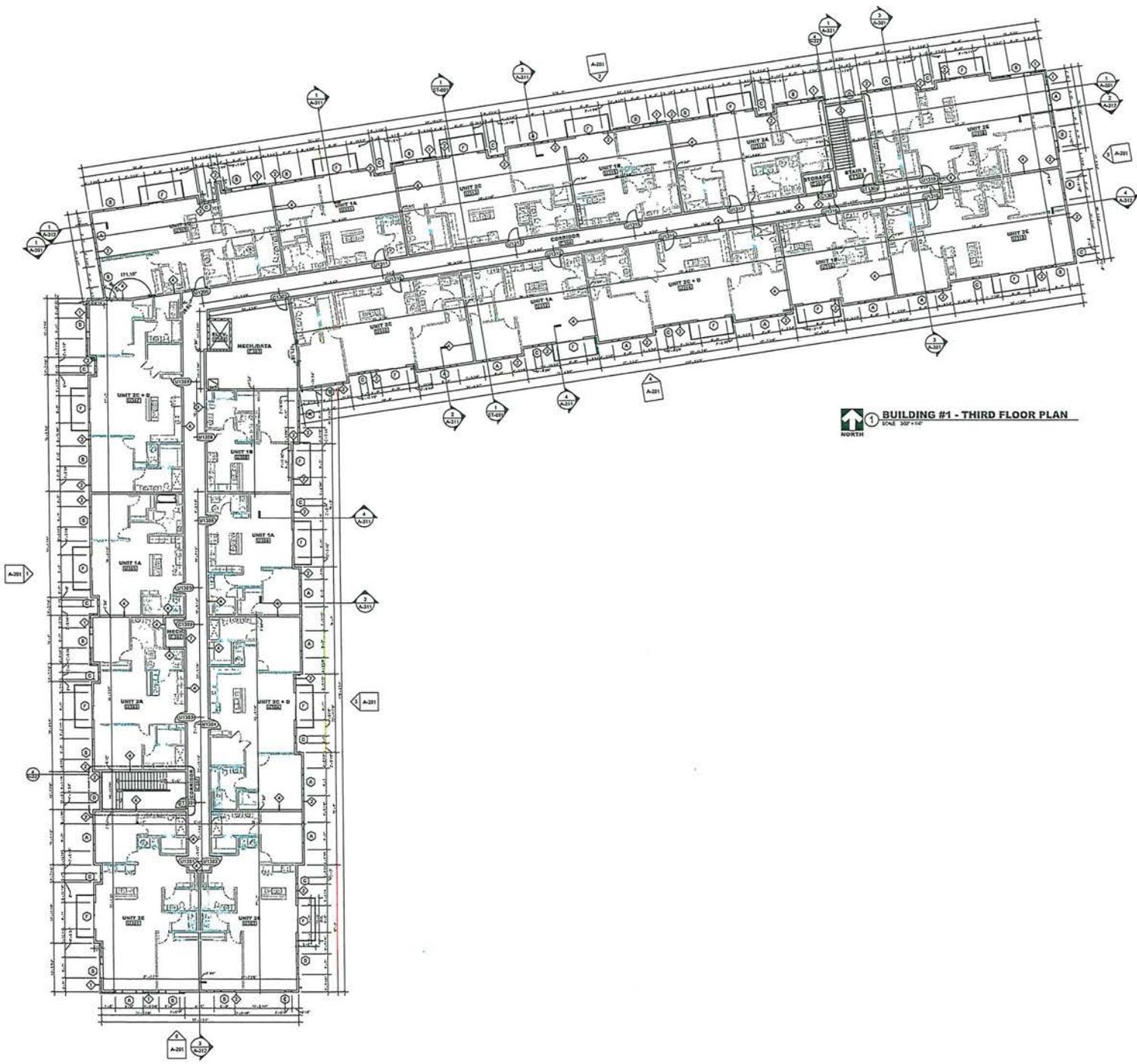
UNIT 1A	10	IT 1-BEDROOMS - 25%
UNIT 1B	7	
UNIT 2A	6	
UNIT 2B	6	
UNIT 2C	3	
UNIT 2D	3	
UNIT 2E	3	
TOTAL	48 (03 GARAGES)	



**TELUS CONDOS**  
 DES MOINES, IA

DATE	10/20/2017
BY	SP
SCALE	AS SHOWN
PROJECT	TELUS CONDOS
NO. OF SHEETS	10
SHEET NO.	10

21/152  
 SP  
 BUILDING #1 - SECOND FLOOR PLAN



**1 BUILDING #1 - THIRD FLOOR PLAN**  
SCALE: 3/8" = 1'-0"

**GENERAL FLOOR PLAN NOTES**

1. REFER TO ADDITIONAL NOTES AND REQUIREMENTS ON ALL OTHER DRAWINGS AND OTHER DISCIPLINE DRAWINGS.
2. ALL LITE SIFU, FURNITURE, FIXTURES AND EQUIPMENT SHOWN SHOULD BE CONSIDERED CONCEPTUAL AND MAY HAVE BE PROVIDED FOR COORDINATION PURPOSES ONLY. REFER TO ENR, FIRE SUPPRESSION, PLUMBING, MECHANICAL, ELECTRICAL, COMMUNICATIONS, ELECTRONIC SAFETY & SECURITY DOCUMENTS AND CONSULTANTS FOR SPECIFIC DESIGN INFORMATION & REQUIREMENTS.
3. DIMENSIONS ARE TO FACE OF FINISH MATERIAL OR CENTER OF STRUCTURE UNLESS OTHERWISE SPECIFIED.
4. ALL CONCRETE, PIPES, WIRING, ROUGH-IN, ETC. SHOULD BE WELD AS TIGHT AS POSSIBLE TO THE ROOF OR FLOOR DECK OR SHOULD BE KEPT WITHIN INTERIOR WALL CASING. NOTHING SHALL BE BELOW THE BOTTOM EDGE OF MAIN BUILDING FRAMES. NOTHING SHALL BE EXPOSED IN FINISHED SPACES. NOTHING SHALL PREVENT A SMOOTH CONTINUOUS FINISH WALL OR CEILING AS NOTED ON THE DRAWING AND NO PLUMBING RUN IN EXTERIOR WALLS UNLESS PRIOR APPROVAL AND CONSULTANTS FOR SPECIFIC DESIGN INFORMATION & REQUIREMENTS. BETWEEN TRADES IS REQUIRED.
5. RETAIN VERTICAL EXPANSION JOINT ANNUAL AT COLLECTOR LINE AND AT THE HEAD OF BOOT JAMB/SOPENINGS OR AS OTHERWISE NOTED OR WHERE REQUIRED PER STANDARD PRACTICE TO REDUCE STRESS CRACKING. NO CONTROL/EXPANSION JOINT IN LEAD BEARING AREAS. STAY AWAY 2'-0" MIN.
6. CAST & SEAL ALL CONTROL/EXPANSION SAWCUT JOINTS AT ALL INTERIOR & EXTERIOR MASONRY & CONCRETE.
7. PROVIDE LOWER LEVEL AND/OR UNDER FLOOR DRAINAGE SYSTEM TO REDUCE PROTECTING UNITS AND PROTECT AGAINST POSITIVE DRAINAGE. PROVIDE PROTECTING WALL DRAINAGE SYSTEM TO REDUCE LATERAL PRESSURE AND PROTECT AGAINST POSITIVE DRAINAGE. BOTH SYSTEMS SHALL OUTLET TO APPROPRIATE HELD.
8. PROVIDE POSITIVE DRAINAGE OF SURFACE WATER AWAY FROM BUILDING WITHOUT POONDING OF WATER ADJACENT TO BUILDING OR ON PAVEMENTS.

**BUILDING #1 UNIT MIX:**

**FIRST FLOOR**

UNIT 1B	1
UNIT 1C	1
UNIT 2B	1
UNIT 2C-D	1
UNIT 2D	1
TOTAL	5 (31 REG GARAGES, 1 ADA GARAGES)

**SECOND FLOOR**

UNIT 1A	4
UNIT 2A	4
UNIT 2C	4
UNIT 2D	4
UNIT 2E	4
TOTAL	20

**THIRD FLOOR**

UNIT 1A	4
UNIT 1B	4
UNIT 2A	4
UNIT 2C	4
UNIT 2D	4
TOTAL	20

**TOTALS**

UNIT 1A	12	17 1-BEDROOMS + 2IN
UNIT 1B	5	
UNIT 2A	8	
UNIT 2B	2	
UNIT 2C	9	
UNIT 2D	5	
UNIT 2E	4	
TOTAL	45 (22 GARAGES)	



Architectural, Mechanical, Electrical, Plumbing, Fire Protection, Structural, and Civil Engineering Services  
1117 EAST 17TH AVENUE, SUITE 100  
DENVER, CO 80202  
(303) 733-8888



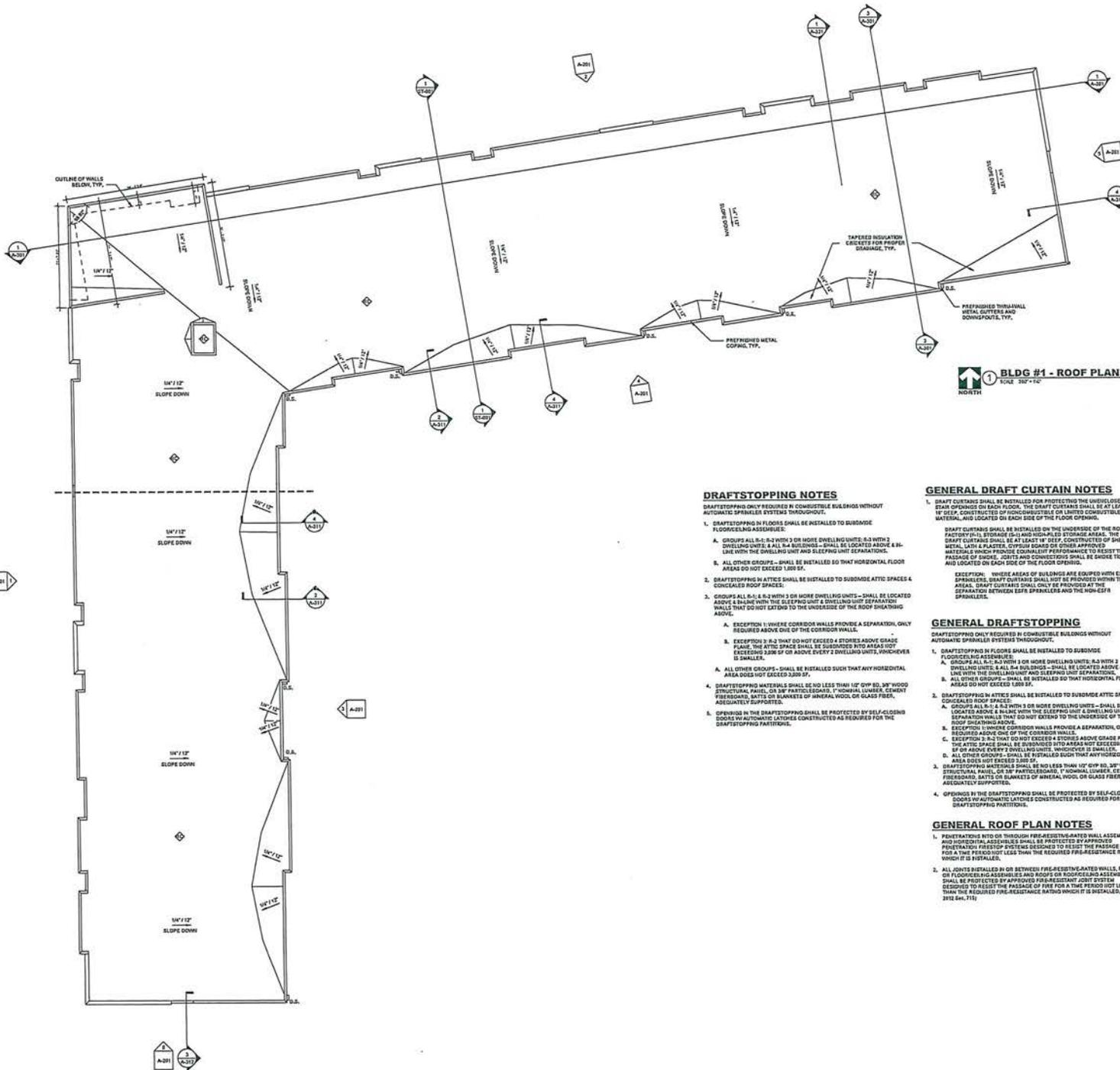
HUBBELL REALTY COMPANY

**TELUS CONDOS**  
DES MOINES, IA

DATE	10-20-2022
BY	10-20-2022
PROJECT	TELUS CONDOS
UNIT/FLOOR	3RD FLOOR
DESIGNED BY	10-20-2022
CHECKED BY	10-20-2022

DATE: 10-20-2022  
SCALE: 3/8" = 1'-0"  
PROJECT: TELUS CONDOS  
UNIT/FLOOR: 3RD FLOOR  
DRAWN BY: 10-20-2022  
CHECKED BY: 10-20-2022

**A-103**



**BLDG #1 - ROOF PLAN**  
SCALE 3/8" = 1'-0"

**DRAFTSTOPPING NOTES**

- DRAFTSTOPPING ONLY REQUIRED IN COMBUSTIBLE BUILDINGS WITHOUT AUTOMATIC SPARKLER SYSTEMS THROUGHOUT.
- DRAFTSTOPPING IN FLOORS SHALL BE INSTALLED TO SUBGRADE FLOORCEILING ASSEMBLIES.
    - GROUPS ALL 8'-0" W/THS OR MORE DWELLING UNITS & WITH 2 DWELLING UNITS & ALL 84 BUILDINGS - SHALL BE LOCATED ABOVE & IN-LINE WITH THE DWELLING UNIT AND SLEEPING UNIT SEPARATIONS.
    - ALL OTHER GROUPS - SHALL BE INSTALLED SO THAT HORIZONTAL FLOOR AREAS DO NOT EXCEED 1,600 SF.
  - DRAFTSTOPPING IN ATTICS SHALL BE INSTALLED TO SUBGRADE ATTIC SPACES & CONCEALED ROOF SPACES.
    - GROUPS ALL 8'-0" W/THS OR MORE DWELLING UNITS - SHALL BE LOCATED ABOVE & IN-LINE WITH THE DWELLING UNIT & SLEEPING UNIT SEPARATIONS. WALLS THAT DO NOT EXTEND TO THE UNDERSIDE OF THE ROOF SHEATHING ABOVE.
    - EXCEPTION 1: WHERE CORRIDOR WALLS PROVIDE A SEPARATION, ONLY REQUIRED ABOVE ONE OF THE CORRIDOR WALLS.
    - EXCEPTION 2: 8'-0" THAT DO NOT EXCEED 4 STORES ABOVE GRADE PLANE, THE ATTIC SPACE SHALL BE SUBGRADED INTO AREAS NOT EXCEEDING 3,000 SF OR ABOVE EVERY 2 DWELLING UNITS, WHICHEVER IS SMALLER.
  - ALL OTHER GROUPS - SHALL BE INSTALLED SUCH THAT ANY HORIZONTAL AREA DOES NOT EXCEED 3,000 SF.
  - DRAFTSTOPPING MATERIALS SHALL BE NO LESS THAN 1/2" OVP 80, 3/4" WOOD STRUCTURAL PANEL, OR 3/8" PARTICLEBOARD, 1" NOMINAL LUMBER, CEMENT FIBERBOARD, MATS OR BLANKETS OF MINERAL WOOL OR GLASS FIBER, ADEQUATELY SUPPORTED.
  - OPENINGS IN THE DRAFTSTOPPING SHALL BE PROTECTED BY SELF-CLOSING DOORS W/ AUTOMATIC LATCHES CONSTRUCTED AS REQUIRED FOR THE DRAFTSTOPPING PARTITIONS.
    - EXCEPTION 1: WHERE CORRIDOR WALLS PROVIDE A SEPARATION, ONLY REQUIRED ABOVE ONE OF THE CORRIDOR WALLS.
    - EXCEPTION 2: 8'-0" THAT DO NOT EXCEED 4 STORES ABOVE GRADE PLANE, THE ATTIC SPACE SHALL BE SUBGRADED INTO AREAS NOT EXCEEDING 3,000 SF OR ABOVE EVERY 2 DWELLING UNITS, WHICHEVER IS SMALLER.
  - OPENINGS IN THE DRAFTSTOPPING SHALL BE PROTECTED BY SELF-CLOSING DOORS W/ AUTOMATIC LATCHES CONSTRUCTED AS REQUIRED FOR THE DRAFTSTOPPING PARTITIONS.

**GENERAL DRAFT CURTAIN NOTES**

- DRAFT CURTAINS SHALL BE INSTALLED FOR PROTECTING THE UNINSULATED SEAM OPENINGS ON EACH FLOOR. THE DRAFT CURTAINS SHALL BE AT LEAST 1/2" SEEN, CONSTRUCTED OF FIBERGLASS/STYRENE OR LIMITED COMBUSTIBLE MATERIAL, AND LOCATED ON EACH SIDE OF THE FLOOR OPENING.
 

DRAFT CURTAINS SHALL BE INSTALLED ON THE UNDERSIDE OF THE ROOF IN FACTORY-FINISHED CEILING AND HIGH-CEILING STORAGE AREAS. THE DRAFT CURTAINS SHALL BE AT LEAST 1/2" SEEN, CONSTRUCTED OF SHEET METAL, LATH & PLASTER, COPPER SHEATH OR COVER APPROVED METATERIALS WITH A MINIMUM 1" CLEARANCE TO THE UNDERSIDE OF THE PASSAGE OF SMOKE, HEAT AND CONNECTIONS SHALL BE SNUG TIGHT, AND LOCATED ON EACH SIDE OF THE FLOOR OPENING.

EXCEPTION: WHERE AREAS OF BUILDINGS ARE EQUIPPED WITH ESTE SPARKLERS, DRAFT CURTAINS SHALL NOT BE PROVIDED WITH THESE AREAS. DRAFT CURTAINS SHALL ONLY BE PROVIDED AT THE SEPARATION BETWEEN ESTE SPARKLERS AND THE NON-ESTE SPARKLERS.

**GENERAL DRAFTSTOPPING**

- DRAFTSTOPPING ONLY REQUIRED IN COMBUSTIBLE BUILDINGS WITHOUT AUTOMATIC SPARKLER SYSTEMS THROUGHOUT.
- DRAFTSTOPPING IN FLOORS SHALL BE INSTALLED TO SUBGRADE FLOORCEILING ASSEMBLIES.
    - GROUPS ALL 8'-0" W/THS OR MORE DWELLING UNITS & WITH 2 DWELLING UNITS & ALL 84 BUILDINGS - SHALL BE LOCATED ABOVE & IN-LINE WITH THE DWELLING UNIT AND SLEEPING UNIT SEPARATIONS.
    - ALL OTHER GROUPS - SHALL BE INSTALLED SO THAT HORIZONTAL FLOOR AREAS DO NOT EXCEED 1,600 SF.
  - DRAFTSTOPPING IN ATTICS SHALL BE INSTALLED TO SUBGRADE ATTIC SPACES & CONCEALED ROOF SPACES.
    - GROUPS ALL 8'-0" W/THS OR MORE DWELLING UNITS - SHALL BE LOCATED ABOVE & IN-LINE WITH THE DWELLING UNIT & SLEEPING UNIT SEPARATIONS. WALLS THAT DO NOT EXTEND TO THE UNDERSIDE OF THE ROOF SHEATHING ABOVE.
    - EXCEPTION 1: WHERE CORRIDOR WALLS PROVIDE A SEPARATION, ONLY REQUIRED ABOVE ONE OF THE CORRIDOR WALLS.
    - EXCEPTION 2: 8'-0" THAT DO NOT EXCEED 4 STORES ABOVE GRADE PLANE, THE ATTIC SPACE SHALL BE SUBGRADED INTO AREAS NOT EXCEEDING 3,000 SF OR ABOVE EVERY 2 DWELLING UNITS, WHICHEVER IS SMALLER.
  - ALL OTHER GROUPS - SHALL BE INSTALLED SUCH THAT ANY HORIZONTAL AREA DOES NOT EXCEED 3,000 SF.
  - DRAFTSTOPPING MATERIALS SHALL BE NO LESS THAN 1/2" OVP 80, 3/4" WOOD STRUCTURAL PANEL, OR 3/8" PARTICLEBOARD, 1" NOMINAL LUMBER, CEMENT FIBERBOARD, MATS OR BLANKETS OF MINERAL WOOL OR GLASS FIBER, ADEQUATELY SUPPORTED.
  - OPENINGS IN THE DRAFTSTOPPING SHALL BE PROTECTED BY SELF-CLOSING DOORS W/ AUTOMATIC LATCHES CONSTRUCTED AS REQUIRED FOR THE DRAFTSTOPPING PARTITIONS.

**GENERAL ROOF PLAN NOTES**

- PENETRATIONS INTO OR THROUGH FIRE-RESISTIVE-RATED WALL ASSEMBLIES AND HORIZONTAL ASSEMBLIES SHALL BE PROTECTED BY APPROVED PENETRATION FIRESTOP SYSTEMS DESIGNED TO RESIST THE PASSAGE OF FIRE FOR A TIME PERIOD NOT LESS THAN THE REQUIRED FIRE-RESISTANCE RATING WHICH IT IS INSTALLED. (IBC 2018 544.711)
- ALL JOISTS INSTALLED OR BETWEEN FIRE-RESISTIVE-RATED WALLS, FLOORS OR FLOORCEILING ASSEMBLIES AND ROOFS OR ROOFCEILING ASSEMBLIES SHALL BE PROTECTED BY APPROVED FIRE-RESISTIVE JOIST SYSTEMS DESIGNED TO RESIST THE PASSAGE OF FIRE FOR A TIME PERIOD NOT LESS THAN THE REQUIRED FIRE-RESISTANCE RATING WHICH IT IS INSTALLED. (IBC 2018 544.711)

**GENERAL ROOF PLAN NOTES**

- REFER TO ADDITIONAL NOTES AND REFERENCES ON ALL OTHER DOCUMENTS AND OTHER DISCIPLINE DOCUMENTS.
- ALL FEATURES AND EQUIPMENT SHOWN SHOULD BE CONSIDERED CONCEPTUAL AND MAY BE PROVIDED FOR COORDINATION PURPOSES ONLY. REFER TO ELEC., FIRE SUPPRESSION, PLUMBING, MECHANICAL, ELECTRICAL, COMMUNICATIONS, ELECTRONIC SAFETY & SECURITY DOCUMENTS FOR COORDINATION FOR SPECIFIC DESIGN INFORMATION & REQUIREMENTS. REFER TO STRUCTURAL DRAWINGS FOR ROOF OPENING, STRUCTURAL, FRAMING.
- ALL ROOF PENETRATIONS FOR UNIFORM ROOF VENTS AND SIMILAR ITEMS SHALL NOT BE SHOWN ON THE ROOF PLAN. FINISHES FOR ROOF FLASHINGS SHALL BE PROVIDED BY THE ROOFING SUB-CONTRACTOR SO AS TO OBTAIN FULL EOOD WORKMANSHIP. REFER TO ALL OTHER CONSTRUCTION DOCUMENTS FOR ADDITIONAL ROOF PENETRATIONS REGARDING ROOF FLASHING WORK.
- IT SHOULD BE THE RESPONSIBILITY OF THE ROOF SUB-CONTRACTOR TO PROVIDE ALL NECESSARY DETAILS AND SYSTEMS TO ACCOMMODATE AERODYNAMIC FORCE OF THE BUILDING AND BUILDING COMPONENTS FOR LONG-LASTING LIFE.
- CONTRACTOR SHALL KEEP ROOF CLEAN OF ALL DEBRIS DURING ALL CONSTRUCTION WORK.
- ROOF SYSTEM AND ANY/VVOR ASSOCIATED WITH THE ROOF SHALL BE DONE AS TO NOT VOID ANY ROOF WARRANTIES.
- FLASHING:
  - FLASHING SHALL BE INSTALLED IN SUCH A MANNER SO AS TO PREVENT WATER FROM ENTERING THE ROOF THROUGH JOISTS OR COPINGS & AT INTERSECTIONS WITH PARAPET WALLS & OTHER PENETRATIONS THROUGH THE ROOF PANEL.
  - FLASHING SHALL BE INSTALLED AT WALL AND ROOF INTERSECTIONS, AT GUTTERS, WHEREVER THERE IS A CHANGE IN ROOF SLOPE OR DIRECTION AND AROUND ROOF OPENINGS, WHERE FLASHING IS NOT, THE METAL SHALL BE CORROSION RESISTANT WITH A THICKNESS OF NOT LESS THAN 3/16" AND BE GALVANNEUM COATED.
  - PARAPET WALLS SHALL BE PROPERLY COPED WITH NON-COMBUSTIBLE WEATHERING DETAILS OF A WIDTH NO LESS THAN THE THICKNESS OF THE PARAPET WALL.
- ROOF DRAINAGE:
  - DESIGN OF ROOF DRAINAGE SYSTEMS SHALL COMPLY WITH THE INTL. PLUMBING CODE. FINAL DETAILS DESIGN OF ROOF DRAINAGE SYSTEM TO BE BY PLUMBING CONSULTANT.
  - OVERSIZED ROOF DRAINS, GUTTERS AND/OR DOWNPOUTS SHOULD BE PROVIDED TO ELIMINATE ANY OVERFLOW OR BACKUP.
  - ROOF LESS THAN 2 ROOF DRAINS SHALL BE INSTALLED IN ROOF AREAS 10,000 SF AND ROOF LESS THAN FOUR ROOF DRAINS SHALL BE INSTALLED IN ROOF'S OVER 10,000 SF IN AREA.
  - WHERE ROOF DRAINS ARE REQUIRED, SECONDARY (EMERGENCY) ROOF DRAINS OR EQUIPPED SHALL BE PROVIDED WHERE THE ROOF PENETRATION CONSTRUCTION EXTENDS ABOVE THE ROOF IN SUCH A MANNER THAT WATER WILL BE CAPTURED IF THE PRIMARY DRAIN ALONG BUILDUP FOR ANY REASON.
    - SECONDARY ROOF DRAIN SYSTEMS SHALL HAVE THE END POINT OF DISCHARGE SEPARATE FROM THE PRIMARY SYSTEM. DISCHARGE SHALL BE ABOVE GRADE & LOCATED THAT WOULD USUALLY BE OCCUPIED BY THE BUILDING OCCUPANTS OR MAINTENANCE PERSONNEL.
    - SECONDARY EQUIPPERS SHALL BE SEED TO PREVENT THE DEPTH OF PENETRATING WATER FROM EXCEEDING THAT FOR WHICH THE ROOFING IS DESIGNED. THEY SHALL HAVE AN OPENING DIMENSION OF NOT LESS THAN 1/2".
    - A CHECKER OR SADDLE SHALL BE INSTALLED ON THE RIDGE SIDE OF ANY PENETRATION GREATER THAN 2" WIDE AS MEASURED PERPENDICULAR TO THE SLOPE.
- ALL ROOF SLOPES, INCLUDING FLATTENED ROOF AREAS TO OBTAIN POSITIVE DRAINAGE, TO BE INTEGRATED INTO UNDESIGNED ROOF SUPPORT DESIGN. DESIGN TAPERED FLOOR INSULATION TO MAINTAIN POSITIVE DRAINAGE. POSITIVE DRAINAGE ALL THE WAY TO ROOF DRAIN FROM ALL PENETRATIONS AND DRAINS IN ROOF SLOPES.
- PROVIDE CONCRETE SPLASH BLOCKS AT ALL DOWNPOUT LOCATIONS.
- ROOF VENTILATION INTAKE & EXHAUST VENTS SHALL BE PROVIDED IN LEAK & ALL ENCLOSED ATTIC & ROOF PARTS SPACES TO CREATE CROSS VENTILATION AS ACCORDANCE WITH CODES & THE MANUFACTURER'S INSTALLATION INSTRUCTIONS.
- INSULATE AND SEAL TO ENSURE ADEQUATE AIR TIGHTNESS BARRIER AROUND ALL EXTERIOR PENETRATIONS TO PREVENT WATER AND INSULATION PUFFING.
- ROOF ACCESS & GUARDS:
  - WHERE EQUIPMENT REQUIRING ACCESS OR APPLIANCES ARE LOCATED ON AN ELEVATED STRUCTURE ON THE ROOF OF A BUILDING SUCH THAT PERSONNEL WILL HAVE TO CLIMB HIGHER THAN 6 FEET ABOVE GRADE TO ACCESS SUCH EQUIPMENT OR APPLIANCES, A PERMANENT INTERIOR OR EXTERIOR MEANS OF ACCESS SHALL BE PROVIDED. SUCH ACCESS SHALL NOT REQUIRE CLIMBING OVER OBSTRUCTIONS GREATER THAN 20" IN HEIGHT OR WALKING ON ROOF'S HORIZONTAL SLOPE GRADIENTS THAN 4:1.
  - WHERE THE ROOF HATCH OPENING OR ROOF EQUIPMENT IS LOCATED WITHIN 6" OF THE ROOF EDGE, SUCH ROOF ACCESS OR ROOF EDGE SHALL BE PROTECTED BY GUARDS, GUARDS TO BE AT ABOVE WALKING SURFACE & PREVENT THE PASSAGE OF A SPHERE 12" DIAMETER.
  - ANY ROOF HATCHES SHALL BE THEMATICALLY MARKED.
  - SEE DETAIL 4041 ACCESS NOTES FOR ADDITIONAL REQUIREMENTS.
- ACCESS TO UNOCCUPIED SPACES
  - CEILING SPACES SHALL BE PROVIDED WITH NO FEWER THAN 1 ACCESS OPENING WHICH SHALL NOT BE LESS THAN 12" X 20".
  - IN OPENINGS OF NO LESS THAN 20" X 20" SHALL BE PROVIDED TO ACCESS ATTIC AREA HAVING A CLEAR HEIGHT OF OVER 30". CLEAR HEADROOM OF ROOF LESS THAN 30" SHALL BE PROVIDED IN THE ATTIC SPACE AT OR ABOVE THE ACCESS OPENING.
  - ACCESS TO MECHANICAL APPLIANCES INSTALLED IN UNDER FLOOR AREAS, ATTIC SPACES AND ON ROOFS SHALL BE PROVIDED AND SHALL BE IN ACCORDANCE WITH THE INTL. MECHANICAL CODE.
  - ACCESS TO MECHANICAL APPLIANCES SHALL BE PROVIDED WITH AN OPENING AND UNRESTRICTED PASSAGEWAY LARGE ENOUGH TO ALLOW REMOVAL OF THE LARGEST APPLIANCE. THE PASSAGEWAY SHALL NOT BE LESS THAN 30" X 20" AND NOT MORE THAN 20" X 20" UNLESS MEASURED FROM THE CENTERLINE OF THE PASSAGEWAY FROM THE OPENING TO THE APPLIANCE. THE PASSAGEWAY SHALL HAVE CONTINUOUS FLOORING NOT LESS THAN 3/4" WIDE. A LEVEL SURFACE SHALL BE PROVIDED TO THE FRONT OF SERVICE SIDE OF THE APPLIANCE.
- PROVIDE ROOF EDGE GUARDS/GUARDS ABOVE ALL EXTERIOR PORTS TO BUILDING.
- ROOF TRUSSES DESIGN BUILT BY TRUSS MANUFACTURER.
- PROVIDE MANUFACTURED ROOF PROTECTED GAS PIPING AND CONDUIT SUPPORTS AS REQUIRED. PAINT ORIGINALLY "YELLOW" W/ HALOGEN FREE CROSSBOND ONLY.
- FLAT (0% SLOPE) ROOF: MEMBRANE ROOF.
  - ROOF MEMBRANE TO EXTEND UP AND OVER TOP OF ANY PARAPET WALLS AND DRAPE OVER FRONT OF WALL. MEMBRANE TO EXTEND UP VERTICALLY UNDER FINISH MATERIAL 1".
  - PROVIDE WALKWAY PADS ON ALL MEMBRANE ROOFING FROM ROOF ACCESS POINTS TO EACH END OF BUILDING.
- SINGLE-PLY MEMBRANE ROOFING:
  - SINGLE-PLY MEMBRANE ROOFING SHALL HAVE A DESIGN SLOPE OF A MINIMUM OF 1/4" IN 12" FOR DRAINAGE.

**SIMONSON**  
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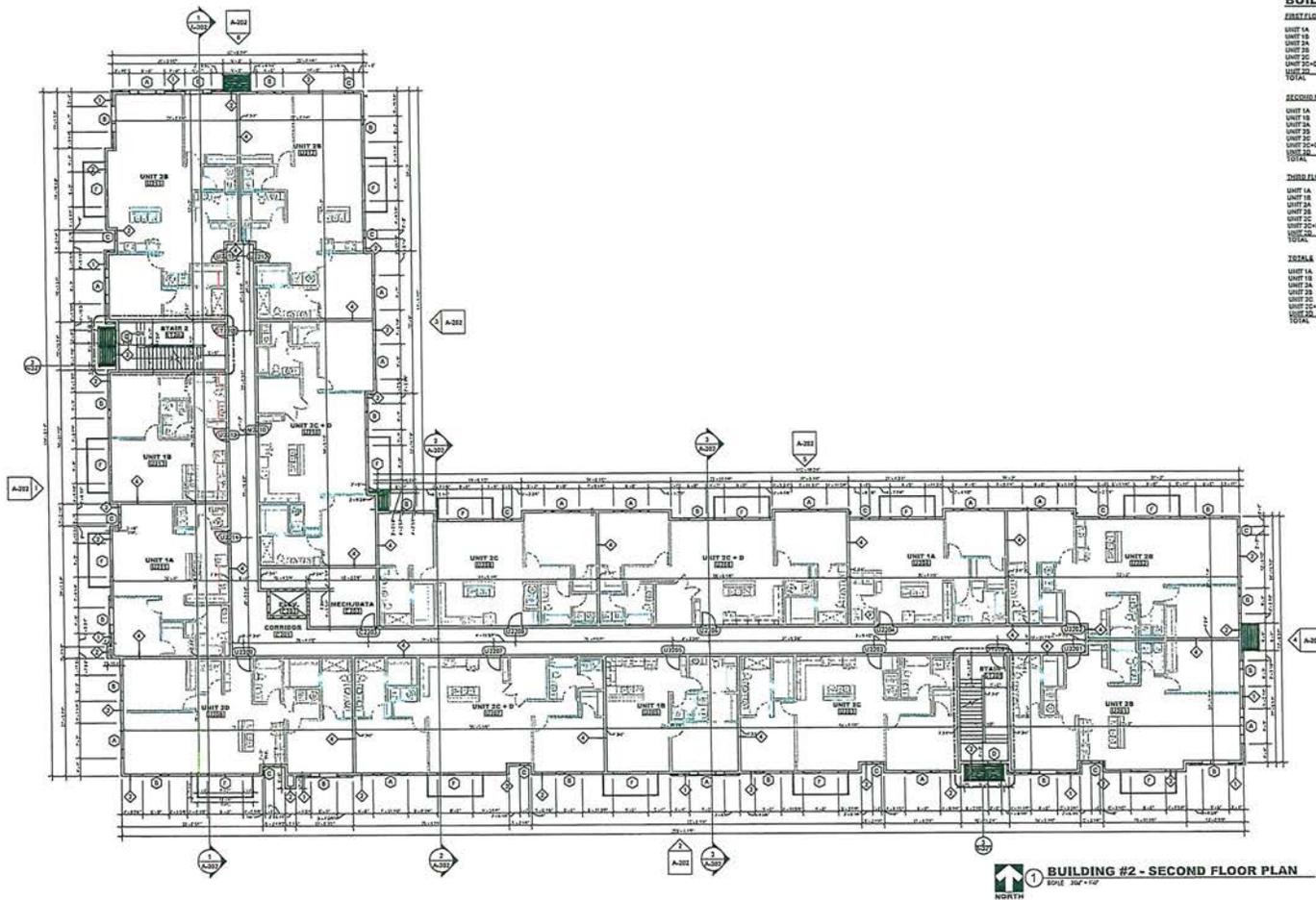
**HUBBELL REALTY COMPANY**

**TELUS CONDOS**  
DES MOINES, IA

DATE	DESCRIPTION
11/11/2024	ISSUED FOR PERMIT
11/11/2024	ISSUED FOR CONSTRUCTION
11/11/2024	ISSUED FOR OCCUPANCY

2024 11/11  
21163  
04/20/2024  
DES MOINES, IA  
BUILDING #1  
ROOF PLAN  
A-104





**1 BUILDING #2 - SECOND FLOOR PLAN**  
SCALE: 3/8" = 1'-0"

**GENERAL FLOOR PLAN NOTES**

1. REFER TO ADDITIONAL NOTES AND REQUIREMENTS ON ALL OTHER DOCUMENTS AND OTHER DISCIPLINE DOCUMENTS.
2. ALL SITE AND FURNITURE, FIXTURES AND EQUIPMENT SHOWN SHOULD BE CONSIDERED CONCEPTUAL AND MAY HAVE BE PROVIDED FOR COORDINATION PURPOSES ONLY. REFER TO CIVIL, FIRE SUPPRESSION, PLUMBING, MECHANICAL, ELECTRICAL, COMMUNICATIONS, ELECTRONIC SAFETY & SECURITY DOCUMENTS AND CONSULTANTS FOR SPECIFIC DESIGN INFORMATION & REQUIREMENTS.
3. DIMENSIONS ARE TO FACE OF FINISH MATERIAL OR CENTER OF STRUCTURE UNLESS OTHERWISE SPECIFIED.
4. ALL CONCRETE, PIPES, WIRING, ROUGH-IN, ETC. SHOULD BE HELD AS TIGHT AS POSSIBLE TO THE HOLES OR FLOOR DECK OR SHOULD BE KEPT WITHIN INTERIOR WALL CAVITIES. NOTHING SHALL BE BELOW THE BOTTOM EDGE OF MAIN BUILDING FRAMES. NOTHING SHALL BE EXPOSED IN FINISHED SPACES. NOTHING SHALL PREVENT A SMOOTH CONTINUOUS FINISH WALL OR CEILING AS NOTED ON THE DRAWINGS AND TO PLUMBING PLAN IN EXTERIOR WALLS UNLESS PRIOR APPROVAL IS GRANTED BY THE ARCHITECT OR OWNER. PLACEMENT COORDINATION BETWEEN TRADES IS REQUIRED.
5. INSTALL VERTICAL EXPANSION JOINT ANIMAL AT COLUMN/GRID LINES AND AT THE HEAD OF SOON INDEPENDENT OR AS OTHERWISE NOTED OR WHERE REQUIRED PER STANDARD MATERIAL PRACTICE TO REDUCE STRESS CRACKING. NO CONTROL JOINT/EXPANSION JOINT IN LOAD BEARING AREAS. KEEP AWAY 24" MIN.
6. CAULK & SEAL ALL CONTROL-EXPANSION, SAWCUT JOINTS AT ALL EXTERIOR & EXTERIOR MASONRY & CONCRETE.
7. PROVIDE LOWER LEVEL AND/OR UNDER FLOOR DRAINAGE SYSTEM TO REDUCE HYDROSTATIC UPLIFT AND PROTECT AGAINST MOISTURE SEEPAGE. PROVIDE FOUNDATION WALL DRAINAGE SYSTEM TO REDUCE LATERAL PRESSURES AND PROTECT AGAINST MOISTURE SEEPAGE. BOTH SYSTEMS SHALL OUTLET TO APPROPRIATE RELIEF.
8. PROVIDE POSITIVE DRAINAGE OF SURFACE WATERS AWAY FROM BUILDING WITHOUT FORMING OF WATER AGICENT TO BUILDING OR ON PAVEMENTS.

**BUILDING #2 UNIT MIX:**

FIRST FLOOR	
UNIT 1A	1
UNIT 2A	0
UNIT 2B	0
UNIT 2C	1
UNIT 2C-B	1
UNIT 2D	1
TOTAL	5
(21 REO. GARAGES, 2 ADA GARAGES)	

SECOND FLOOR	
UNIT 1A	2
UNIT 1B	2
UNIT 2A	2
UNIT 2B	4
UNIT 2C	1
UNIT 2C-B	1
UNIT 2D	1
TOTAL	14

THIRD FLOOR	
UNIT 1A	2
UNIT 1B	2
UNIT 2A	4
UNIT 2B	4
UNIT 2C	3
UNIT 2C-B	3
UNIT 2D	1
TOTAL	14

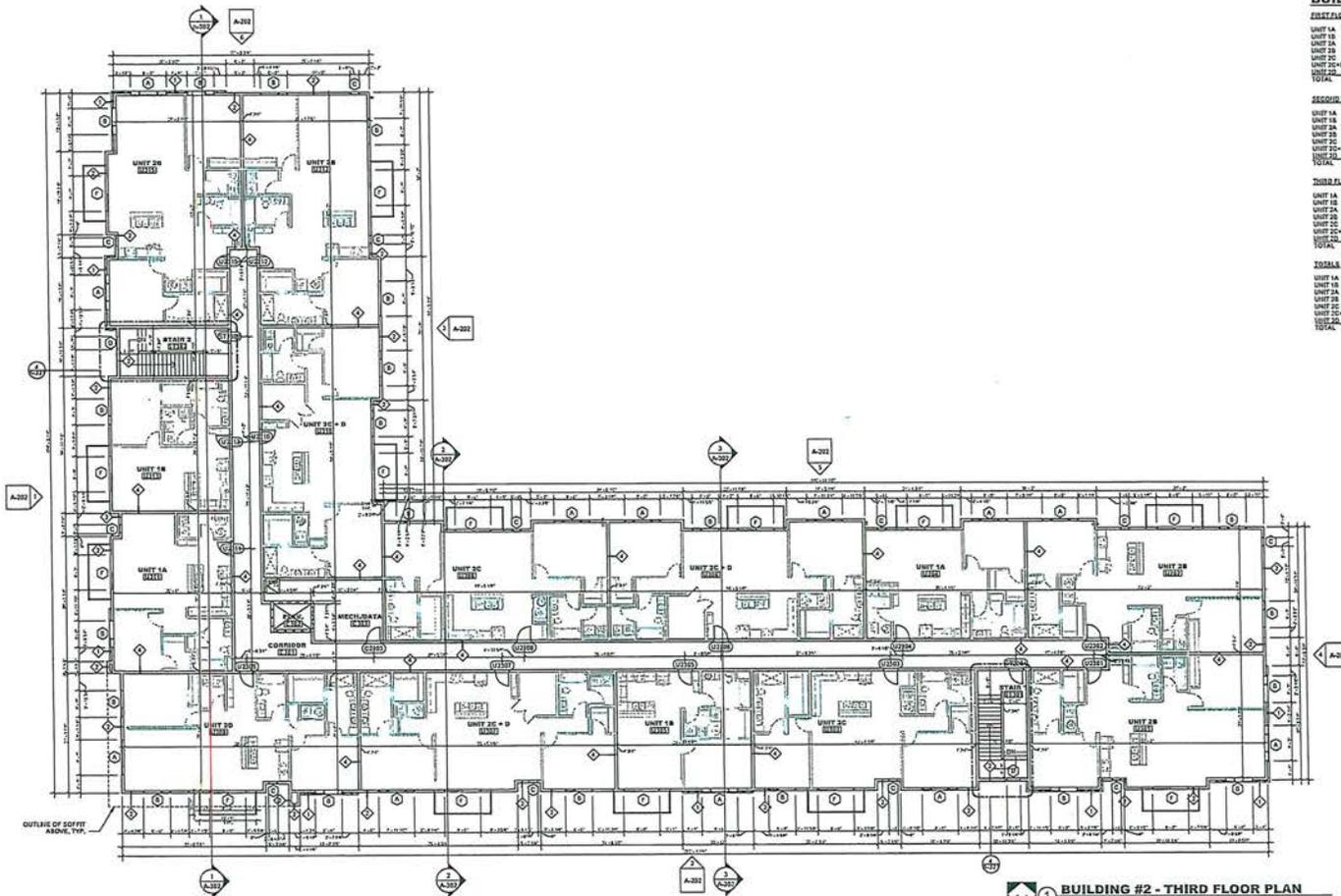
TOTAL	
UNIT 1A	5
UNIT 1B	4
UNIT 2A	6
UNIT 2B	8
UNIT 2C	5
UNIT 2C-B	5
UNIT 2D	4
TOTAL	37
(11 3-BEDROOMS - 20%)	
(21 GARAGES)	



**TELUS CONDOS**  
DES MOINES, IA

DATE	10/20/2017
BY	10/20/2017
REV	10/20/2017
DESCRIPTION	
DATE	10/20/2017
BY	10/20/2017
REV	10/20/2017
DESCRIPTION	

DATE: 10/20/2017  
FIG. No.: 21102  
SHEET No.: 02  
PROJECT: BUILDING #2 - SECOND FLOOR PLAN  
SHEET No.: A-112



**1 BUILDING #2 - THIRD FLOOR PLAN**  
 NORTH  
 SIZE: 387' x 147'

**GENERAL FLOOR PLAN NOTES**

1. REFER TO ADDITIONAL NOTES AND REQUIREMENTS ON ALL OTHER DOCUMENTS AND OTHER DISCIPLINE DOCUMENTS.
2. ALL SITE BFD, FURNITURE, FIXTURES AND EQUIPMENT SHOWN SHOULD BE CONSIDERED CONCEPTUAL AND MATTERIES BE PROVIDED FOR COORDINATION PURPOSES ONLY. REFER TO CIVIL, FIRE SUPPRESSION, PLUMBING, MECHANICAL, ELECTRICAL, COMMUNICATIONS, ELECTRONIC SAFETY & SECURITY DOCUMENTS FOR CONSTRUCTION INFORMATION FOR SPECIFIC DESIGN INFORMATION & REQUIREMENTS.
3. DIMENSIONS ARE TO FACE OF FINISH MATERIAL OR CENTER OF STRUCTURE UNLESS OTHERWISE SPECIFIED.
4. ALL CONDUITS, PIPES, WIRING, ROUGH-IN, ETC. SHOULD BE HELD AS TIGHT AS POSSIBLE TO THE ROOF OR FLOOR JOCK OR SHOULD BE LEFT WITHIN INTERFLOOR WALL CAVITIES. NOTHING SHALL BE BELOW THE BOTTOM EDGE OF MAIN BUILDING FRAMES. NOTHING SHALL BE EXPOSED BY FINISHED SPACES. NOTHING SHALL PRESENT A SMOOTH CONTINUOUS FINISHED WALL OR CEILING AS NOTED ON THE DRAWINGS AND NO PULLBOXES ARE IN EXTERIOR WALLS UNLESS PRIOR APPROVAL BETWEEN TRADES IS REQUIRED.
5. INSTALL VERTICAL EXPANSION JOINT MINIMAL AT COLUMN/ROOF LINES AND AT THE HEAD OF BOON JAMB/SWENSENS OR AS OTHERWISE NOTED OR UNLESS REQUIRED FOR STANDARD MATERIAL PRACTICE TO REDUCE STRESS CRACKING. NO CONTROL, CONTRAEXPANSION JOINT IN LONG BEARING AREAS. START AWAY 2" MIN.
6. CAULK & SEAL ALL CONTROL-EXPANSION, SINKOUT JOINTS AT ALL EXTERIOR & EXTERIOR BALCONY & CONCRETE.
7. PROVIDE LOWER LEVEL AND/OR UNDERFLOOR DRAINAGE SYSTEM TO REDUCE HUMIDITY UPWIND AND PREVENT AGAINST MOISTURE SEEPAGE. PROVIDE FOUNDATION WALL DRAINAGE SYSTEM TO REDUCE LATERAL PRESSURES AND PROTECT AGAINST MOISTURE SEEPAGE. BOTH SYSTEMS SHALL OUTLET TO APPROPRIATE RELIEF.
8. PROVIDE POSITIVE DRAINAGE OF SURFACE WATER AWAY FROM BUILDING WITHOUT PONDING OF WATER ADJACENT TO BUILDING OR ON PAVEMENTS.

**BUILDING #2 UNIT MIX:**

**FIRST FLOOR**

UNIT 1A	1
UNIT 1B	1
UNIT 1C	1
UNIT 1D	1
UNIT 1E	1
UNIT 1F	1
UNIT 1G	1
UNIT 1H	1
UNIT 1I	1
UNIT 1J	1
UNIT 1K	1
UNIT 1L	1
UNIT 1M	1
UNIT 1N	1
UNIT 1O	1
UNIT 1P	1
UNIT 1Q	1
UNIT 1R	1
UNIT 1S	1
UNIT 1T	1
UNIT 1U	1
UNIT 1V	1
UNIT 1W	1
UNIT 1X	1
UNIT 1Y	1
UNIT 1Z	1
TOTAL	21 REG. GARAGES, 2ADA GARAGES

**SECOND FLOOR**

UNIT 2A	2
UNIT 2B	2
UNIT 2C	2
UNIT 2D	2
UNIT 2E	2
UNIT 2F	2
UNIT 2G	2
UNIT 2H	2
UNIT 2I	2
UNIT 2J	2
UNIT 2K	2
UNIT 2L	2
UNIT 2M	2
UNIT 2N	2
UNIT 2O	2
UNIT 2P	2
UNIT 2Q	2
UNIT 2R	2
UNIT 2S	2
UNIT 2T	2
UNIT 2U	2
UNIT 2V	2
UNIT 2W	2
UNIT 2X	2
UNIT 2Y	2
UNIT 2Z	2
TOTAL	42

**THIRD FLOOR**

UNIT 3A	3
UNIT 3B	3
UNIT 3C	3
UNIT 3D	3
UNIT 3E	3
UNIT 3F	3
UNIT 3G	3
UNIT 3H	3
UNIT 3I	3
UNIT 3J	3
UNIT 3K	3
UNIT 3L	3
UNIT 3M	3
UNIT 3N	3
UNIT 3O	3
UNIT 3P	3
UNIT 3Q	3
UNIT 3R	3
UNIT 3S	3
UNIT 3T	3
UNIT 3U	3
UNIT 3V	3
UNIT 3W	3
UNIT 3X	3
UNIT 3Y	3
UNIT 3Z	3
TOTAL	63

**TOTALS**

UNIT 1A	1	11 BEDROOMS + 3IN
UNIT 1B	1	
UNIT 1C	1	
UNIT 1D	1	
UNIT 1E	1	
UNIT 1F	1	
UNIT 1G	1	
UNIT 1H	1	
UNIT 1I	1	
UNIT 1J	1	
UNIT 1K	1	
UNIT 1L	1	
UNIT 1M	1	
UNIT 1N	1	
UNIT 1O	1	
UNIT 1P	1	
UNIT 1Q	1	
UNIT 1R	1	
UNIT 1S	1	
UNIT 1T	1	
UNIT 1U	1	
UNIT 1V	1	
UNIT 1W	1	
UNIT 1X	1	
UNIT 1Y	1	
UNIT 1Z	1	
TOTAL	21	(21 GARAGES)

**SIMONSON**  
 ARCHITECTS  
 1000 UNIVERSITY AVENUE, SUITE 1110  
 DES MOINES, IA 50319  
 PH: 515.281.1111  
 FAX: 515.281.1112  
 WWW.SIMONSONARCHITECTS.COM

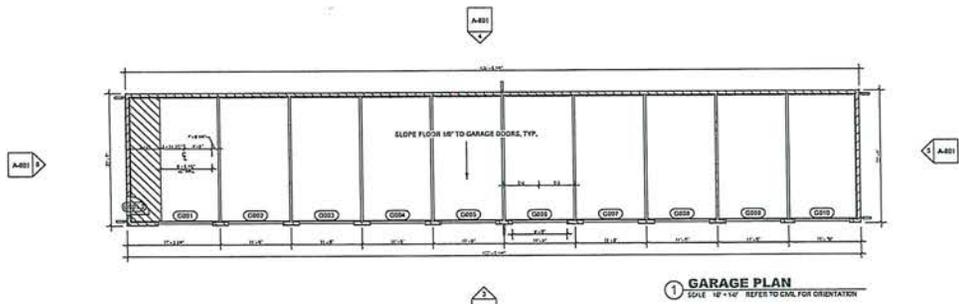
**Hubbell**  
 HUBBELL REALTY COMPANY

**TELUS CONDOS**  
 DES MOINES, IA

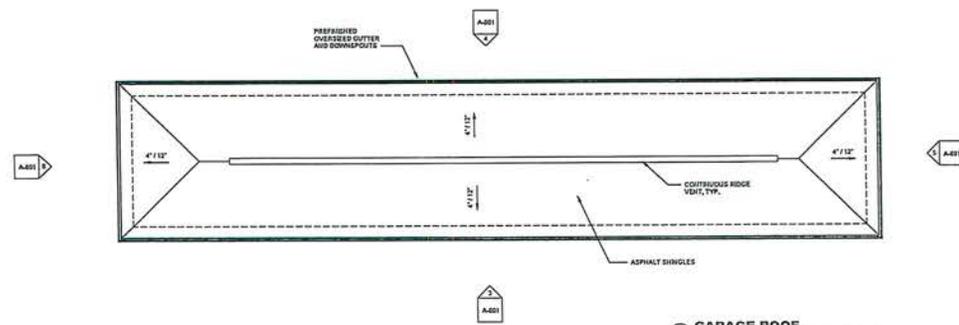
DATE	10/20/2017
BY	10/20/2017
SCALE	AS SHOWN
PROJECT	TELUS CONDOS
DATE	10/20/2017
BY	10/20/2017
SCALE	AS SHOWN
PROJECT	TELUS CONDOS

JOB NO. 21162  
 SHEET NO. 8P  
 PROJECT: BUILDING #2 - THIRD FLOOR PLAN  
 DATE: 10/20/2017  
**A-113**





**1 GARAGE PLAN**  
SCALE 1/2" = 1'-0" REFER TO CML FOR ORIENTATION



**2 GARAGE ROOF**  
SCALE 1/8" = 1'-0" REFER TO CML FOR ORIENTATION

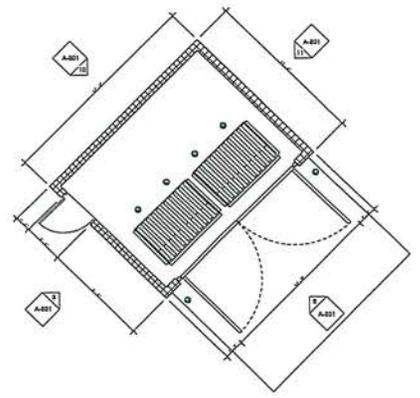
EXTERIOR MATERIAL LEGEND - GARAGE	
MAT. LABEL	MATERIAL DESCRIPTION
MAL	PREFABRICATED BALCONY CLEAR ANODIZED ALUMINUM
MBA	PAV. COFFIN POLISHED METAL, NATURAL, METAL PAINT COLOR - GRAY
CMN	PREFABRICATED CANTOPY, MAPLE OR EQUAL, COLOR, CLEAR ANODIZED
CMW	PAV. COFFIN POLISHED METAL, NATURAL, METAL PAINT COLOR - GRAY
CMG	PAV. COFFIN POLISHED METAL, NATURAL, METAL PAINT COLOR - GRAY
CMH	PAV. COFFIN POLISHED METAL, NATURAL, METAL PAINT COLOR - GRAY
CMJ	PAV. COFFIN POLISHED METAL, NATURAL, METAL PAINT COLOR - GRAY
CMK	PAV. COFFIN POLISHED METAL, NATURAL, METAL PAINT COLOR - GRAY
CMN	PAV. COFFIN POLISHED METAL, NATURAL, METAL PAINT COLOR - GRAY
CMO	PAV. COFFIN POLISHED METAL, NATURAL, METAL PAINT COLOR - GRAY
CMQ	PAV. COFFIN POLISHED METAL, NATURAL, METAL PAINT COLOR - GRAY
CMR	PAV. COFFIN POLISHED METAL, NATURAL, METAL PAINT COLOR - GRAY
CMU	PAV. COFFIN POLISHED METAL, NATURAL, METAL PAINT COLOR - GRAY
CMV	PAV. COFFIN POLISHED METAL, NATURAL, METAL PAINT COLOR - GRAY
CMW	PAV. COFFIN POLISHED METAL, NATURAL, METAL PAINT COLOR - GRAY
CMX	PAV. COFFIN POLISHED METAL, NATURAL, METAL PAINT COLOR - GRAY
CMY	PAV. COFFIN POLISHED METAL, NATURAL, METAL PAINT COLOR - GRAY
CMZ	PAV. COFFIN POLISHED METAL, NATURAL, METAL PAINT COLOR - GRAY
CM1	PAV. COFFIN POLISHED METAL, NATURAL, METAL PAINT COLOR - GRAY
CM2	PAV. COFFIN POLISHED METAL, NATURAL, METAL PAINT COLOR - GRAY
CM3	PAV. COFFIN POLISHED METAL, NATURAL, METAL PAINT COLOR - GRAY
CM4	PAV. COFFIN POLISHED METAL, NATURAL, METAL PAINT COLOR - GRAY
CM5	PAV. COFFIN POLISHED METAL, NATURAL, METAL PAINT COLOR - GRAY
CM6	PAV. COFFIN POLISHED METAL, NATURAL, METAL PAINT COLOR - GRAY
CM7	PAV. COFFIN POLISHED METAL, NATURAL, METAL PAINT COLOR - GRAY
CM8	PAV. COFFIN POLISHED METAL, NATURAL, METAL PAINT COLOR - GRAY
CM9	PAV. COFFIN POLISHED METAL, NATURAL, METAL PAINT COLOR - GRAY
CM0	PAV. COFFIN POLISHED METAL, NATURAL, METAL PAINT COLOR - GRAY

**MISC EXTERIOR MATERIAL FINISHES**

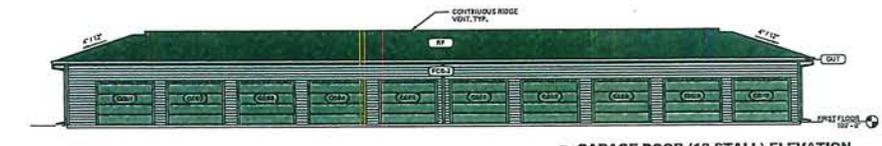
- EXTERIOR MATERIAL FINISHES:
- EXPOSED CONCRETE: GRIND SMOOTH & BURNCOAT WITH CEMENT SLURRY.
  - EXPOSED LATH: GALVANIZED & PAINTED TO MATCH ADJACENT MATERIAL ABOVE.
  - METAL BALCONIES & RAILINGS: GALVANIZED & PAINTED WHITE.
  - FASCIA & SOFFIT: METAL PREFINISHED TO MATCH ADJACENT ROOF COLOR.
  - EQUIPMENT, OUTLETS & CONDUITS: METAL PREFINISHED TO MATCH ADJ. MATERIAL.
  - CAULK & SEALANTS: COLOR MATCHED TO ADJACENT MATERIAL.
  - HOLLOW METAL MAIN DOORS & FRAMES: PAINTED TO MATCH ADJACENT MATERIAL.
  - OVERHEAD PANEL DOORS: METAL PREFINISHED TO MATCH ADJACENT MATERIAL.
  - CONCRETE: METAL PREFINISHED TO MATCH GRAY.
  - TRIM: METAL PREFINISHED TO MATCH ADJ. OPENING FRAMING.
  - METAL PREFINISHED TO MATCH ADJ. OPENING FRAMING.
  - EXTERIOR UTILITY EQUIPMENT: IF ALLOWED, PAINTED TO MATCH ADJACENT MATERIAL.

**TYPICAL U.S.A.I.**

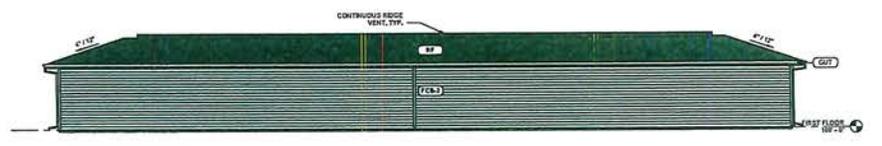
- TRIM SIZES, TYP. U.S.A.I.:
- WINDOWS & DOORS 3'-0"
- VERTICAL 3'-0"
- HORIZONTAL 3'-0"



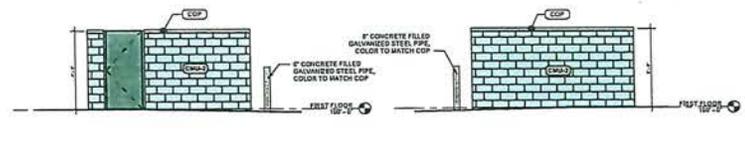
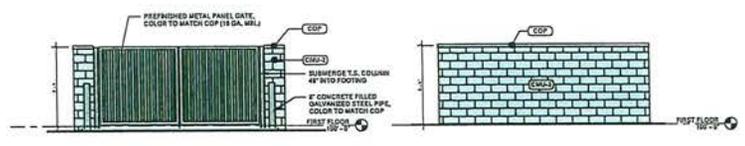
**3 TRASH ENCLOSURE PLAN**  
SCALE 1/8" = 1'-0"



**3 GARAGE DOOR (10 STALL) ELEVATION**  
SCALE 1/8" = 1'-0"



**4 GARAGE (10 STALL) ELEVATION**  
SCALE 1/8" = 1'-0"



**5 GARAGE SIDE 1 (10 STALL) ELEVATION**  
SCALE 1/8" = 1'-0"



**6 GARAGE SIDE 2 (10 STALL) ELEVATION**  
SCALE 1/8" = 1'-0"



**TELUS CONDOS**  
DES MOINES, IA

DATE	211022
BY	EP
PROJECT	GARAGE & TRASH ENCLOSURE
SCALE	AS SHOWN

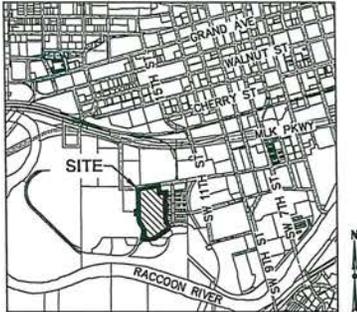
211022  
EP  
GARAGE & TRASH ENCLOSURE  
A-801

PRELIMINARY PLAT / DEVELOPMENT PLAN FOR:

# GRAY'S STATION PLAT 5

DES MOINES, IOWA

**VICINITY MAP**  
NOT TO SCALE



DES MOINES, IOWA

**OWNER**

HRC NPS I LLC  
6900 WESTOWN PKWY  
WEST DES MOINES, IA 50268

**APPLICANT**

HUBBELL REALTY COMPANY  
6900 WESTOWN PKWY  
WEST DES MOINES, IA 50268  
CONTACT: CALEB SMITH  
PH. (515) 243-3228

**ENGINEER**

CIVIL DESIGN ADVANTAGE  
4121 NW URBANDALE DRIVE  
URBANDALE, IA 50322  
CONTACT: RYAN HARGUSTY  
EMAIL: RYAN@CDA-ENG.COM  
PH. (515) 389-4400

**SURVEYOR**

CIVIL DESIGN ADVANTAGE, LLC  
4121 NW URBANDALE DRIVE  
URBANDALE, IA 50322  
CONTACT: CHARLIE MCLOTHLEN  
EMAIL: CHARLIE@CDA-ENG.COM  
PH. (515) 389-4400

**DATE OF SURVEY**

08/05/2022

**BENCHMARKS**

- CITY BENCHMARK #725: BRASS CAP IN THE NORTHEAST TRAFFIC SIGNAL BASE AT THE SOUTHWEST CORNER OF MARTIN LUTHER KING DRIVE AND SOUTHWEST 11TH STREET. ELEVATION=29.81
- CITY BENCHMARK #5581: BRASS TABLET IN THE CONCRETE WALL 43.6 FEET WEST OF THE EAST FACE NEAR THE SANITARY SEWER PUMP STATION. ELEVATION=25.48

**SUBMITTAL DATES**

FIRST SUBMITTAL: 10/19/2022  
SECOND SUBMITTAL: 01/04/2023

**LEGAL DESCRIPTION**

A PART OF LOTS 6, 7, AND 9 CENTRAL DES MOINES INDUSTRIAL PARK AND A PART OF PARCEL 2019-1467 OF SAID LOTS 6, 7, AND 9, AS SHOWN ON THE PLAT OF SURVEY RECORDED IN BOOK 17073, PAGE 735, BOTH OFFICIAL PLATS IN THE CITY OF DES MOINES, POLK COUNTY, IOWA AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF LOT 12, GRAY'S STATION PLAT 2, AN OFFICIAL PLAT; THENCE SOUTH 08°20'42" EAST ALONG THE WEST LINE OF SAID LOT 12, A DISTANCE OF 145.50 FEET TO THE SOUTHWEST CORNER OF SAID LOT 12; THENCE NORTH 81°39'18" EAST ALONG THE SOUTHERLY LINE OF SAID LOT 12, A DISTANCE OF 128.11 FEET TO THE NORTHWEST CORNER OF OUTLOT 'Y', SAID GRAY'S STATION PLAT 2; THENCE SOUTH 00°01'40" EAST ALONG THE WESTERLY LINE OF SAID OUTLOT 'Y' AND THE WESTERLY LINE OF GRAY'S STATION PLAT 3, AN OFFICIAL PLAT, 68.73 FEET; THENCE SOUTH 89°58'20" WEST CONTINUING ALONG SAID WESTERLY LINE OF SAID GRAY'S STATION PLAT 3, A DISTANCE OF 12.77 FEET; THENCE SOUTH 00°01'40" EAST CONTINUING ALONG SAID WESTERLY LINE, 107.81 FEET; THENCE SOUTH 89°58'20" WEST CONTINUING ALONG SAID WESTERLY LINE, 6.23 FEET; THENCE SOUTH 00°01'40" EAST CONTINUING ALONG SAID WESTERLY LINE AND THE WESTERLY LINE OF GRAY'S STATION PLAT 4, AN OFFICIAL PLAT, 122.09 FEET; THENCE NORTH 89°58'20" EAST CONTINUING ALONG SAID WESTERLY LINE OF SAID GRAY'S STATION PLAT 4, A DISTANCE OF 17.61 FEET; THENCE SOUTH 00°01'40" EAST CONTINUING ALONG SAID WESTERLY LINE, 63.00 FEET; THENCE SOUTH 89°58'20" WEST CONTINUING ALONG SAID WESTERLY LINE, 8.11 FEET; THENCE SOUTH 00°01'40" EAST CONTINUING ALONG SAID WESTERLY LINE, 148.23 FEET; THENCE SOUTHERLY CONTINUING ALONG SAID WESTERLY LINE AND A CURVE CONCAVE WESTERLY WHOSE RADIUS IS 80.00 FEET, WHOSE ARC LENGTH IS 56.30 FEET AND WHOSE CHORD BEARS SOUTH 20°09'18" WEST, 56.20 FEET; THENCE SOUTH 49°39'41" EAST CONTINUING ALONG SAID WESTERLY LINE, 52.02 FEET; THENCE SOUTH 29°22'59" EAST CONTINUING ALONG SAID WESTERLY LINE, 105.81 FEET TO THE SOUTHWESTERLY CORNER OF STREET LOT 'A', SAID GRAY'S STATION PLAT 4; THENCE SOUTHWESTERLY ALONG A CURVE CONCAVE SOUTHEASTERLY WHOSE RADIUS IS 288.00 FEET, WHOSE ARC LENGTH IS 32.35 FEET AND WHOSE CHORD BEARS SOUTH 57°39'24" WEST, 32.34 FEET; THENCE SOUTH 54°23'40" WEST, 249.63 FEET; THENCE WESTERLY ALONG A CURVE CONCAVE NORTHERLY WHOSE RADIUS IS 368.00 FEET, WHOSE ARC LENGTH IS 302.72 FEET AND WHOSE CHORD BEARS SOUTH 77°57'44" WEST, 234.28 FEET; THENCE NORTH 79°28'18" WEST, 26.20 FEET; THENCE NORTH 11°31'42" EAST, 70.00 FEET; THENCE SOUTH 79°28'18" EAST, 22.83 FEET; THENCE NORTH 00°01'11" WEST, 61.22 FEET; THENCE NORTHERLY ALONG A CURVE CONCAVE WESTERLY WHOSE RADIUS IS 224.00 FEET, WHOSE ARC LENGTH IS 131.34 FEET AND WHOSE CHORD BEARS NORTH 16°56'00" WEST, 129.46 FEET; THENCE NORTH 33°44'49" WEST, 43.38 FEET; THENCE NORTHERLY ALONG A CURVE CONCAVE EASTERLY WHOSE RADIUS IS 278.00 FEET, WHOSE ARC LENGTH IS 161.83 FEET AND WHOSE CHORD BEARS NORTH 18°50'00" WEST, 159.52 FEET; THENCE NORTH 00°00'11" WEST, 58.59 FEET; THENCE NORTH 89°19'18" WEST, 43.82 FEET; THENCE NORTH 00°40'42" EAST, 52.00 FEET TO THE SOUTH LINE OF LOT 8, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE SOUTH 89°19'18" EAST ALONG SAID SOUTH LINE, 43.18 FEET TO THE SOUTHEAST CORNER OF SAID LOT 8; THENCE NORTH 00°01'11" WEST ALONG THE EAST LINE OF SAID LOT 8, A DISTANCE OF 333.17 FEET; THENCE NORTHERLY CONTINUING ALONG SAID EAST LINE AND A CURVE CONCAVE WESTERLY WHOSE RADIUS IS 804.42 FEET, WHOSE ARC LENGTH IS 84.04 FEET AND WHOSE CHORD BEARS NORTH 03°28'39" WEST, 83.89 FEET; THENCE NORTH 08°58'08" WEST CONTINUING ALONG SAID EAST LINE, 157.45 FEET TO THE NORTHEAST CORNER OF SAID LOT 8, ALSO BEING A POINT ON THE SOUTHERLY LINE OF TITTLE STREET RIGHT OF WAY; THENCE NORTH 81°10'49" EAST ALONG SAID SOUTHERLY RIGHT OF WAY LINE, 411.78 FEET TO THE POINT OF BEGINNING AND CONTAINING 11.47 ACRES (499,715 SQUARE FEET).

THE PROPERTY IS SUBJECT TO ANY AND ALL EASMENTS OF RECORD.

**ZONING**

GRAY'S STATION PUD - ZON2017-00087

**EXISTING/PROPOSED USE**

EXISTING USE: UNDEVELOPED  
PROPOSED USE: RESIDENTIAL

**TAX ABATEMENT - SUSTAINABILITY**

- BIKE RACKS FOR PUBLIC USE THAT PROVIDE A MINIMUM OF 10% OF THE AUTOMOBILE PARKING PROVIDED.
- 50% INCREASE IN REQUIRED LANDSCAPING.
- PRIMARY ENTRY WITHIN A MILE OF A DART TRANSIT STOP.
- REDEVELOPMENT OF A PREVIOUSLY DEVELOPED SITE.

**INDEX OF SHEETS**

NO.	DESCRIPTION
CO.1	COVER SHEET
C1.1	TOPOGRAPHIC SURVEY / DEMOLITION PLAN
C2.0-2.4	DIMENSION PLAN
C3.0-3.5	GRADING PLAN
C4.0-4.4	UTILITY PLAN
C5.1-5.2	DETAILS
LO2.01-LO2.05	RDG LANDSCAPE PLAN (SITE LAYOUT PLAN)
LO4.01-LO4.05	RDG LANDSCAPE PLAN (SITE PLANTING PLAN)
LO5.01	RDG LANDSCAPE PLAN (DETAILS)

**DEVELOPMENT SUMMARY**

TOTAL SITE AREA:	11.47 ACRES (499,715 SF)
EXISTING IMPERVIOUS AREA:	0.00 ACRES (0 SF)
OPEN SPACE CALCULATION:	
TOTAL SITE:	- 499,715 SF (11.47 AC.)
BUILDINGS:	- 84,115 SF
STREETS:	- 85,884 SF
DRIVEWAYS:	- 12,745 SF
SIDEWALKS:	- 24,388 SF
OPEN SPACE PROVIDED:	- 282,622 SF (58.0%)

UNITS:  
69 TOWNHOME UNITS  
64 CONDOMINIUM UNITS (LOT 67)  
149 TOTAL UNITS (12.89 UNITS PER ACRE)

**NOTES**

- ALL PUBLIC IMPROVEMENTS, INCLUDING SPOT ELEVATIONS, ARE SHOWN FOR REFERENCE ONLY. REFER TO THE SEPARATE PRIVATE CONSTRUCTION CONTRACT DOCUMENTS FOR PUBLIC IMPROVEMENT PLANS. IF THERE ARE ANY DISCREPANCIES BETWEEN THE DEVELOPMENT PLAN AND THE PUBLIC IMPROVEMENT PLANS THE DETAILED PUBLIC IMPROVEMENT PLANS SHALL GOVERN.
- THIS PROJECT IS ELIGIBLE FOR TAX ABATEMENT IN ACCORDANCE WITH THE URBAN RENAISSANCE DEVELOPMENT AGREEMENT - SECTION 5.02 (0X16629 PG251-288).

**UTILITY WARNING**

ANY UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY AND RECORDS OBTAINED BY THIS SURVEYOR. THE SURVEYOR MAKES NO GUARANTEE THAT THE UTILITIES SHOWN COMPRISE ALL THE UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UTILITIES SHOWN ARE IN THE EXACT LOCATION SHOWN.

**GENERAL LEGEND**

PROPOSED	EXISTING
PROPERTY BOUNDARY	SANITARY MANHOLE
LOT LINE	WATER VALVE BOX
CENTER LINE	FIRE HYDRANT
RIGHT OF WAY	WATER CURB STOP
BUILDING SETBACK	WELL
PERMANENT EASEMENT	STORM SEWER MANHOLE
TEMPORARY EASEMENT	STORM SEWER SINGLE INTAKE
TYPE SW-501 STORM INTAKE	STORM SEWER DOUBLE INTAKE
TYPE SW-502 STORM INTAKE	FLARED END SECTION
TYPE SW-503 STORM INTAKE	DECIDUOUS TREE
TYPE SW-505 STORM INTAKE	DECIDUOUS TREE
TYPE SW-506 STORM INTAKE	DECIDUOUS SHRUB
TYPE SW-512 STORM INTAKE	CONIFEROUS SHRUB
TYPE SW-513 STORM INTAKE	ELECTRIC POWER POLE
TYPE SW-401 STORM MANHOLE	GUY ANCHOR
TYPE SW-402 STORM MANHOLE	STREET LIGHT
FLARED END SECTION	POWER POLE W/ TRANSFORMER
TYPE SW-301 SANITARY MANHOLE	UTILITY POLE W/ LIGHT
STORM/SANITARY CLEANOUT	ELECTRIC BOX
WATER VALVE	ELECTRIC TRANSFORMER
FIRE HYDRANT ASSEMBLY	ELECTRIC MANHOLE OR VAULT
DETECTABLE WARNING PANEL	TRAFFIC SIGN
WATER CURB STOP	TELEPHONE JUNCTION BOX
SANITARY SEWER	TELEPHONE MANHOLE/VAULT
SANITARY SERVICE	TELEPHONE POLE
STORM SEWER	GAS VALVE BOX
STORM SERVICE	CABLE TV JUNCTION BOX
WATERMAIN WITH SIZE	MAIL BOX
WATER SERVICE	BENCHMARK
SAWTOOTH (FULL DEPTH)	SOIL BORING
SILT FENCE	UNDERGROUND TV CABLE
USE AS CONSTRUCTED	GAS MAIN
MINIMUM PROTECTION ELEVATION	FIBER OPTIC
	UNDERGROUND TELEPHONE
	OVERHEAD ELECTRIC
	UNDERGROUND ELECTRIC
	FIELD TILE
	SANITARY SEWER W/ SIZE
	STORM SEWER W/ SIZE
	WATER MAIN W/ SIZE



CIVIL DESIGN ADVANTAGE  
4121 NW URBANDALE DRIVE, URBANDALE, IA 50322  
PH: (515) 389-4400  
PROJECT NO. 2202.113

THE PROJECT REQUIRES AN IOWA NPDES PERMIT #2 AND CITY OF DES MOINES GRADING PERMIT. CIVIL DESIGN ADVANTAGE WILL PROVIDE THE PERMITS AND THE INITIAL STORM WATER POLLUTION PREVENTION PLAN (SWPPP) FOR THE CONTRACTORS USE DURING CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR UPDATING THE SWPPP THROUGHOUT CONSTRUCTION AND MEETING LOCAL, STATE AND FEDERAL REQUIREMENTS.

ALL CONSTRUCTION MATERIALS, DUMPSTERS, DETACHED TRAILERS OR SIMILAR ITEMS ARE PROHIBITED ON PUBLIC STREETS OR WITHIN THE PUBLIC R.O.W.

THE 2022 EDITION OF THE SDAS STANDARD SPECIFICATIONS, THE PUBLIC RIGHTS-OF-WAY ACCESSIBILITY GUIDELINES (PROVIDED) AND ALL CITY SUPPLEMENTALS, IF APPLICABLE, SHALL APPLY TO ALL WORK ON THIS PROJECT UNLESS OTHERWISE NOTED.

**PRELIMINARY PLAT APPROVAL:**

APPROVED  APPROVED WITH CONDITIONS - SEE EXHIBIT "A"

IN ACCORDANCE WITH CHAPTER 106, DES MOINES MUNICIPAL CODE, AS AMENDED.

DEVELOPMENT SERVICES DIRECTOR \_\_\_\_\_ DATE \_\_\_\_\_

I HEREBY CERTIFY THAT THIS ENGINEERING DOCUMENT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF IOWA.

**PROFESSIONAL ENGINEER**  
Ryan Hargusty  
No. 20811  
IOWA  
2021

**PRELIMINARY**  
FOR CONSTRUCTION

DATE: 10/19/2022  
BY: DES MOINES REVENUE DATE IS DECEMBER 31, 2023  
PAGES OR SHEETS COVERED BY THIS SEAL:  
C SERIES SHEETS

**SITE PLAN APPROVAL:**

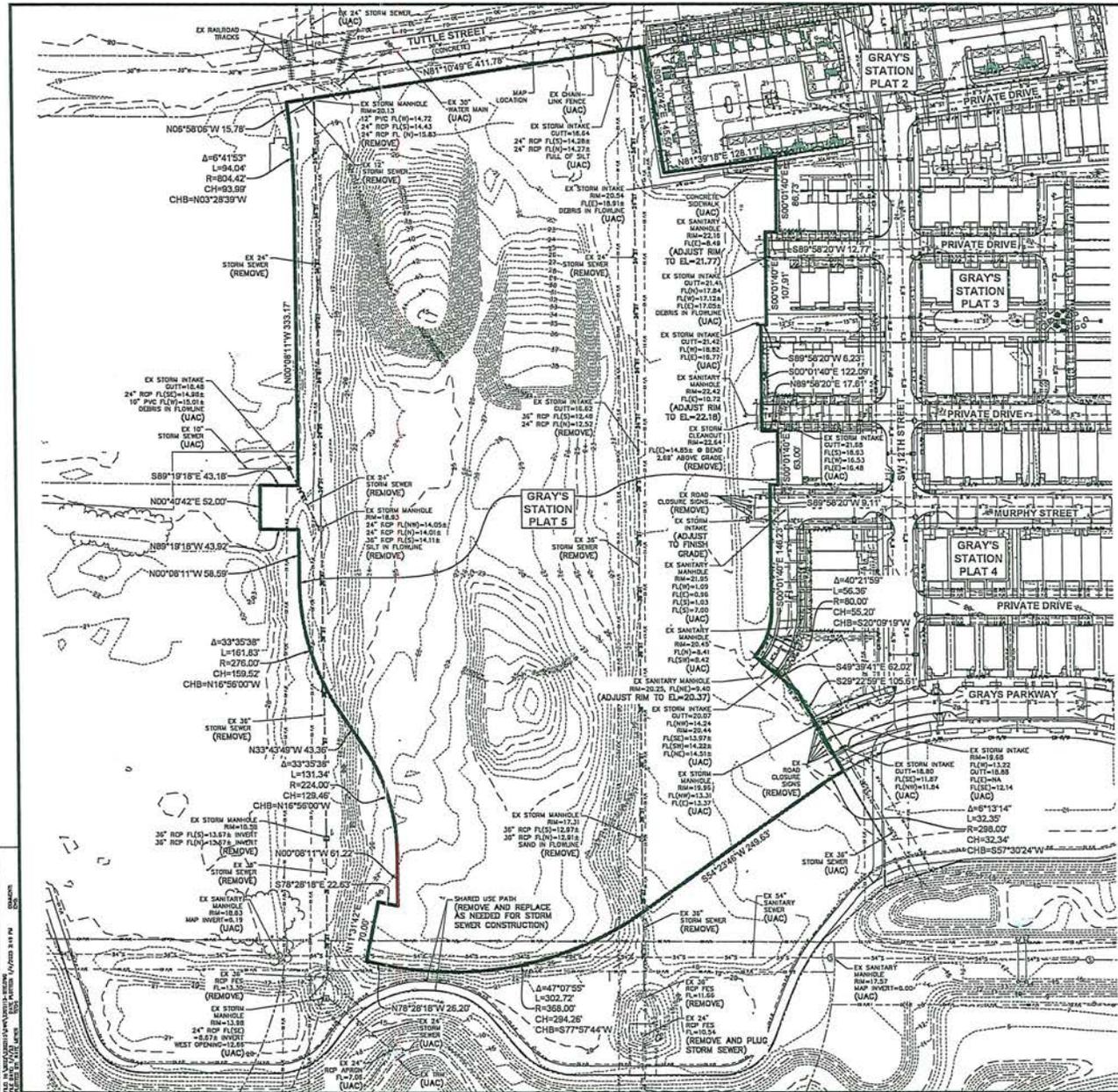
APPROVED  APPROVED WITH CONDITIONS - SEE EXHIBIT "A"

IN ACCORDANCE WITH SECTION 135-9, 2019 DES MOINES MUNICIPAL CODE, AS AMENDED.

NO CHANGES TO THIS PLAN UNLESS APPROVED IN WRITING FROM THE DEVELOPMENT SERVICES DIRECTOR.

DEVELOPMENT SERVICES DIRECTOR \_\_\_\_\_ DATE \_\_\_\_\_

DRAWN BY: CHARLIE MCLOTHLEN, SURVEYOR, LICENSE NO. 10173, IOWA BOARD OF SURVEYORS



**DEMOLITION NOTES**

- PRIOR TO ANY WORK AT THE SITE, CONTRACTOR SHALL EXAMINE ANY APPLICABLE DRAWINGS AVAILABLE FROM THE OWNER OR ENGINEER AND CONSULT WITH OWNER'S PERSONNEL AND UTILITY COMPANY REPRESENTATIVES. NO CONSTRUCTION WILL BE ALLOWED FOR DAMAGE FROM FAILURE TO COMPLY WITH THIS REQUIREMENT.
- PRIOR TO EXCAVATION, CONTRACTOR SHALL NOTIFY IN WRITING (48 HRS NOTICE) THE FOLLOWING:
  - CITY
  - APPROPRIATE UTILITY COMPANIES
  - OWNER
  - CIVIL DESIGN ADVANTAGE ARCHITECT
- DO NOT DISTURB EXISTING UTILITIES UNLESS OTHERWISE NOTED. COORDINATE REMOVAL OR ABANDONMENT OF ALL UTILITIES WITH THE APPROPRIATE UTILITY SUPPLIER AND REGULATORY AGENCIES.
- PROTECT EXISTING UTILITIES THAT ARE TO REMAIN. THE LOCATIONS OF ALL UTILITIES INDICATED ON THE PLANS ARE TAKEN FROM EXISTING RECORDS. THE EXACT LOCATION AND ELEVATION OF ALL UTILITIES MUST BE DETERMINED BY THE CONTRACTOR. IT SHALL BE THE DUTY OF THE CONTRACTOR TO ADDITION WHETHER ANY ADDITIONAL FACILITIES OTHER THAN THOSE SHOWN ON THE PLAN MAY BE PRESENT.
- BACKFILL ALL EXCAVATIONS WITH COMPACTED MATERIAL COMPACTED TO 95% MAXIMUM STANDARD PROCTOR DENSITY AND MOISTURE RATIO OF OPTIMUM MOISTURE TO 4% ABOVE OPTIMUM MOISTURE. TESTING OF BACKFILL TO BE BY A GEOTECHNICAL ENGINEER EMPLOYED BY THE OWNER. IN THE EVENT OF A TEST FAILURE, ANY RETESTING SHALL BE PAID FOR BY THE CONTRACTOR.
- FIELD VERIFY EXISTING GRADES AND LOCATION OF EXISTING UTILITIES, CONDUIT LINES, POLES, TREES, PAVING, BUILDING AND OTHER SITE FEATURES PRIOR TO DEMOLITION AND IMMEDIATELY INFORM THE ENGINEER AND/OR OWNER OF ANY DISCREPANCIES.
- DEMOLITION NOTES AS SHOWN ON THE PLAN ARE NOT ALL INCLUSIVE. CONTRACTOR TO ABANDON IN PLACE OR REMOVE AND DISPOSE OF ALL EXISTING SITE IMPROVEMENTS ABOVE AND BELOW GROUND TO COMPLY WITH THE GENERAL INTENT OF THIS DOCUMENT.
- ALL CONSTRUCTION/DEMOLITION DEBRIS SHALL BE DISPOSED OF OFF-SITE IN FULL COMPLIANCE WITH CURRENT ENVIRONMENTAL REGULATIONS.
- CONSTRUCTION LIMITS SHALL BE CONFINED TO THE SITE BOUNDARY AS NOTED. ANY DAMAGE TO PROPERTIES OUTSIDE THE SITE BOUNDARY SHALL BE AT THE SOLE RESPONSIBILITY OF THE CONTRACTOR.
- ALL WORK SHALL BE IN ACCORDANCE WITH IOWA STANDARDS. NOTINGS INDICATED ON THE DRAWINGS SHALL RELIEVE THE CONTRACTOR FROM COMPLYING WITH ANY APPROPRIATE SAFETY REGULATIONS.

**TRAFFIC CONTROL NOTES**

- ALL APPLICABLE CITY PERMITS, INCLUDING BUT NOT LIMITED TO CLOSURE PERMITS, SHALL BE OBTAINED PRIOR TO ANY CONSTRUCTION WITHIN CITY BLOCK OR LAKE CLOSURES.
- ALL TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
- PERMANENT SIGNING THAT CONVEYS A MESSAGE CONTRARY TO THE MESSAGE OF TEMPORARY SIGNING AND NOT APPLICABLE TO THE WORKING CONDITIONS SHALL BE COVERED BY THE CONTRACTOR WHEN DIRECTED BY THE CITY.
- THE CONTRACTOR SHALL COORDINATE HIS TRAFFIC CONTROL WITH OTHER CONSTRUCTION PROJECTS IN THE AREA.
- STANDARD CLOSURE SIGNS REQUIRED FOR ALL SIDEWALK CLOSURES, SHOULDER AND TEMPORARY PEDESTRIAN ACCESS ROUTE THROUGH THE CONSTRUCTION SHALL MEET THE REQUIREMENTS OF PUBLIC RIGHT-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG).
- THE CONTRACTOR IS CAUTIONED NEITHER TO DESTROY NOR REMOVE ANY EXISTING PAVEMENT, NOR TO DISTURB THE EXISTING TRAFFIC PATTERNS MORE THAN IS NECESSARY FOR THE PROPER EXECUTION OF THE WORK.
- ALL SIGNING AND LAKE STRIPING WILL NEED TO COMPLY WITH MUTCD. MAINTENANCE AND REPLACEMENT OF THE EXISTING STRIPING WILL BE THE RESPONSIBILITY OF THE APPLICANT.

NOTES: ALL PUBLIC IMPROVEMENTS, INCLUDING SPOT ELEVATIONS, ARE SHOWN FOR REFERENCE ONLY. REFER TO THE SEPARATE PRIVATE CONSTRUCTION CONTRACT DOCUMENTS FOR PUBLIC IMPROVEMENT PLANS. IF THERE ARE ANY DISCREPANCIES BETWEEN THE ELEVATION PLAN AND THE PUBLIC IMPROVEMENT PLANS THE DETAILED PUBLIC IMPROVEMENT PLANS SHALL GOVERN.

DATE	DESIGNED	CHECKED
REVISIONS	FEATURE SUBMITTAL	FIRST SUBMITTAL

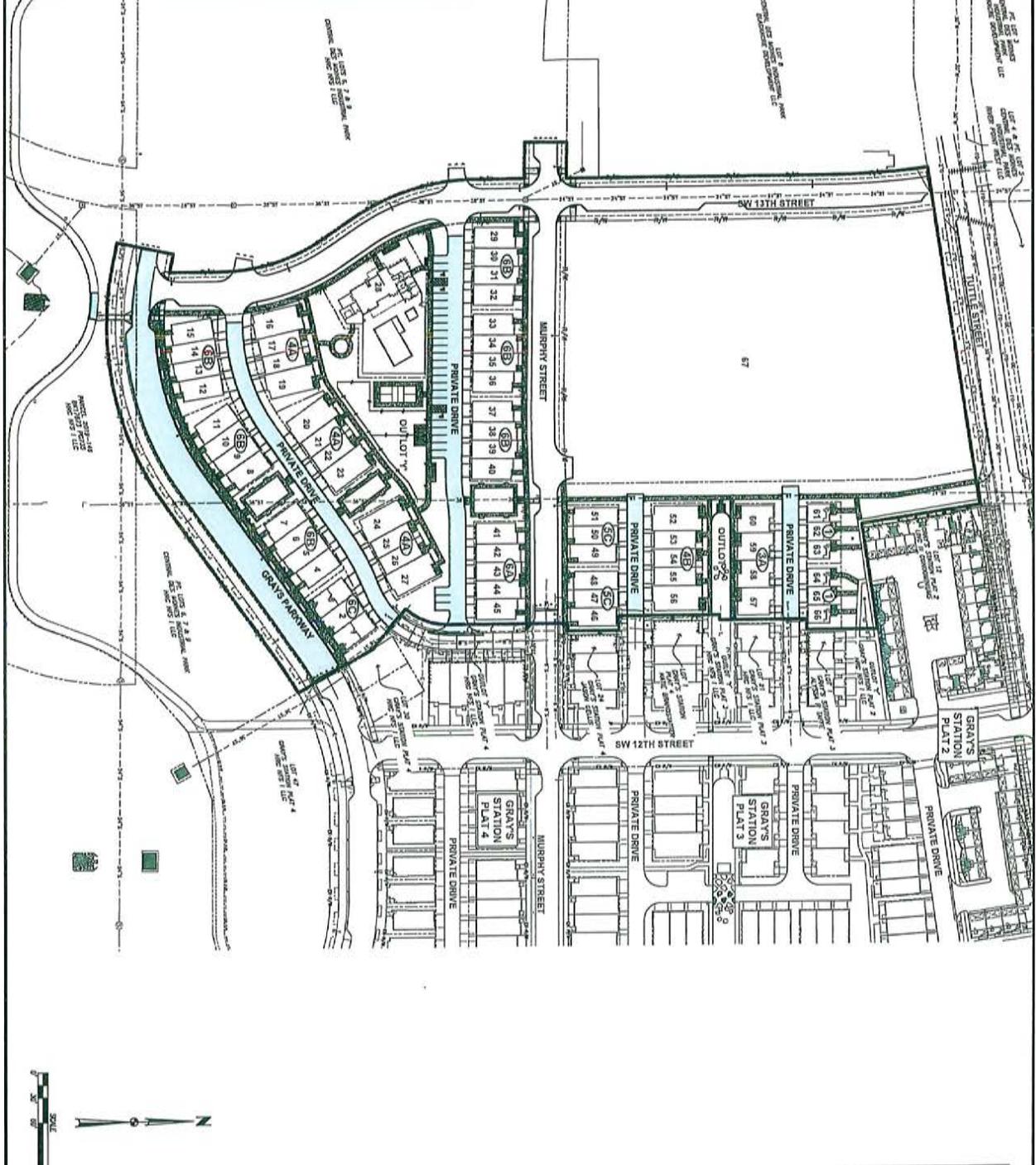
4121 NW URBANDALE DRIVE  
 URBANDALE, IA 50322  
 PHONE: (515) 969-4400



CIVIL DESIGN ADVANTAGE  
 ENGINEER

**GRAY'S STATION PLAT 5**  
 TOPOGRAPHIC SURVEY / DEMOLITION PLAN

C1.1  
 2202.113



NOTE: ALL PUBLIC IMPROVEMENTS, INCLUDING SPOUT ELEVATIONS, ARE SHOWN FOR REFERENCE ONLY. BEST TO THE SEWERAGE AND WATER DEPARTMENT PLANS. THERE ARE ANY DISCREPANCIES BETWEEN THE DEVELOPER'S PLANS AND THE SEWERAGE AND WATER DEPARTMENT'S PLANS, THE DEVELOPER'S PLANS SHALL CONTROL.

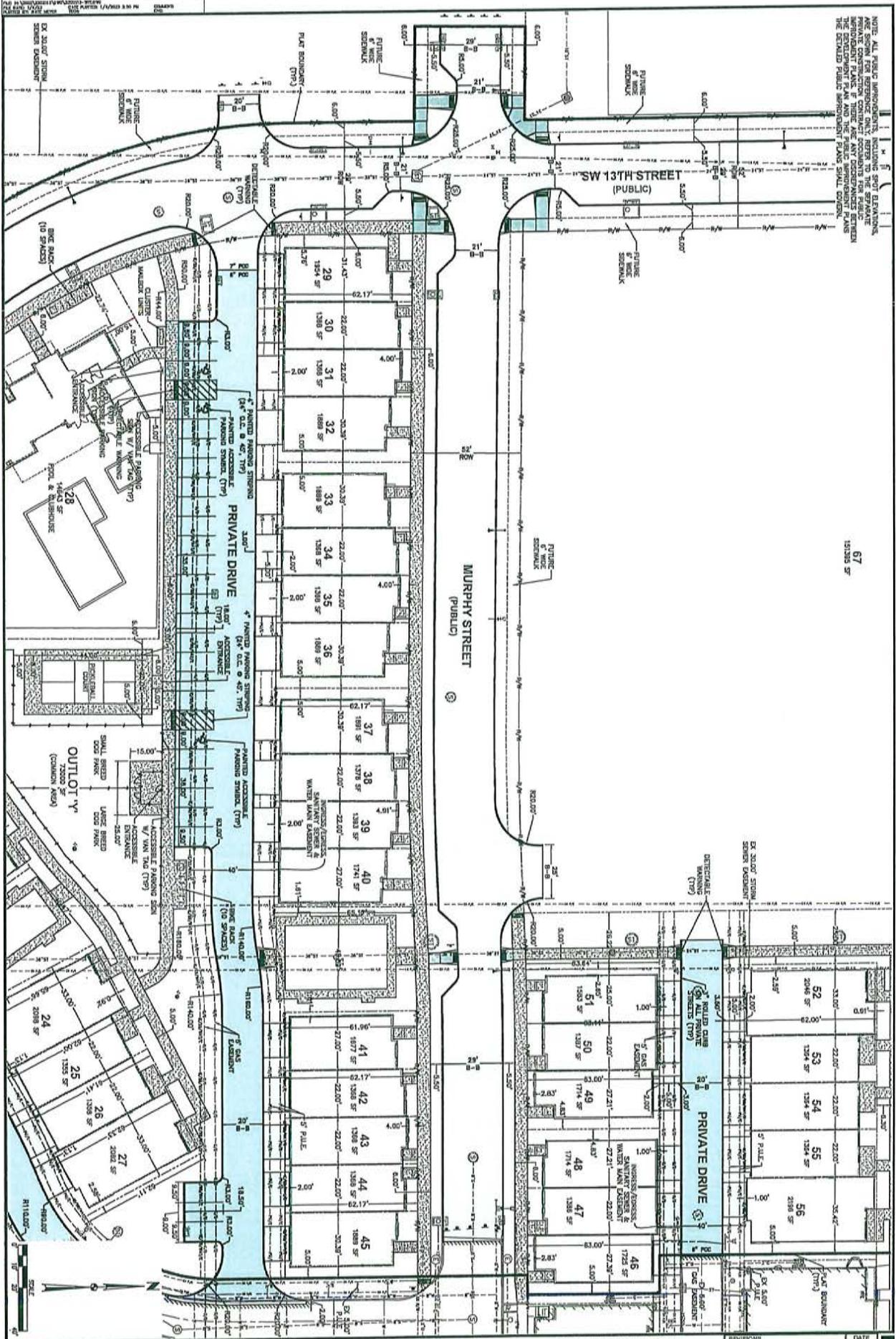
PLAT	PRODUCT NUMBER	PRODUCT DESCRIPTION
5	1 (LOTS 87-89)	PRODUCT 1 - 3 UNITS
5	3A (LOTS 87-89)	PRODUCT 3A - 4 UNITS
5	4A (LOTS 18-27)	PRODUCT 4A - 5 UNITS
5	4B (LOTS 28-35)	PRODUCT 4B - 5 UNITS
5	5C (LOTS 46-51)	PRODUCT 5C - 3 UNITS
5	5A (LOTS 61-63)	PRODUCT 5A - 5 UNITS
5	5B (LOTS 64-66)	PRODUCT 5B - 4 UNITS
5	5C (LOTS 67-69)	PRODUCT 5C - 3 UNITS

PLAT	PRODUCT NUMBER	LOT NUMBER	COLOR PACKAGE NUMBER
5	1	87-89	1
5	3A	87-89	1
5	4A	18-27	2
5	4A	28-35	1
5	4B	28-27	2
5	4B	32-36	2
5	5C	46-46	1
5	5C	46-51	1
5	5A	41-45	1
5	5B	4-7	2
5	5B	8-11	2
5	5B	12-15	2
5	5B	16-19	2
5	5B	20-23	2
5	5B	24-27	2
5	5C	31-30	2
5	5C	31-40	2
5	5C	1-3	2



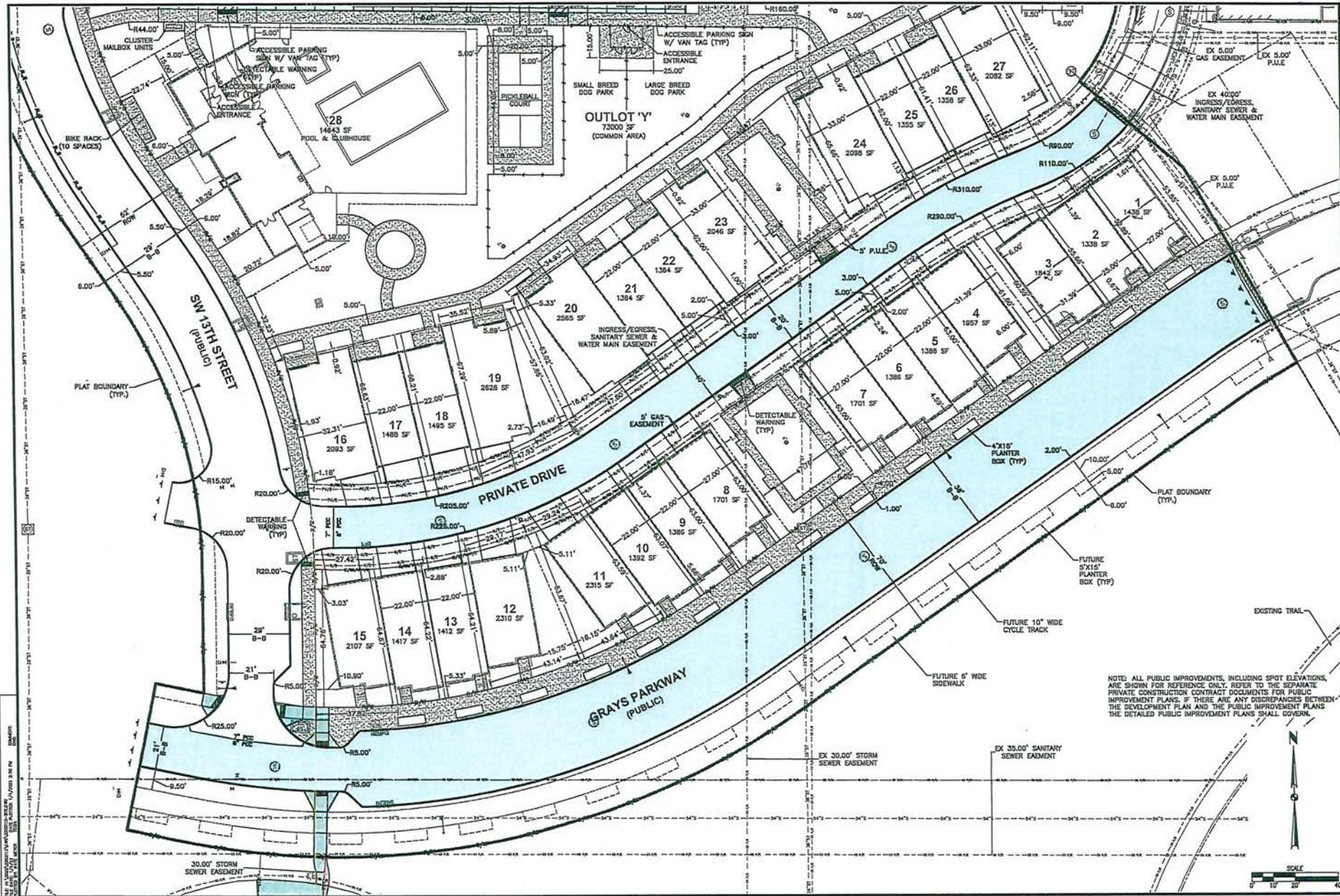


NOTE: ALL PUBLIC IMPROVEMENTS, INCLUDING GRYE ELEVATIONS, ARE SHOWN FOR REFERENCE ONLY. REFER TO THE SEPARATE IMPROVEMENT PLAN FOR ALL PUBLIC IMPROVEMENTS. ANY DISCREPANCIES BETWEEN THE SEPARATE PUBLIC IMPROVEMENT PLANS SHALL BE RESOLVED BY THE ENGINEER.



67  
15/20/20

<p><b>C2.3</b> 22022-1113</p>	<p><b>GRAY'S STATION PLAT 5</b> DIMENSION PLAN</p>	<p><b>CIVIL DESIGN ADVANTAGE</b></p>	<p>4121 NW URBANDALE DRIVE URBANDALE, IA 50322 PHONE: (515) 369-4400</p>	<p>ENGINEER:</p>	<p>TECH:</p>	<p>DATE</p>			
<p>REVISIONS</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td style="width: 80%;">SECOND SUBMITTAL</td> <td style="width: 20%;">01/09/2023</td> </tr> <tr> <td>FIRST SUBMITTAL</td> <td>10/19/2022</td> </tr> </table>						SECOND SUBMITTAL	01/09/2023	FIRST SUBMITTAL	10/19/2022
SECOND SUBMITTAL	01/09/2023								
FIRST SUBMITTAL	10/19/2022								



NOTE: ALL PUBLIC IMPROVEMENTS, INCLUDING SPOT ELEVATIONS, ARE SHOWN FOR REFERENCE ONLY. REFER TO THE SEPARATE PRIVATE CONSTRUCTION CONTRACT DOCUMENTS FOR PUBLIC IMPROVEMENT PLANS. IF THERE ARE ANY DISCREPANCIES BETWEEN THE DEVELOPMENT PLAN AND THE PUBLIC IMPROVEMENT PLANS THE DETAILED PUBLIC IMPROVEMENT PLANS SHALL GOVERN.

DATE	
REVISIONS	
SECOND SUBMITTAL	03/20/20
FIRST SUBMITTAL	01/07/20

4121 NW URBANDALE DRIVE  
 URBANDALE, IA 50322  
 PHONE: (515) 369-4400

TECH: \_\_\_\_\_  
 ENGINEER: \_\_\_\_\_

**GRAY'S STATION PLAT 5**  
 DIMENSION PLAN

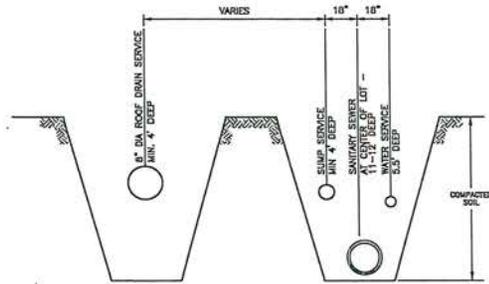
**C2.4**  
 2202.113



DES MOINES, IOWA

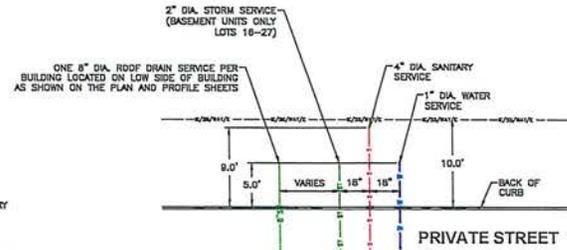
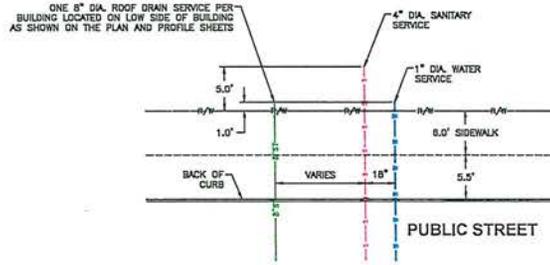




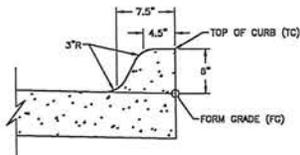


**NOTES:**

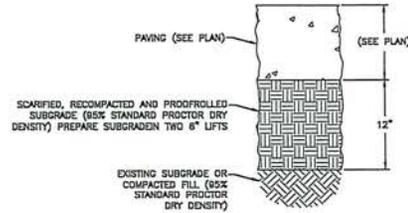
1. MARK THE LOCATION OF ALL SANITARY SEWER SERVICE STUBS AT THE TIME OF INSTALLATION BY A TWO-INCH WIDE DETECTABLE MARKING TAPE INSTALLED AT A DEPTH OF 18 INCHES TO 24 INCHES BELOW FINISHED GRADE, DIRECTLY OVER THE SERVICE STUB, FOR ITS ENTIRE LENGTH AND BROUGHT UP TO THE SURFACE AT THE END OF THE SERVICE STUB ADJACENT TO THE POST MARKING THE STUB LOCATION. THE TAPE SHALL BE GREEN IN COLOR AND MARKED "SANITARY SEWER SERVICE STUB BURIED BELOW".
2. PLACE ONE 6" BLUE STEEL POST BURIED 3' AT WATER CURB BOX.
3. PLACE ONE 6" RED STEEL POST BURIED 3' TO LOCATE STORM & STORM DRAIN SERVICES.
4. 2" DIA STORM SERVICES SHALL BE INSTALLED ONLY FOR UNITS WITH BASEMENTS (LOTS 18-27).



**1 SERVICE LOCATION DETAIL**  
NOT TO SCALE

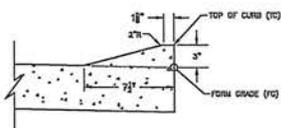


**2 6" INTEGRAL CURB**  
NOT TO SCALE

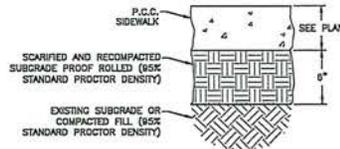


- NOTES:**
1. SUBGRADE PREPARATION SHALL EXTEND 24" BEYOND THE EDGE OF PAVING OR BACK OF CURB, EXCEPT WHEN ABUTTING EXISTING PAVEMENT.

**3 12" SUBGRADE PREPARATION**  
NOT TO SCALE

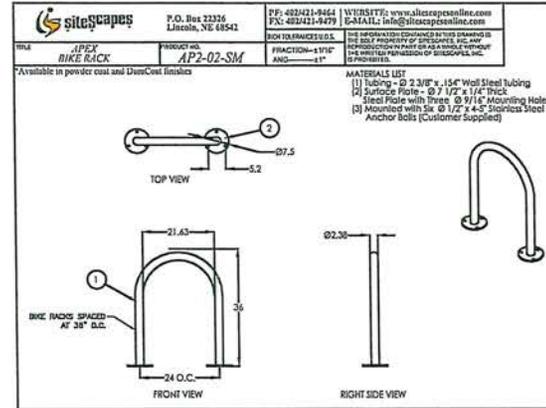


**4 3" INTEGRAL ROLLED CURB**  
NOT TO SCALE



- NOTES:**
1. SUBGRADE PREPARATION SHALL EXTEND 12" BEYOND THE EDGE OF PAVING, EXCEPT WHEN ABUTTING EXISTING PAVEMENT.

**5 SIDEWALK**  
NOT TO SCALE



**6 BIKE RACK**  
NOT TO SCALE

DATE	
REVISIONS	
SECOND SUBMITTAL	01/04/2012
FIRST SUBMITTAL	10/12/2011

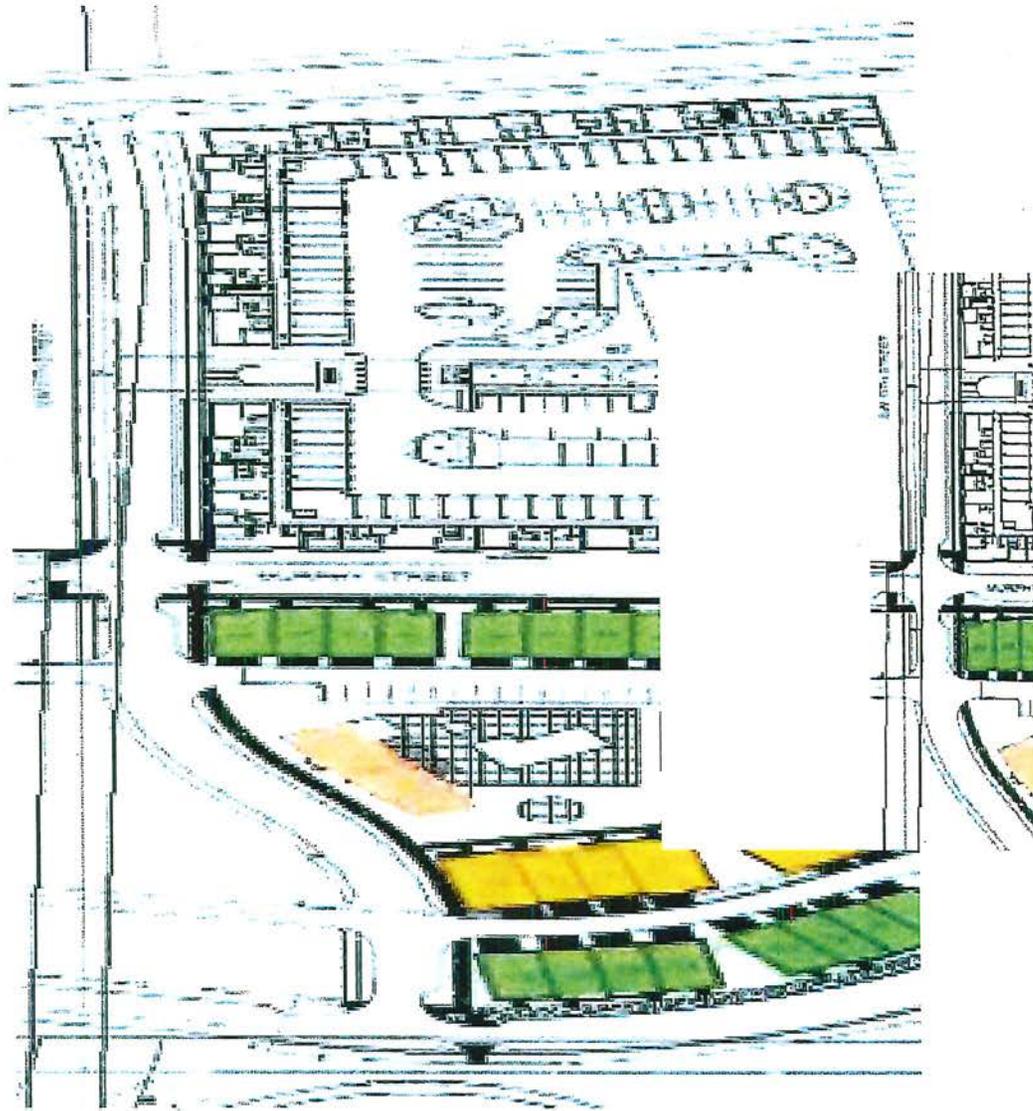
4121 NW URBANDALE DRIVE  
URBANDALE, IA 50022  
PHONE: (515) 368-4400



**GRAY'S STATION PLAT 5**  
DETAILS

**C5.2**  
2202.113

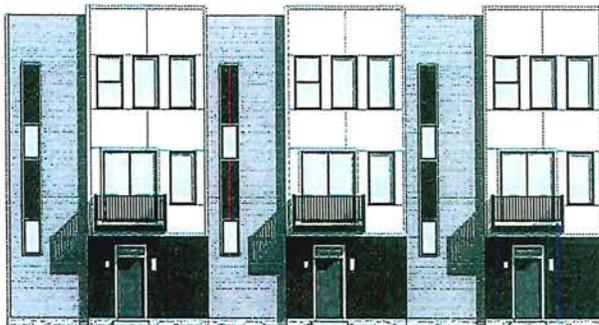






- ROOF MEMBRANE - CARLISLE SURE WELD TPO | GRAY
- FIBER CEMENT LAP SIDING & TRIM - HARDIE | AGED PEWTER
- FIBER CEMENT PANEL & TRIM - HARDIE | PEARL GRAY
- RAILING | BLACK
- METAL PANEL - FIRESTONE | DARK BRONZE
- PAINT - SW 7674- PEPPERCORN

NEW FRONT ELEVATION



OLD FRONT ELEVATION

## Elevation Updates in Phase II

In Phase 2 we are proposing the same Product types/units as were approved for Phase 1. The floorplans for each unit type will remain the same. We made some revisions to the Elevations and Exterior Color Packages as we felt that they needed to be updated for Phase II. Some of the elevations didn't have enough intentional variations and the color packages were too light in color for an urban neighborhood.

The following pages show the new and the old front elevations of each product type and a description of the changes.

### Product 1

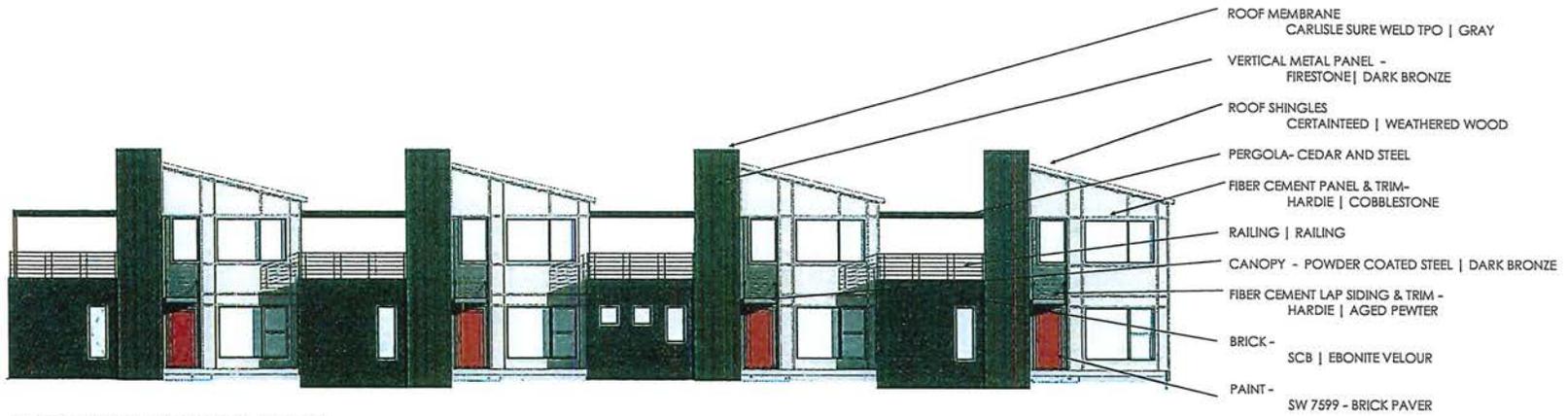
The desire was to create a darker color package for this Product and add some variation between the units.

- Increased visual interest through varying heights of the entry element clad in metal panels.
- Revised the color scheme and made it darker to achieve a more urban look.

## PRODUCT 1 | COMPARISON

COLOR PACKAGE: Product 1—#1

GRAY'S STATION | 07.07.2022 | 3



NEW FRONT ELEVATION

**Product 3**

This elevation looked too busy and had too many design themes and variations.

- Revised vertical elements to all be the same color and revised material to dark metal panels with a vertical pattern.
- The cube-shaped building elements were revised from Hardie lap siding/stone to all stone, to unify the overall building design.



OLD FRONT ELEVATION

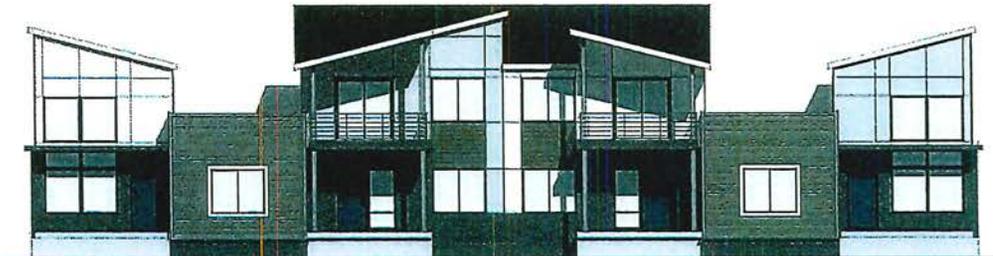
PRODUCT 3 | COMPARISON

COLOR PACKAGE: Product 3—#1

GRAY'S STATION | 07.07.2022 | 4



NEW FRONT ELEVATION



OLD FRONT ELEVATION

#### Product 4

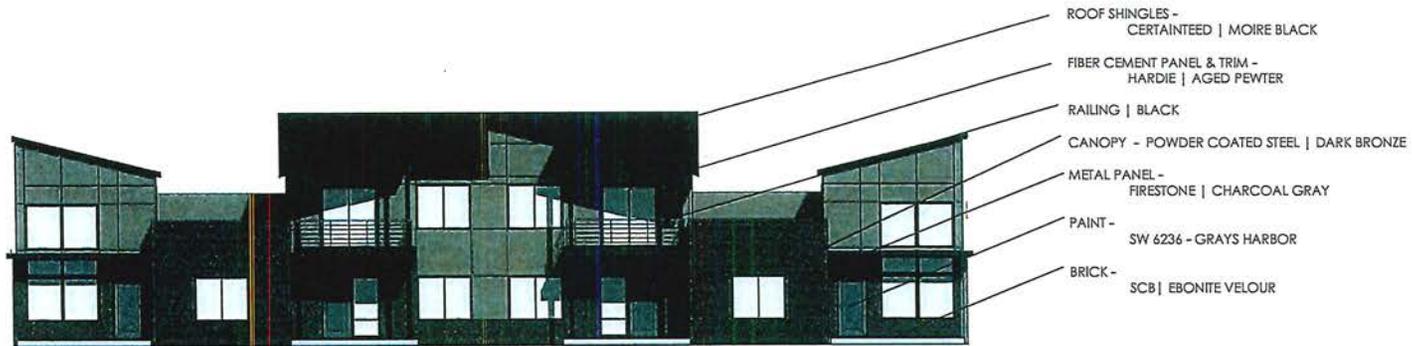
This elevation was too busy and the color scheme too light for this urban environment.

- Changed to a darker color scheme to achieve a more urban look.
- Changed Hardie lap siding on front to metal panels.
- Eliminated the white-colored Hardie panels and matched the panel color on both sides to achieve a unified appearance.
- Steel metal posts will be painted dark gray.

## PRODUCT 4 | COMPARISON

COLOR PACKAGE: Product 4—#1

GRAY'S STATION | 07.07.2022 | 5

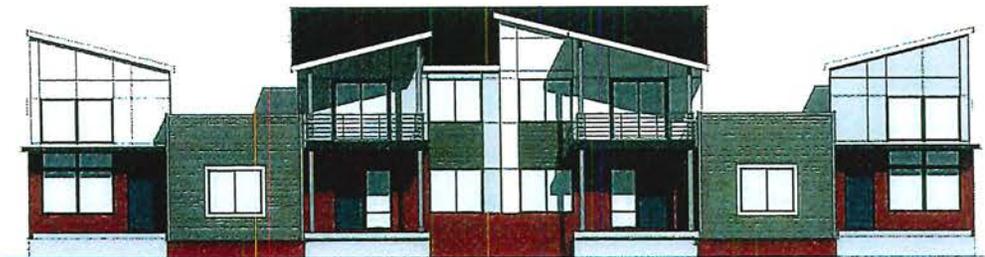


NEW FRONT ELEVATION

**Product 4**

This elevation was too busy and the color scheme too light for this urban environment.

- Changed to a darker color scheme to achieve a more urban look.
- Changed Hardie lap siding on front to metal panels.
- Eliminated the white-colored Hardie panels and matched the panel color on both sides to achieve a unified appearance.
- Steel metal posts will be painted dark gray.

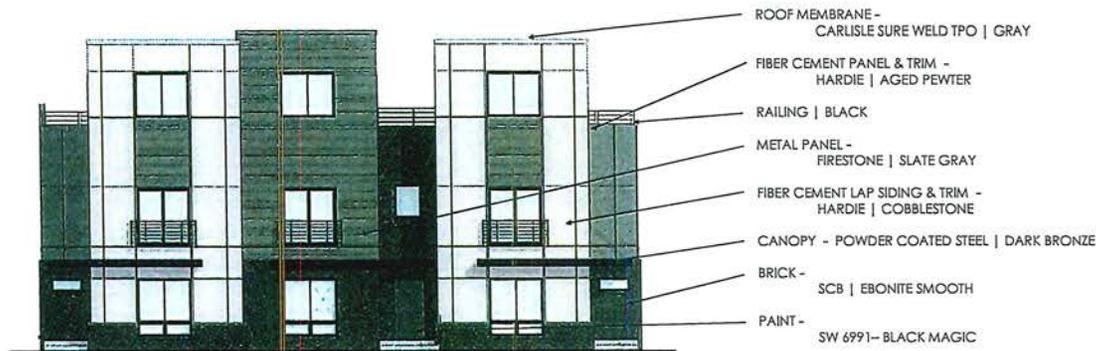


OLD FRONT ELEVATION

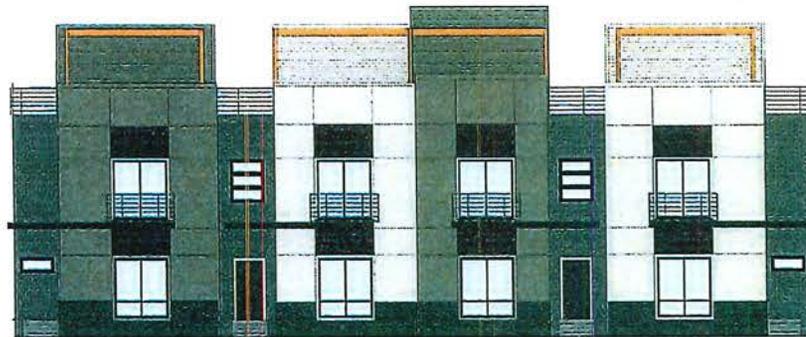
**PRODUCT 4 | COMPARISON**

COLOR PACKAGE: Product 4—#2

GRAY'S STATION | 07.07.2022 | 6



NEW FRONT ELEVATION



OLD FRONT ELEVATION

### Product 5

Elevations appear repetitive and uninspired. We only built one Product 5 building in Phase 1 so far. It was built without the 3<sup>rd</sup> level optional bonus room. We intend to build the bonus room on Product 5 in Phase 2.

- Varied the height of the brick to create diverse building elements and to reduce the effect of the stark vertical building elements.
- Replaced the Hardie panels of one of the main building elements with metal panels.
- Black windows

## PRODUCT 5 | COMPARISON

COLOR PACKAGE: Product 5—#1

GRAY'S STATION | 07.07.2022 | 7



NEW FRONT ELEVATION



OLD FRONT ELEVATION

### Product 6

Elevation appeared boring and repetitive. These needed additional visual interest and a more urban look.

- Varied the height of the brick/stone to create diverse building elements and to reduce the repetitive effect of the vertical building elements.
- Introduced building elements sided in metal panels with a vertical pattern.
- Replaced the Hardie panels of one or more of the main building elements with metal panels (horizontal pattern).
- Black windows
- Darkened the color packages

## PRODUCT 6 | COMPARISON

COLOR PACKAGE: Product 6—#1

GRAY'S STATION | 07.07.2022 | 8



NEW FRONT ELEVATION



OLD FRONT ELEVATION

#### Product 6

Elevation appeared boring and repetitive. These needed additional visual interest and a more urban look.

- Varied the height of the brick/stone to create diverse building elements and to reduce the repetitive effect of the vertical building elements.
- Introduced building elements sided in metal panels with a vertical pattern.
- Replaced the Hardie panels of one or more of the main building elements with metal panels (horizontal pattern).
- Black windows
- Darkened the color packages

## PRODUCT 6 | COMPARISON

COLOR PACKAGE: Product 6—#2

GRAY'S STATION | 07.07.2022 | 9



NEW FRONT ELEVATION



OLD FRONT ELEVATION

#### Product 6

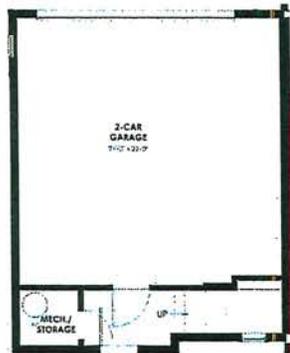
Elevation appeared boring and repetitive. These needed additional visual interest and a more urban look.

- Varied the height of the brick/stone to create diverse building elements and to reduce the repetitive effect of the vertical building elements.
- Introduced building elements sided in metal panels with a vertical pattern.
- Replaced the Hardie panels of one or more of the main building elements with metal panels (horizontal pattern).
- Black windows
- Darkened the color packages

## PRODUCT 6 | COMPARISON

COLOR PACKAGE: Product 6—#3

GRAY'S STATION | 07.07.2022 | 10



FIRST FLOOR

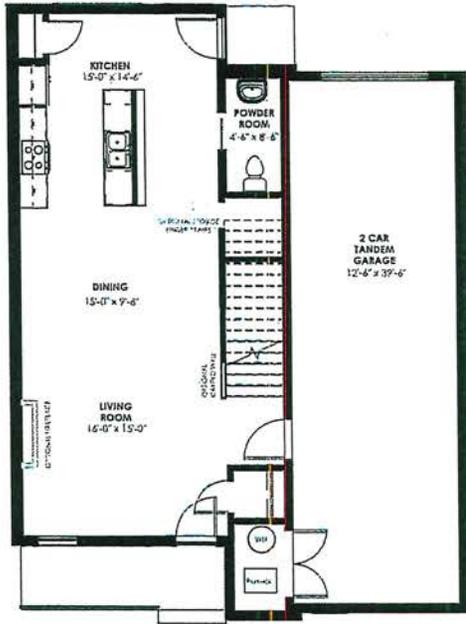


SECOND FLOOR

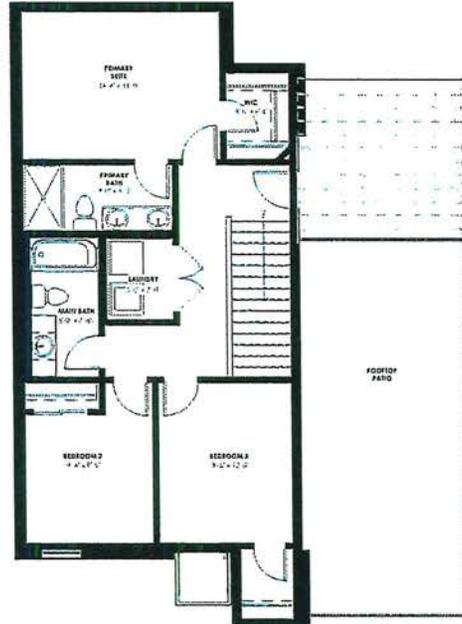


THIRD FLOOR

PRODUCT 1 | PRESTON  
 Lots: 61-63 and 64-66  
 GRAY'S STATION | 07.07.2022 | 45



FIRST FLOOR

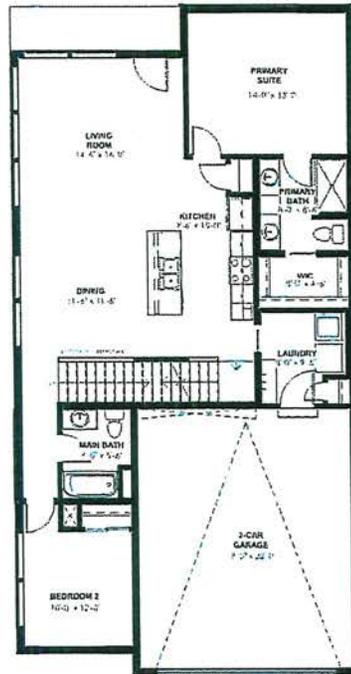


SECOND FLOOR

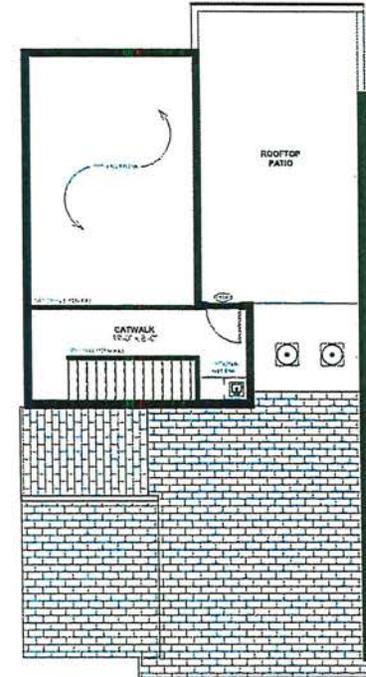
PRODUCT 3 | BEXLEY  
 Lots: 57-60  
 GRAY'S STATION | 07.07.2022 | 46



LOWER LEVEL

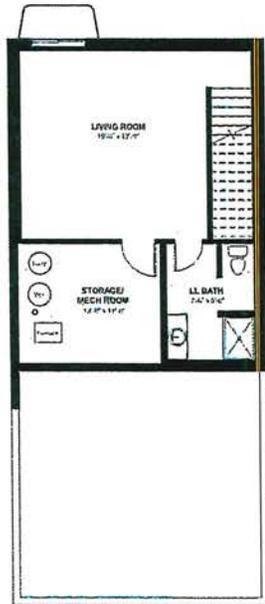


FIRST FLOOR

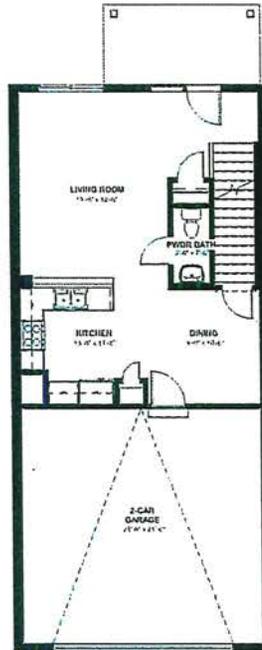


SECOND FLOOR

PRODUCT 4 | MAISON  
 Lots: 16&19, 20&23, 24&27 and 52&56  
 GRAY'S STATION | 07.07.2022 | 47



LOWER LEVEL

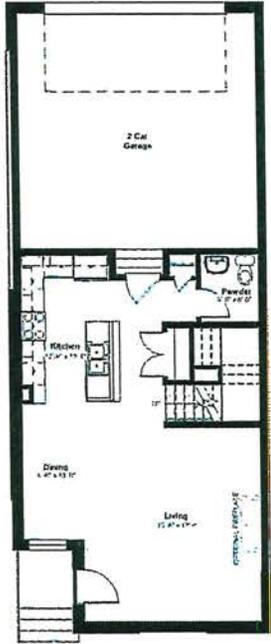


FIRST FLOOR

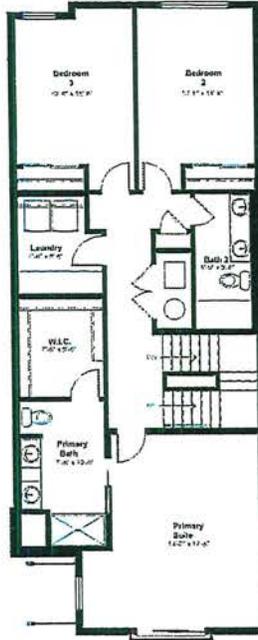


SECOND FLOOR

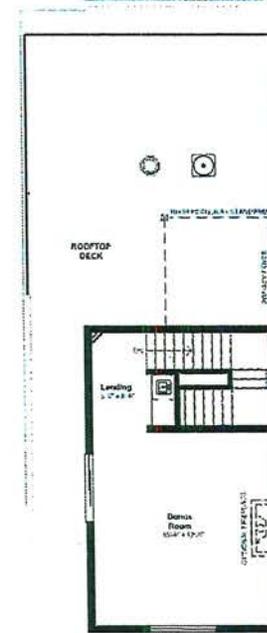
PRODUCT 4 | MARLOW  
 Lots: 17&18, 21&22, 25&26, and 53-55  
 GRAY'S STATION | 07.07.2022 | 48



FIRST FLOOR



SECOND FLOOR

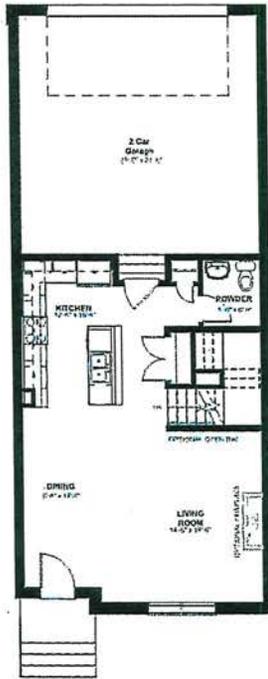


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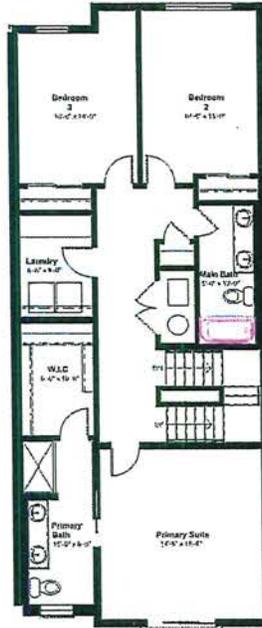
## PRODUCT 5 | CHAVA

Lots: 46&48 and 49&51

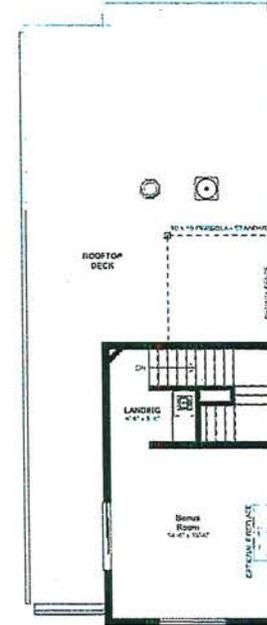
GRAY'S STATION | 07.07.2022 | 49



FIRST FLOOR



SECOND FLOOR



THIRD FLOOR

## PRODUCT 5 | ASANA

Lots: 47 and 50

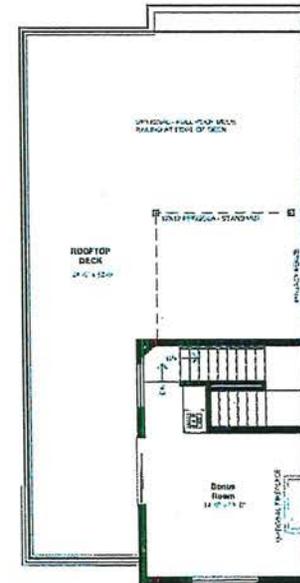
GRAY'S STATION | 07.07.2022 | 50



FIRST FLOOR



SECOND FLOOR

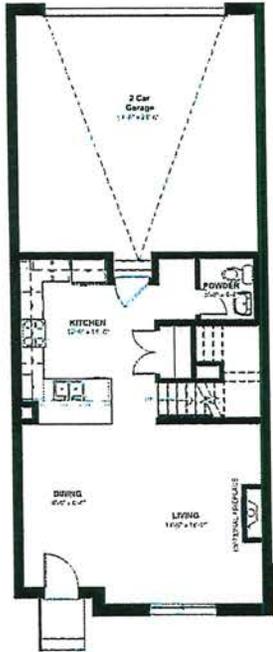


THIRD FLOOR

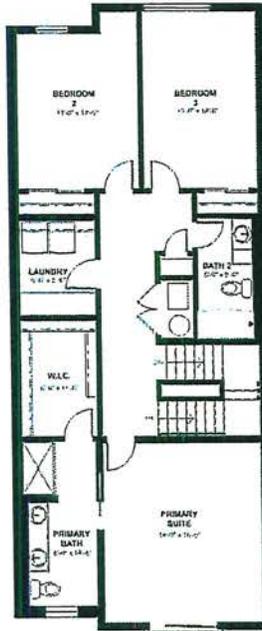
## PRODUCT 6 | GRAYSON

Lots: 1&3, 4&7, 8&11, 12&15, 29&32, 33&36, 37&40, and 41&45

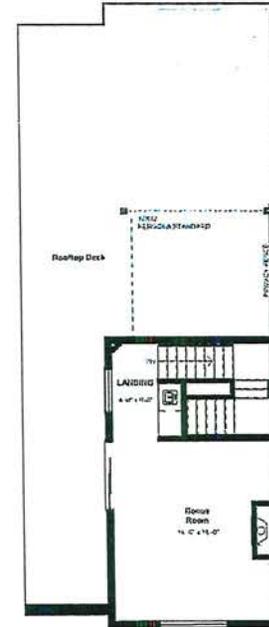
GRAY'S STATION | 07.07.2022 | 51



FIRST FLOOR



SECOND FLOOR

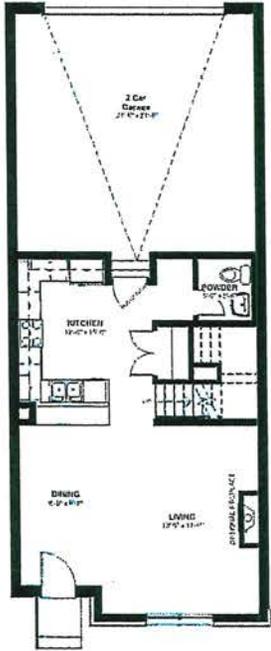


THIRD FLOOR

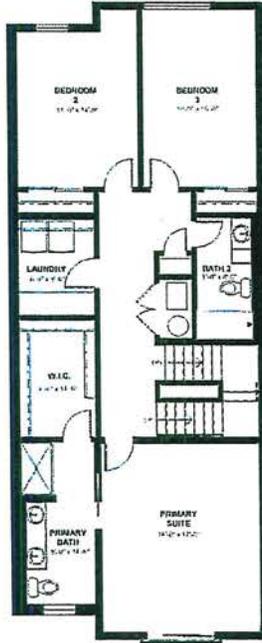
## PRODUCT 6 | CORDOVA

Lots: 2, 5, 9, 13, 30, 34, 38 and 42&44

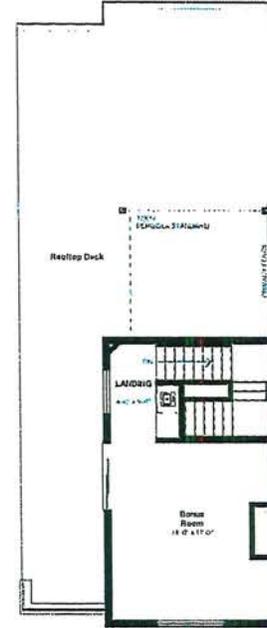
GRAY'S STATION | 07.07.2022 | 52



FIRST FLOOR



SECOND FLOOR



THIRD FLOOR

## PRODUCT 6 | CORDOVA PLUS

Lots: 6, 10, 14, 31, 35, 39 and 43

GRAY'S STATION | 07.07.2022 | 53



Hubbell Homes  
Des Moines, Iowa

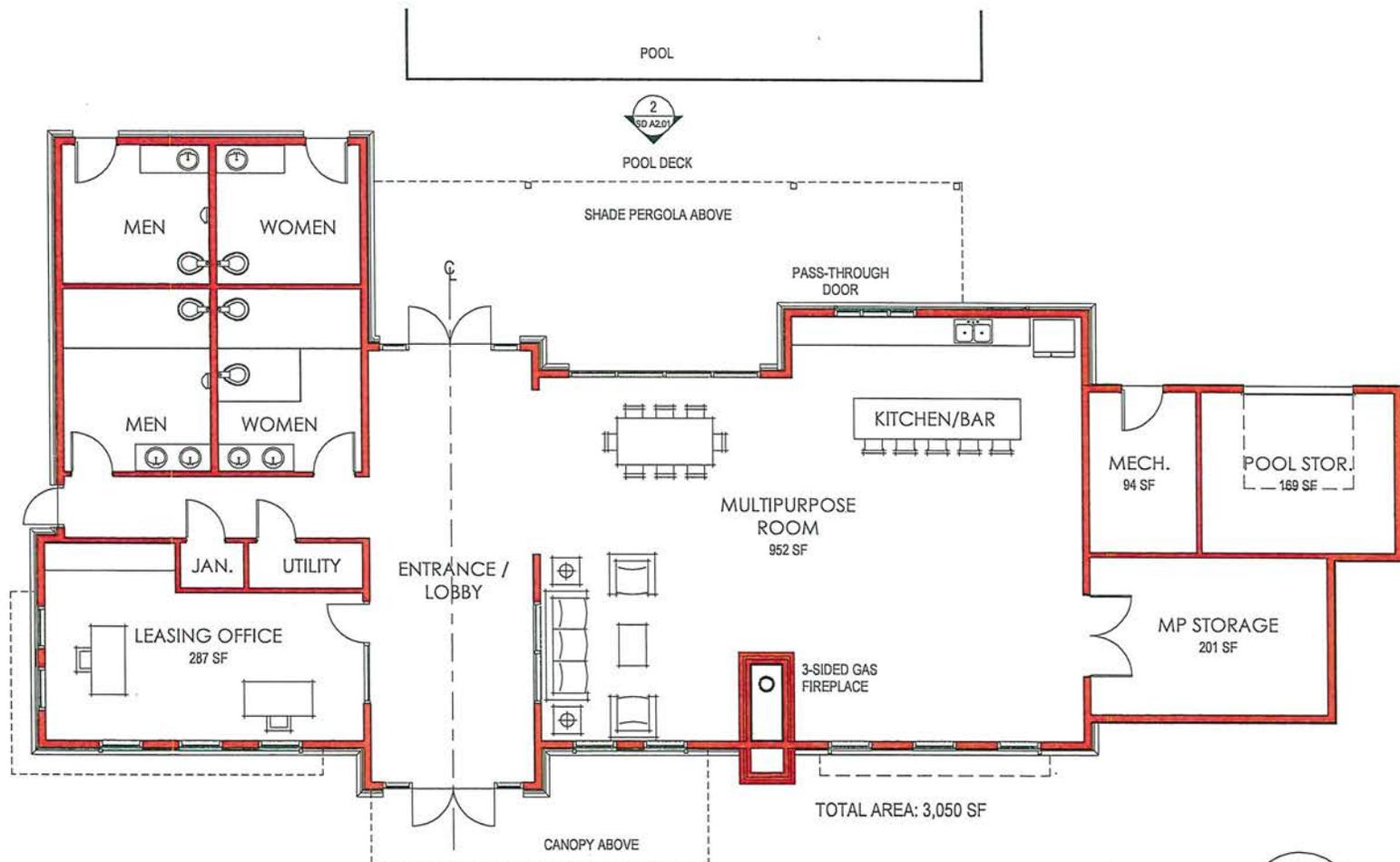
PERSPECTIVE

GRAY'S STATION CLUBHOUSE

The drawings presented are illustrative character and design intent only, and are subject to change based upon final design considerations (i.e. applicable codes, structural, and MEP design requirements, unit plan/floor plan changes, etc.) © 2018 BSB Design, Inc.

[AM220361]





Hubbell Homes  
Des Moines, Iowa

CONCEPTUAL FLOOR PLAN 7/14/2022

GRAY'S STATION CLUBHOUSE



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[AM220361]



1 SOUTH ELEVATION  
1/8" = 1'-0"



2 NORTH ELEVATION  
1/8" = 1'-0"

Hubbell Homes  
Des Moines, Iowa

ELEVATIONS

GRAY'S STATION CLUBHOUSE

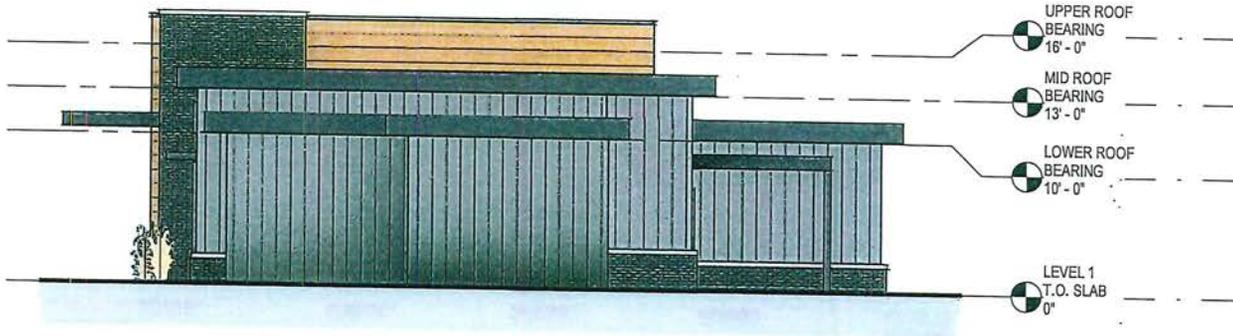
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[AM220361]





1 WEST ELEVATION  
1/8" = 1'-0"



2 EAST ELEVATION  
1/8" = 1'-0"

ELEVATIONS

Hubbell Homes  
Des Moines, Iowa

GRAY'S STATION CLUBHOUSE



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