



**Date** March 27, 2023

**RESOLUTION HOLDING HEARING REGARDING REQUEST FROM HUBBELL REALTY COMPANY (OWNER), REPRESENTED BY CALEB SMITH (OFFICER), FOR REVIEW AND APPROVAL OF A 1<sup>ST</sup> AMENDMENT TO THE GRAY’S STATION PUD CONCEPTUAL PLAN, FOR PROPERTY LOCATED IN THE VICINITY OF 1300 TUTTLE STREET, TO UPDATE THE REALIGNMENT OF SOUTHWEST 13<sup>TH</sup> STREET AND TO REVISE REVIEW REQUIREMENTS FOR FUTURE PUD DEVELOPMENT SITE PLANS**

**WHEREAS**, on February 2, 2023, the City Plan and Zoning Commission considered a request from Hubbell Realty Company (Owner), represented by Caleb Smith (Officer), for review and approval of a 1<sup>st</sup> amendment to the Gray’s Station PUD Conceptual Plan, on property located in the vicinity of 1300 Tuttle Street, to update the realignment of Southwest 13<sup>th</sup> Street and to revise review requirements for future PUD Development Site Plans; and

**WHEREAS**, the City Plan and Zoning Commission voted 8-1-1 to **APPROVE** the 1<sup>st</sup> amendment to the Gray’s Station PUD Conceptual Plan, on property located in the vicinity of 1300 Tuttle Street, to update the realignment of Southwest 13<sup>th</sup> Street and to revise review requirements for future PUD development site plans, subject to revisions to the ‘Notes’ section to allow administrative review of future development site plans in lieu of public review by the Plan and Zoning Commission and City Council, as follows:

1. Any final development plan for a new row home type development or a minor revision to an existing development is subject to administrative (by City staff) review and approval.
2. Any final development plan for a mixed-use building or a multiple-household residential building is subject to review and approval by the Plan and Zoning Commission, with any appeal of that decision considered by the City Council; and

**WHEREAS**, the Property is legally described as follows:

OUTLOTS Y AND Z AND A PART OF LOTS 6, 7 AND 9, CENTRAL DES MOINES INDUSTRIAL PARK, AN OFFICIAL PLAT AND PART OF LOTS 80, 81, 82, 85 AND 86 IN FACTORY ADDITION, AN OFFICIAL PLAT, ALL BEING IN THE CITY OF DES MOINES, POLK COUNTY, IOWA AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF SAID LOT 9, CENTRAL DES MOINES INDUSTRIAL PARK HAVING A STATE PLANE IOWA SOUTH COORDINATE VALUE OF NORTHING 575161.83 AND EASTING 1602813.57; THENCE SOUTH 89° (DEGREES) 48' (MINUTES) 50" (SECONDS) EAST ALONG THE NORTHERLY LINE OF SAID LOT 9, A DISTANCE OF 339.93 FEET TO THE SOUTHWEST CORNER OF LOT 2, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE SOUTH 89°46'52" EAST CONTINUING ALONG SAID NORTHERLY LINE, 419.55 FEET TO THE SOUTHEAST CORNER OF SAID LOT 2 ALSO BEING A POINT ON THE WEST RIGHT OF WAY LINE OF SW 14TH STREET; THENCE SOUTH 00°01'02" EAST CONTINUING ALONG SAID NORTHERLY LINE, 69.98





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FEET TO THE SOUTHWEST CORNER OF STREET LOT B, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE SOUTH 89°35'21" EAST CONTINUING ALONG SAID NORTHERLY LINE, 70.00 FEET TO THE NORTHWEST CORNER OF LOT 8, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE SOUTH 00°08'06" WEST CONTINUING ALONG SAID NORTHERLY LINE, 423.70 FEET TO THE SOUTHWEST CORNER OF SAID LOT 8; THENCE SOUTH 89°19'18" EAST CONTINUING ALONG SAID NORTHERLY LINE, 307.20 FEET TO THE SOUTHEAST CORNER OF SAID LOT 8, ALSO BEING THE NORTHEAST CORNER OF SAID LOT 9; THENCE NORTH 00°08'11" WEST ALONG THE WESTERLY LINE OF SAID LOT 7, CENTRAL DES MOINES INDUSTRIAL PARK, 333.39 FEET; THENCE NORTHERLY CONTINUING ALONG SAID WESTERLY LINE AND A CURVE CONCAVE WESTERLY WHOSE RADIUS IS 804.20 FEET, WHOSE ARC LENGTH IS 93.82 FEET AND WHOSE CHORD BEARS NORTH 03°29'07" WEST, 93.77 FEET; THENCE NORTH 06°58'06" WEST CONTINUING ALONG SAID WESTERLY LINE, 15.78 FEET TO THE NORTHWEST CORNER OF SAID LOT 7 ALSO BEING THE SOUTH RIGHT OF WAY LINE OF TUTTLE STREET; THENCE NORTH 81°10'49" EAST ALONG THE NORTH LINE OF SAID LOT 7, A DISTANCE OF 411.78 FEET TO THE NORTHEAST CORNER OF SAID LOT 7; THENCE NORTH 00°05'48" WEST ALONG THE WEST LINE OF SAID LOT 6, CENTRAL DES MOINES INDUSTRIAL PARK, 72.11 FEET TO THE NORTHWEST CORNER OF SAID LOT 6; THENCE NORTH 81°52'47" EAST ALONG THE NORTH LINE OF SAID LOT 6, A DISTANCE OF 347.62 FEET TO THE SOUTHEAST CORNER OF LOT 4, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE NORTH 00°31'45" WEST ALONG THE WESTERLY LINE OF SAID OUTLOT Y, CENTRAL DES MOINES INDUSTRIAL PARK, 63.43 FEET; THENCE NORTH 87°29'10" EAST CONTINUING ALONG SAID WESTERLY LINE, 10.37 FEET; THENCE NORTH 00°12'49" WEST CONTINUING ALONG SAID WESTERLY LINE, 554.99 FEET TO THE NORTHEAST CORNER OF LOT 5, SAID CENTRAL DES MOINES INDUSTRIAL PARK AND HAVING A STATE PLANE IOWA SOUTH COORDINATE VALUE OF NORTHING 575907.16 AND EASTING 1604699.48; THENCE SOUTH 89°04'27" EAST ALONG THE NORTH LINE OF SAID OUTLOT Y, 15.36 FEET TO THE NORTHEAST CORNER OF SAID OUTLOT Y; THENCE NORTH 00°21'12" WEST ALONG THE WEST LINE OF SAID LOT 85, FACTORY ADDITION, 8.59 FEET TO THE SOUTHERLY RIGHT OF WAY LINE OF MARTIN LUTHER KING JR PARKWAY; THENCE NORTH 86°10'40" EAST ALONG SAID SOUTHERLY RIGHT OF WAY LINE, 100.23 FEET; THENCE NORTH 84°40'04" EAST CONTINUING ALONG SAID SOUTHERLY RIGHT OF WAY LINE, 199.44 FEET TO THE WESTERLY RIGHT OF WAY LINE OF SW 11TH STREET; THENCE SOUTH 15°27'22" EAST ALONG SAID WESTERLY RIGHT OF WAY LINE, 173.21 FEET; THENCE SOUTH 74°42'29" WEST, 99.77 FEET; THENCE SOUTH 15°25'34" EAST, 75.29 FEET; THENCE NORTH 74°32'27" EAST, 99.81 FEET TO SAID WESTERLY RIGHT OF WAY LINE; THENCE SOUTH 15°27'22" EAST ALONG SAID WESTERLY RIGHT OF WAY LINE, 344.37 FEET TO A CORNER ON THE EASTERLY LINE OF SAID LOT 81; THENCE SOUTH 39°15'32" WEST ALONG SAID EASTERLY LINE, 97.08 FEET; THENCE SOUTH 79°27'19" WEST, 73.56 FEET TO THE EASTERLY LINE OF THE WEST HALF OF SAID LOT 81; THENCE





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SOUTHERLY ALONG SAID EASTERLY LINE AND A CURVE CONCAVE EASTERLY WHOSE RADIUS IS 716.25 FEET, WHOSE ARC LENGTH IS 712.02 FEET AND WHOSE CHORD BEARS SOUTH 06°02'51" WEST, 683.06 FEET; THENCE SOUTH 67°48'12" WEST, 51.41 FEET TO A POINT ON THE EAST LINE OF THE WEST 100 FEET OF SAID LOT 80, FACTORY ADDITION; THENCE SOUTH 00°01'40" EAST ALONG SAID EAST LINE, 767.72 FEET; THENCE SOUTH 45°36'19" EAST, 692.69 FEET TO THE WESTERLY LINE OF SAID LOT 81; THENCE SOUTH 76°09'29" EAST, 50.00 FEET TO SAID EASTERLY LINE OF THE WEST HALF OF LOT 81, HAVING A STATE PLANE IOWA SOUTH COORDINATE VALUE OF NORTHING 573317.97 AND EASTING 1605461.84; THENCE SOUTHERLY ALONG SAID EASTERLY LINE AND A CURVE CONCAVE WESTERLY WHOSE RADIUS IS 740.65 FEET, WHOSE ARC LENGTH IS 76.04 FEET AND WHOSE CHORD BEARS SOUTH 17°00'55" WEST, 76.01 FEET; THENCE NORTH 70°20'25" WEST, 50.00 FEET TO SAID WESTERLY LINE OF LOT 81; THENCE NORTH 45°36'19" WEST, 663.55 FEET TO SAID EAST LINE OF THE WEST 100 FEET OF LOT 80; THENCE SOUTH 00°01'40" EAST ALONG SAID EAST LINE, 439.03 FEET TO THE NORTH BANK AND MEANDER LINE OF THE RACCOON RIVER; THENCE NORTH 71°44'57" WEST AND ALONG SAID MEANDER LINE AND THE SOUTHERLY LINE OF SAID OUTLOT Z, CENTRAL DES MOINES INDUSTRIAL PARK, 888.50 FEET; THENCE NORTH 83°01'49" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 463.82 FEET; THENCE NORTH 89°10'53" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 291.24 FEET; THENCE SOUTH 82°06'06" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 296.82 FEET; THENCE SOUTH 72°02'02" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 228.32 FEET TO THE SOUTHWEST CORNER OF SAID OUTLOT Z; THENCE NORTH 00°03'17" EAST ALONG THE WEST LINE OF SAID CENTRAL DES MOINES INDUSTRIAL PARK, 1647.05 FEET TO THE POINT OF BEGINNING AND CONTAINING 83.73 ACRES (3,647,186 SQUARE FEET) AND IS SUBJECT TO ANY AND ALL EASEMENTS OF RECORD.; and

**WHEREAS**, on February 20, 2023 by Roll Call No. 23-0224, it was duly resolved by the City Council that the request for approval of the 1<sup>st</sup> Amendment to the Gray’s Station PUD Conceptual Plan be set down for hearing on March 6, 2023, at 5:00 p.m., at the City Council Chambers; and

**WHEREAS**, on March 6, 2023, by Roll Call No. 23-0331, the City Council opened and further continued said hearing to 5:00 p.m. on March 27, 2023, upon request by the applicants’ representative for additional review time; and

**WHEREAS**, due notice of said hearing was published in the Des Moines Register, as provided by law, setting forth the time and place for hearing on said proposed 1<sup>st</sup> Amendment to the Gray’s Station PUD Conceptual Plan; and



**Date** March 27, 2023

**WHEREAS**, in accordance with said notice, those interested in said proposed 1<sup>st</sup> Amendment to the Gray’s Station PUD Conceptual Plan, both for and against, have been given opportunity to be heard with respect thereto and have presented their views to the City Council.

**NOW, THEREFORE, BE IT RESOLVED**, by the City Council of the City of Des Moines, Iowa, as follows:

1. Upon due consideration of the facts, and any and all statements of interested persons and arguments of counsel, any objections to the proposed 1<sup>st</sup> Amendment to the Gray’s Station PUD Conceptual Plan are hereby overruled, and the hearing is closed.
2. The proposed 1<sup>st</sup> Amendment to the Gray’s Station PUD Conceptual Plan is hereby found to be in conformance with PlanDSM: Creating Our Tomorrow Plan and is hereby approved as stated above, subject to conditions set forth above.

MOVED by \_\_\_\_\_ to adopt, SECOND by \_\_\_\_\_.

FORM APPROVED:

/s/ Chas M. Cahill  
Chas M. Cahill  
Assistant City Attorney

(ZONG-2022-000095)

COUNCIL ACTION	YEAS	NAYS	PASS	ABSENT
COWNIE				
BOESEN				
GATTO				
SHEUMAKER				
MANDELBAUM				
VOSS				
WESTERGAARD				
TOTAL				
MOTION CARRIED			APPROVED	

**CERTIFICATE**

I, LAURA BAUMGARTNER, City Clerk of said City hereby certify that at a meeting of the City Council of said City of Des Moines, held on the above date, among other proceedings the above was adopted.

IN WITNESS WHEREOF, I have hereunto set my hand and affixed my seal the day and year first above written.

\_\_\_\_\_  
City Clerk

\_\_\_\_\_  
Mayor



February 14, 2023

Communication from the City Plan and Zoning Commission advising that at their February 2, 2023 meeting, the following action was taken regarding a request from Hubbell Realty Company (owner), represented by Caleb Smith (officer), for the following regarding property located in the vicinity of 1300 Tuttle Street:

- A) Review and approval of a 1<sup>st</sup> amendment to the Gray's Station PUD Conceptual Plan, to update the realignment of Southwest 13<sup>th</sup> Street and to allow administrative review of future development site plans in leu of public review by the Plan and Zoning Commission and City Council.
- B) Review and approval of a PUD Final Development Plan "Gray's Station Telus Condos," to allow for the construction of two condo buildings containing 84 residential units.
- C) Review and approval of a PUD Final Development Plan "Gray's Station Plat 5 Townhomes and Clubhouse," to allow for the construction of 65 townhomes and a clubhouse amenity building.

**COMMISSION RECOMMENDATION:**

After public hearing, the members voted 8-1-1 as follows.

Commission Action:	Yes	Nays	Pass	Absent
Francis Boggus				X
Dan Drendel	X			
Leah Rudolphi	X			
Carol Maher		X		
Abby Chungath				X
Kayla Berkson	X			
Chris Draper			X	
Todd Garner				X
Johnny Alcivar	X			
Justyn Lewis	X			
Carolyn Jenison				X
William Page	X			
Andrew Lorentzen				X
Emily Webb	X			
Katie Gillette	X			



**APPROVAL** of Part A) Approval of the request to amend the Gray's Station PUD Conceptual Plan to update the realignment of Southwest 13th Street.

Furthermore, Staff recommends limited approval of the request to amend the Gray's Station PUD Conceptual Plan to revise the 'Notes' section to allow administrative review of future development site plans in lieu of public review by the Plan and Zoning Commission and City Council, as follows:

- Any final development plan for a new row home type development or a minor revision to an existing development is subject to administrative (by staff) review and approval.
- Any final development plan for a mixed-use building or a multiple-household residential building is subject to review and approval by the Plan and Zoning Commission, with any appeal of that decision considered by the City Council.

Part B) Approval of the proposed PUD Final Development Plan "Gray's Station Telus Condos," subject to compliance with all administrative review comments, plus the following conditions:

1. Provision of a "band" of architectural concrete masonry unit materials from grade up to 2' for the standalone garages that matches the concrete masonry units proposed for the condo buildings.
2. Provision of a sidewalk connection from the southeastern corner of "Building 2" that crosses the internal vehicular drive to link up with the north-south pedestrian pathway that bisects this development area.
3. Provision of more-detailed exterior façade materiality information that matches the standards outlined within the PUD Conceptual Plan.
4. Verification that all exterior façade glazing contains a minimum 50% transmittance factor and a reflectance factor of no more than .25.

Part C) Approval of the proposed PUD Final Development Plan "Gray's Station Plat 5 Townhomes and Clubhouse," subject to compliance with all administrative review comments, plus the following conditions:

1. Provision of a sidewalk connection from the clubhouse's outdoor recreation area/dog park that crosses a private drive to link up with a proposed courtyard space in between lots "40" and "41."
2. Verification that any proposed rooftop mechanical equipment will be screened from view from all sides.
3. Provision of additional information regarding mechanical equipment and associated screening (both ground- and roof-mounted) for the clubhouse building.
4. Provision of more-detailed materiality information for the clubhouse building that matches the standards outlined within the PUD Conceptual Plan.



5. Verification that all exterior façade glazing contains a minimum 50% transmittance factor and a reflectance factor of no more than .25.

## **STAFF RECOMMENDATION TO THE P&Z COMMISSION**

Part A) Staff recommends approval of the request to amend the Gray's Station PUD Conceptual Plan to update the realignment of Southwest 13th Street.

Furthermore, Staff recommends limited approval of the request to amend the Gray's Station PUD Conceptual Plan to revise the 'Notes' section to allow administrative review of future development site plans in lieu of public review by the Plan and Zoning Commission and City Council, as follows:

- Any final development plan for a new row home type development or a minor revision to an existing development is subject to administrative (by staff) review and approval.
- Any final development plan for a mixed-use building or a multiple-household residential building is subject to review and approval by the Plan and Zoning Commission, with any appeal of that decision considered by the City Council.

Part B) Staff recommends approval of the proposed PUD Final Development Plan "Gray's Station Telus Condos," subject to compliance with all administrative review comments, plus the following conditions:

1. Provision of a "band" of architectural concrete masonry unit materials from grade up to 2' for the standalone garages that matches the concrete masonry units proposed for the condo buildings.
2. Provision of a sidewalk connection from the southeastern corner of "Building 2" that crosses the internal vehicular drive to link up with the north-south pedestrian pathway that bisects this development area.
3. Provision of more-detailed exterior façade materiality information that matches the standards outlined within the PUD Conceptual Plan.
4. Verification that all exterior façade glazing contains a minimum 50% transmittance factor and a reflectance factor of no more than .25.

Part C) Staff recommends approval of the proposed PUD Final Development Plan "Gray's Station Plat 5 Townhomes and Clubhouse," subject to compliance with all administrative review comments, plus the following conditions:

1. Provision of a sidewalk connection from the clubhouse's outdoor recreation area/dog park that crosses a private drive to link up with a proposed courtyard space in between lots "40" and "41."
2. Verification that any proposed rooftop mechanical equipment will be screened from view from all sides.



3. Provision of additional information regarding mechanical equipment and associated screening (both ground- and roof-mounted) for the clubhouse building.
4. Provision of more-detailed materiality information for the clubhouse building that matches the standards outlined within the PUD Conceptual Plan.
5. Verification that all exterior façade glazing contains a minimum 50% transmittance factor and a reflectance factor of no more than .25.

## STAFF REPORT TO THE PLANNING COMMISSION

### I. GENERAL INFORMATION

1. **Purpose of Request:** The applicant is requesting an amendment to the Gray's Station PUD Conceptual Plan, which would alter the alignment of Southwest 13<sup>th</sup> Street between Tuttle Street and Grays Parkway. Further, the applicant is proposing to amend the PUD's 'Notes' to remove the requirement that "All Final Development Plans are subject to review and approval by the Plan & Zoning Commission and the City Council," which would instead allow administrative review in lieu.

Additionally, the developer is seeking to subdivide the 'Gray's Station Plat 5' area within the PUD to allow the development and construction of 65 one-household residential lots containing row homes, a clubhouse amenity building, a lot containing two condo buildings with 84 residential units, and two outlots.

2. **Size of Site:** Gray's Station PUD – 84.4 acres; Plat 5 Area – 11.1 acres.
3. **Existing Zoning (site):** Gray's Station PUD, Planned Unit Development District.
4. **Existing Land Use (site):** The area is currently undeveloped land.
5. **Adjacent Land Use and Zoning:**

**North** – "DX2"; Uses are Tuttle Street, open space, and vacant industrial buildings.

**South** – "Gray's Station PUD" & "F"; Uses are open space, a city stormwater management facility, the Raccoon River, and Gray's Lake Park.

**East** – "Gray's Station PUD"; Uses are townhomes and multiple-household dwelling units.

**West** – "Gray's Station PUD" & "DX2"; Uses are industrial warehousing and undeveloped land.

6. **General Neighborhood/Area Land Uses:** The subject property is located generally to the south of West Martin Luther King, Jr. Parkway and to the west of Southwest 11<sup>th</sup> Street in the southern fringes of Downtown. The site's vicinity consists of mixed-use and medium-density residential development, parkland, and undeveloped parcels. The Raccoon River flanks the southern boundary of the site. Gray's Lake Park lies further south of the river.



- 7. Applicable Recognized Neighborhood(s):** The subject property is located within the Downtown Des Moines Neighborhood Association. All neighborhood associations were notified of the public hearing by emailing of the Preliminary Agenda on January 13, 2023, and by emailing of the Final Agenda on January 27, 2023. Additionally, separate notifications of the hearing for this specific item were mailed on January 13, 2023 (20 days prior to the public hearing) and January 23, 2023 (10 days prior to the public hearing) to the Downtown Des Moines Neighborhood Association and to the primary titleholder on file with the Polk County Assessor for each property within 250 feet of the site.

All agendas and notices are mailed to the primary contact(s) designated by the recognized neighborhood association to the City of Des Moines Neighborhood Services Department on the date of the mailing. The Sheridan Gardens Neighborhood does not have a mailing address on file. The Brook Run Neighborhood Association mailings were sent to Brandon Brown, 120 Southwest 5<sup>th</sup> Street, Unit 101, Des Moines, IA 50309.

The applicant can provide a summary of the neighborhood outreach at the public hearing.

- 8. Relevant Zoning History:** The subject property was rezoned to “PUD” on August 14, 2017, at which time a general “PUD” Conceptual Plan was adopted.

- 9. PlanDSM: Creating Our Tomorrow Plan Land Use Plan Designation:** Downtown Mixed Use and Neighborhood Mixed Use.

- 10. Applicable Regulations:** Taking into consideration the criteria set forth in Chapter 18B of the Iowa Code, the Commission reviews all proposals to amend zoning boundaries or regulations within the City of Des Moines. Such amendments must be in conformance with the comprehensive plan for the City and designed to meet the criteria in 414.3 of the Iowa Code. The Commission may make recommendations to the City Council on conditions to be made in addition to the existing regulations so long as the subject property owner agrees to them in writing. The recommendation of the Commission will be forwarded to the City Council.

The application, accompanying evidence and “PUD” Conceptual Plan shall be considered by the Plan and Zoning commission at a public hearing. The Commission shall review the conformity of the proposed development with the standards of the City Code and with recognized principles of civic design, land use planning, and landscape architecture. After the hearing, the Commission may vote to recommend either approval or disapproval of the amended “PUD” Conceptual Plan as submitted, or to recommend that the developer amend the plan or request to preserve the intent and purpose of this chapter to promote public health, safety, morals and general welfare. The recommendations of the Commission shall be referred to the City Council.

Taking into consideration the criteria set forth in Chapter 18B of the Iowa Code, the Commission shall determine if such Preliminary Plat conforms to the standards and requirements outlined in Chapter 354 of the Iowa Code, and the City Subdivision Ordinance and shall approve, conditionally approve or reject such Plat within 45 days after the date of submission to the City Permit and Development Center. Unless the applicant agrees in writing to an extension of time, the Preliminary Plat shall be



deemed approved if the Commission does not act within such 45-day period. The Commission's action for approval or conditional approval shall be null and void unless the Final Plat is submitted to the City Permit and Development Center within 270 days after the date of such action; provided, however, that the Permit and Development Administrator may grant, upon written request of the applicant, up to a 90-day extension for submittal of the Final Plat to the City Permit and Development Center.

Taking into consideration the criteria set forth in Chapter 18B of the Iowa Code, the Commission shall determine if such Preliminary Plat conforms to the standards and requirements outlined in Chapter 354 of the Iowa Code, and the City Subdivision Ordinance and shall approve, conditionally approve or reject such Plat within 45 days after the date of submission to the City Permit and Development Center. Unless the applicant agrees in writing to an extension of time, the Preliminary Plat shall be deemed approved if the Commission does not act within such 45-day period. The Commission's action for approval or conditional approval shall be null and void unless the Final Plat is submitted to the City Permit and Development Center within 270 days after the date of such action; provided, however, that the Permit and Development Administrator may grant, upon written request of the applicant, up to a 90-day extension for submittal of the Final Plat to the City Permit and Development Center.

Pursuant to Section 135-9.1.1.B of the Planning and Design Ordinance, the site plan review requirements of Chapter 135 are designed to ensure the orderly and harmonious development of property in a manner that shall:

- Promote the most beneficial relation between present and proposed future uses of land and the present and proposed future circulation of traffic throughout the city;
- Permit present development of property commensurate with fair and orderly planning for future development of other properties in the various areas of the city with respect to the availability and capacity, present and foreseeable, of public facilities and services. The factors to be considered in arriving at a conclusion concerning proposed present development of property shall include the following:
  - The maximum population density for the proposed development, the proposed density of use, and consideration of the effect the proposal will have on the capacity of existing water and sanitary sewer lines to the end that existing systems will not become overloaded or capacity so substantially decreased that site use will inhibit or preclude planned future development;
  - Zoning restrictions at the time of the proposal;
- The city's comprehensive plan;
- The city's plans for future construction and provision for public facilities and services; and



- The facilities and services already available to the area which will be affected by the proposed site use;
- Encourage adequate provision for surface and subsurface drainage, in order to ensure that future development and other properties in various areas of the city will not be adversely affected;
- Provide suitable screening of parking, truck loading, refuse and recycling disposal, and outdoor storage areas from adjacent residential districts;
- Encourage the preservation of canopied areas and mature trees and require mitigation for the removal of trees; and
- Consider the smart planning principles set forth in Iowa Code Chapter 18B.

## II. ADDITIONAL APPLICABLE INFORMATION

1. **Gray's Station PUD Conceptual Plan:** If the proposed amendment to the Gray's Station "PUD" is approved by the City Council, the applicant must submit to the Planning and Urban Design Administrator a revised version of the "PUD" Conceptual Plan that reflects any conditions of approval.
2. **Realignment of Southwest 13<sup>th</sup> Street:** The proposed amendment to the PUD incorporates a slight realignment of Southwest 13<sup>th</sup> Street. The initial alignment proposed for Southwest 13<sup>th</sup> Street was straight north-south between Tuttle Street and Grays Parkway. With the proposed realignment, the southern portion of the street curves slightly east to join Grays Parkway. The slight realignment would not change or impact the design of the PUD in any significant manner.
3. **Review and Approval:** Per the existing PUD Conceptual Plan, all final development plans within the PUD are currently subject to review and approval by the Plan and Zoning Commission and the City Council. The proposed amendment would eliminate the need for a Commission and Council review for future development plans and would allow administrative-only review. While an administrative review is appropriate for minor changes within the existing development plans and for row homes, a more-comprehensive review, including approval by the Plan and Zoning Commission, is necessary for larger mixed-use and multi-family residential developments. The PUD is well-established to allow developments such as row homes. However, there is some ambiguity relating to larger mixed-use and multi-family buildings which might be potentially sited in the southern portion of the PUD. Staff feels a public process is reasonably necessary to allow comprehensive review of those future development plans and other larger, mixed-use, and multiple-household residential buildings.
4. **Preliminary Plat:** The proposed preliminary plat would involve the creation and extension of public and private streets to serve the development. As an infill, Downtown site, the street layouts (both public and private) are proposed to complement the City street grid (in terms of both layout and construction) that already exists in this area. Murphy Street and Grays Parkway are proposed to extend to the west, with additional westward extensions of already-existing east-west private streets. A new segment of Southwest 13<sup>th</sup> Street is proposed to be constructed from Tuttle



Street to Grays Parkway. Additional east-west private streets are proposed in the southern portion of the development.

65 one-household lots would be created for clustered, rowhome-style development. Additionally, larger lots would be created for a neighborhood "clubhouse" building and open space, as well as a lot that would contain (2) 3-story residential buildings for a total of 84 dwelling units.

All new streets would be constructed with urban cross-sections generally consistent with the construction of streets within the Gray's Station area and overall Downtown. Public utilities (sanitary sewer, water service) are proposed to be extended both within public rights-of-way and within private streets. Public storm sewer is also proposed within the development, and the area's overall stormwater management has been contemplated and accounted for with the existing Gray's Station city stormwater facility that sits to the south of this development area.

On-street and off-street pedestrian connections are proposed that would provide linkages both within this development, as well as with the Gray's Station Trail to the south and the recently-platted area to the east. The Tuttle Street Cycle Track project is proposed to be extended through the northern border of this development area. A pedestrian "Paseo" is proposed to be extended from the east and bisect the northern half of the development.

- 5. Development Plan – "Gray's Station Telus Condos":** The Telus condos development plan consists of (2) 3-story residential buildings that would contain 84 dwelling units, as well three standalone garage buildings for motor vehicle parking.

The northern building ("Building 1") is proposed to frame the corner of Tuttle and Southwest 13<sup>th</sup> Streets, with the southern building ("Building 2") proposed to frame the corner of Murphy and Southwest 13<sup>th</sup> Streets. The aforementioned Paseo is proposed between the two buildings before intersecting with Southwest 13<sup>th</sup> Street. A motor vehicle parking area would sit behind the proposed residential buildings. A total of 137 motor vehicle parking spaces (53 uncovered, 84 in garages) are proposed. 20 bicycle parking spaces are proposed adjacent to the Paseo in between the residential buildings.

Building and site design is required to conform to the design criteria set forth in the Gray's Station PUD, specifically the criteria for "High Density Residential" buildings. The buildings are proposed to be clad in a mix of brick, concrete masonry units, fiber cement board, and architectural metal paneling in a manner consistent with the material palette stipulated within the PUD Conceptual Plan. Buildings are proposed to sit abutting street-facing lot lines, with street-facing entrances. Proposed floor plans demonstrate a stacking action amongst the units, with ground floor units opening to the street, and upper-floor units with street- or internal-facing balconies.

- 6. Development Plan – "Gray's Station Plat 5 Row Homes and Clubhouse":** The development plan for the row homes and clubhouse demonstrates 17 individual clusters of 3-5 row homes, as well as a larger area within the southern portion of the development area for the clubhouse and outdoor recreation areas. Each rowhome would sit on its own lot.



Row homes are proposed to be oriented toward either a public street or a designated pedestrian way. Rear-loading garages for motor vehicle parking are proposed for each rowhome. These garages would have space for up to 2 vehicles and are proposed to be accessed from the development's private drives that would function as alleys. Additional motor vehicle parking facilities are proposed to be either on street in a parallel fashion or off of a private drive directly abutting the clubhouse/outdoor recreation area. 20 bicycle parking spaces are demonstrated in a clumped fashion oriented around the outdoor recreation area.

The outdoor recreation area is proposed to consist of a swimming pool, dog park, pickleball courts, landscaping arrangements, and other unprogrammed open space. The clubhouse is designed as a flexible, general-use entertainment space that would open up to the pool area.

Building and individual site design is required to conform to the design criteria set forth in the Gray's Station PUD, specifically the criteria for "Low-Medium Residential" buildings. With the exception of the clubhouse, all buildings that are a part of this development plan are proposed to be at least 2 stories. Variations of 5 different rowhome "product types" are proposed to be placed in clusters throughout the area of this development plan. These product types differ in their building heights, exterior façade materials mixes, color palettes, entryway configurations, and proposed floor plans. Similar to the condo buildings, a mix of fiber cement board paneling, fiber cement board lap siding, architectural metal paneling, brick, stone, and finished concrete surfaces are proposed for the exterior of the row homes. Material placement and percentages differ by specific product type. Individual townhouse clusters are positioned between approximately 5 to 12 feet from the edges of streets or other pedestrian ways. Row homes would contain stoops/porches that would orient toward their respective streets/pedestrian ways.

## **SUMMARY OF DISCUSSION**

Nick Tarpey presented staff report and recommendation.

Chris Draper asked how large a project would need to be before it would come before the commission if the staff recommendation is adopted.

Nick Tarpey stated multi-family or mixed-use projects. Any one household residential project would be administratively reviewed.

Chris Draper asked if there are any future concerns with connectivity to broadband.

Jason Van Essen stated the question of broadband is outside the scope of this PUD. Amendments before the commission would not have an impact.

Will Page asked if the provision "to allow administrative review of future development site plans in lieu of public review by the Plan and Zoning Commission and City Council" is applicable to this project only.

Jason Van Essen stated there is an understood development pattern for the rowhomes and that staff supports reviewing those administratively. Other new project types should



have a public hearing, but staff believes that could be limited to just the Commission, eliminating the need to go before the City Council. These amendments would take effect for future projects.

Johnny Alcivar asked if there was a threshold for rowhome development being administratively reviewed.

Jason Van Essen stated if the staff recommendation is adopted, rowhome development would only be reviewed administratively.

Chris Draper asked if there were thoughts of how different a rowhome development would need to be before its reviewed publicly.

Jason Van Essen stated within the PUD, the developer has created several different designs. City staff feels comfortable reviewing proposed changes given the existing standards within the PUD and past negotiations on previous iterations.

Carol Maher asked if that would give city staff the ability to approve type 2 deign alternatives.

Jason Van Essen stated no, the development will need to match the standards within the PUD. If city staff and the developer disagreed on those standards, it would come before the commission.

Dan Drendel asked if it would be developed under the same PUD if another developer bought out future phases.

Jason Van Essen stated it is possible for another builder to follow the standards within the current PUD. If they wanted to go a different direction, amendments to the PUD would be needed to accommodate new designs.

Chris Draper asked if the existing powerlines would be undergrounded.

Jason Van Essen stated there are a few major lines that will remain.

Caleb Smith, Hubbell Realty Company, 6900 Westown Parkway, WDM stated after a simple restriping plan for the LINC, they thought it would make sense to suggest removing the clause that requires review by the Plan and Zoning Commission and City Council. City staff has come forward with a reasonable approach given the size, style and scope of the townhomes have been well established. It does make sense for the commission to review multi-family projects due to the variety they will have. The overhead utility line is a massive transmission line that would be a multi-million dollar project and something Mid-American Energy isn't supportive of. These plans do include utility easement corridors that will provide all units with telecom communication.

Will Page asked if they could explain their thoughts of darker colors being more appropriate for urban settings.



Caleb Smith stated they learned a lot during the first phase of this development and noticed those darker colors sold better. They also have a network of builders across the country that allows them to see these types of trends.

Carol Maher asked if these are all rental units.

Calen Smith stated there are no rental properties in this phase of the project.

Carol Maher asked about parking around the multi-family development.

Ryan Hardisty, Civil Design Advantage, 4121 NW Urbandale Drive, Urbandale, IA stated the TELUS project has 84 total units with 137 parking spaces. There are 84 total garages with a few being detached.

Carol Maher asked why the parking ratio is more than 1 to 1.

Ryan Hardisty stated given the success of the LINC project, parking issues started to occur once the building was fully rented. Based on those issues, they revamped parking for TELUS.

Carol Maher asked if a parking space is included with purchase of a unit.

Michael Bialas, Hubbell Realty Company, 6900 Westown Parkway, WDM stated the parking spaces and garages will be sold separately. If they are not sold, those spaces would be turned over to the association.

Carol Maher asked if they would offer indoor bike storage.

Michael Bialas stated that is something they are exploring. If all the garages are not sold, they would likely convert one to bike storage.

Carol Maher stated they are only focused on how to fit more cars, instead of encouraging other means of transportation. They should be more forward thinking and include adequate bike parking and EV charging stations.

Emily Webb stated she understands the need for more parking. She lives in an urban rowhouse setting and just having a garage makes it challenging for others to visit you.

Justyn Lewis stated there are parking issues around this area, but he would like to see this development promote bicycling and walkability.

Ryan Hardisty stated they are providing 20 bike parking spaces at the main entrance.

Justyn Lewis asked if that's inside the building.

Ryan Hardisty stated they're exterior, located off SW 13<sup>th</sup> Street.

Will Page asked if they agree with staff recommendations.

Caleb Smith stated yes.

## **CHAIRPERSON OPENED THE PUBLIC HEARING**

Troy Hall, 2530 University Avenue stated he is generally supportive of this development and would like to continue seeing densification of downtown. He would encourage the developer to embrace architectural diversity and encourage different means of transportation.

## **CHAIRPERSON CLOSED THE PUBLIC HEARING**

Chris Draper stated he hopes we aren't straying away from the underlying strategies within tax abatement that would reduce parking ratios.

## **COMMISSION ACTION:**

Will Page made a motion for:

Part A) Approval of the request to amend the Gray's Station PUD Conceptual Plan to update the realignment of Southwest 13th Street.

Furthermore, Staff recommends limited approval of the request to amend the Gray's Station PUD Conceptual Plan to revise the 'Notes' section to allow administrative review of future development site plans in lieu of public review by the Plan and Zoning Commission and City Council, as follows:

- Any final development plan for a new row home type development or a minor revision to an existing development is subject to administrative (by staff) review and approval.
- Any final development plan for a mixed-use building or a multiple-household residential building is subject to review and approval by the Plan and Zoning Commission, with any appeal of that decision considered by the City Council.

Part B) Approval of the proposed PUD Final Development Plan "Gray's Station Telus Condos," subject to compliance with all administrative review comments, plus the following conditions:

1. Provision of a "band" of architectural concrete masonry unit materials from grade up to 2' for the standalone garages that matches the concrete masonry units proposed for the condo buildings.
2. Provision of a sidewalk connection from the southeastern corner of "Building 2" that crosses the internal vehicular drive to link up with the north-south pedestrian pathway that bisects this development area.
3. Provision of more-detailed exterior façade materiality information that matches the standards outlined within the PUD Conceptual Plan.
4. Verification that all exterior façade glazing contains a minimum 50% transmittance factor and a reflectance factor of no more than .25.



Part C) Approval of the proposed PUD Final Development Plan "Gray's Station Plat 5 Townhomes and Clubhouse," subject to compliance with all administrative review comments, plus the following conditions:

1. Provision of a sidewalk connection from the clubhouse's outdoor recreation area/dog park that crosses a private drive to link up with a proposed courtyard space in between lots "40" and "41."
2. Verification that any proposed rooftop mechanical equipment will be screened from view from all sides.
3. Provision of additional information regarding mechanical equipment and associated screening (both ground- and roof-mounted) for the clubhouse building.
4. Provision of more-detailed materiality information for the clubhouse building that matches the standards outlined within the PUD Conceptual Plan.
5. Verification that all exterior façade glazing contains a minimum 50% transmittance factor and a reflectance factor of no more than .25.

Motion passed: 8-1-1

Respectfully submitted,



Jason Van Essen, AICP  
Planning & Urban Design Administrator

JMV:tjh





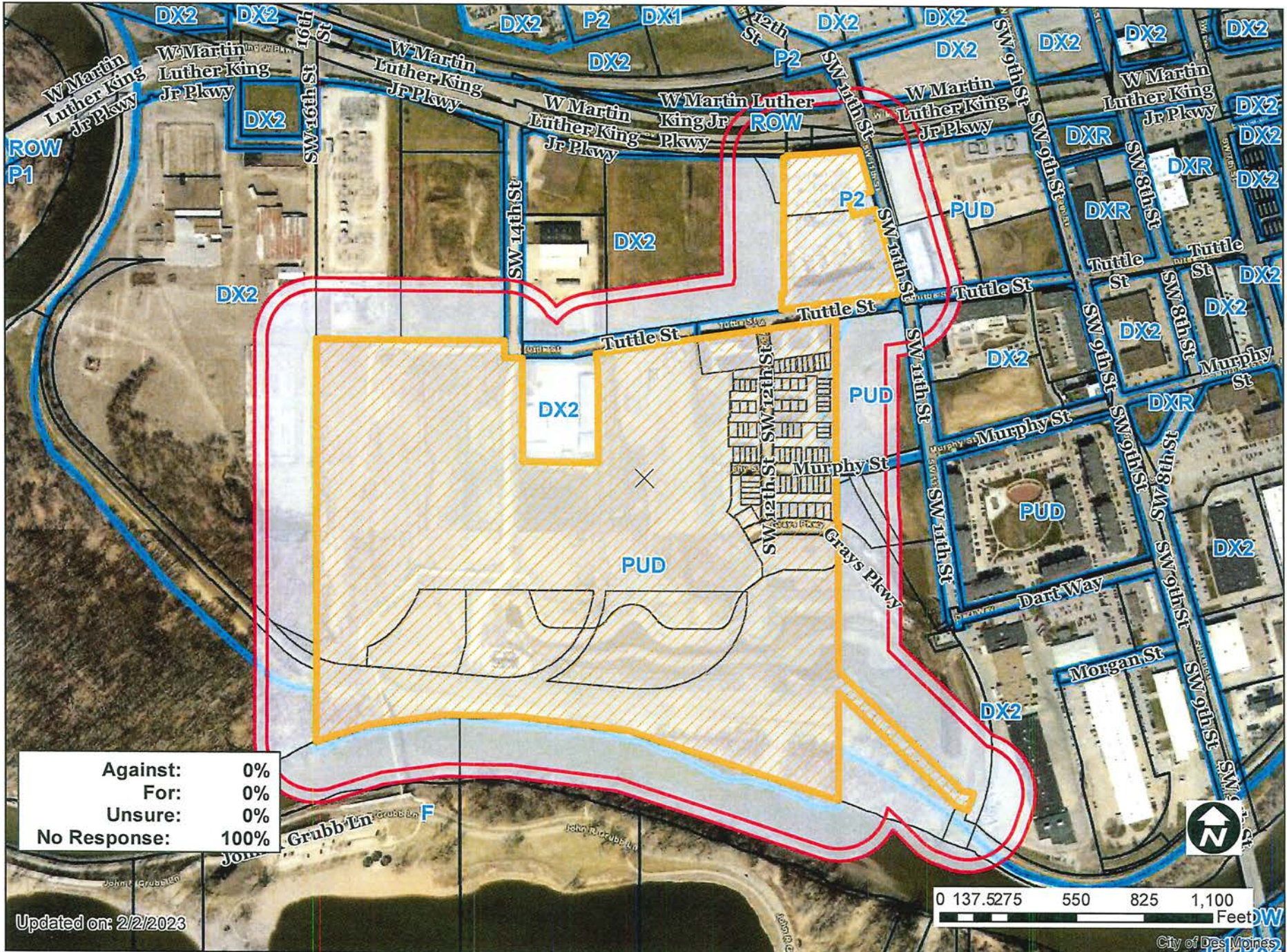
Updated on: 1/12/2023

0 137.5275 550 825 1,100 Feet

1 inch = 517 feet

23-0331



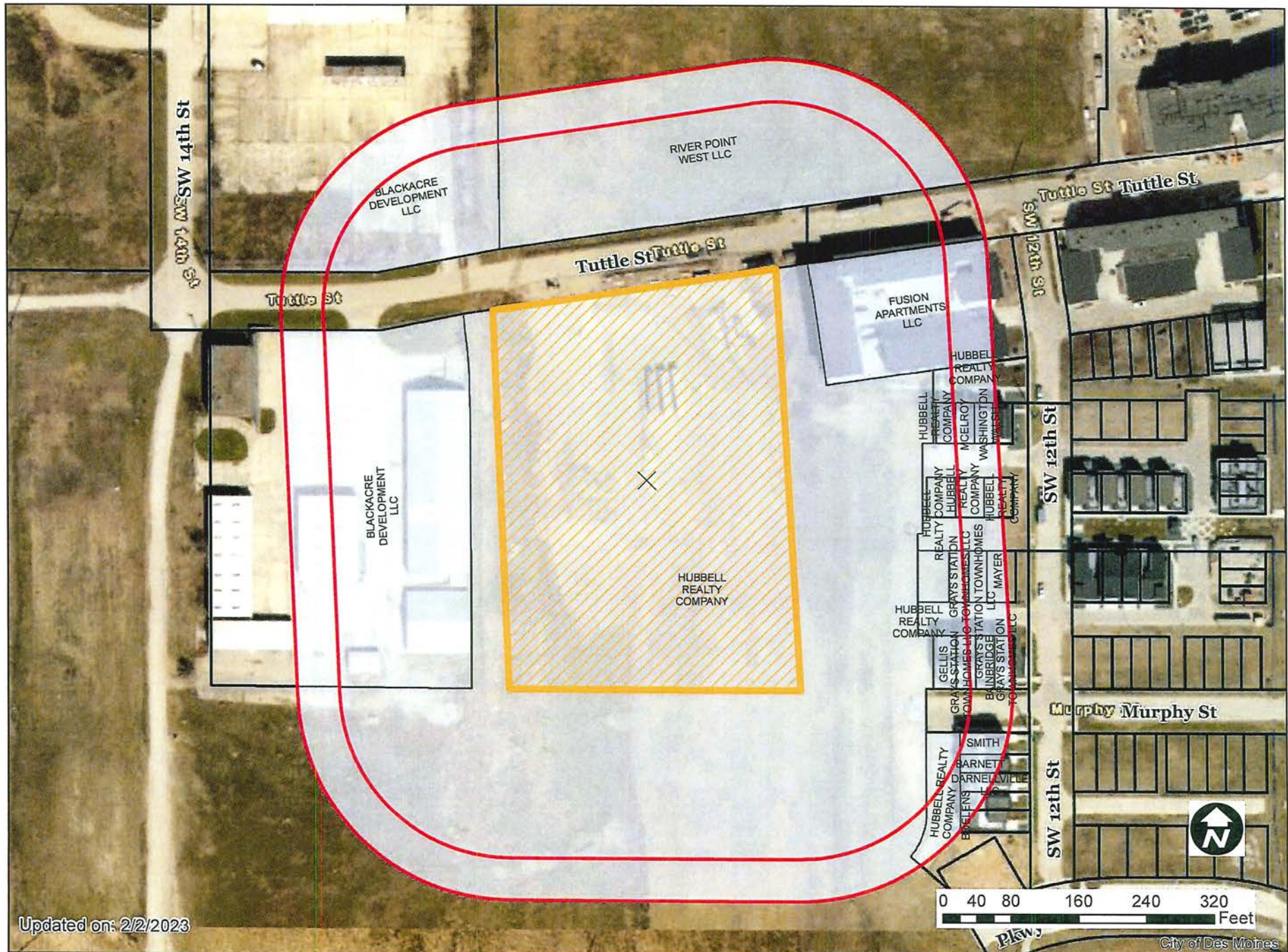


Against:	0%
For:	0%
Unsure:	0%
No Response:	100%

Updated on: 2/2/2023

1 inch = 517 feet

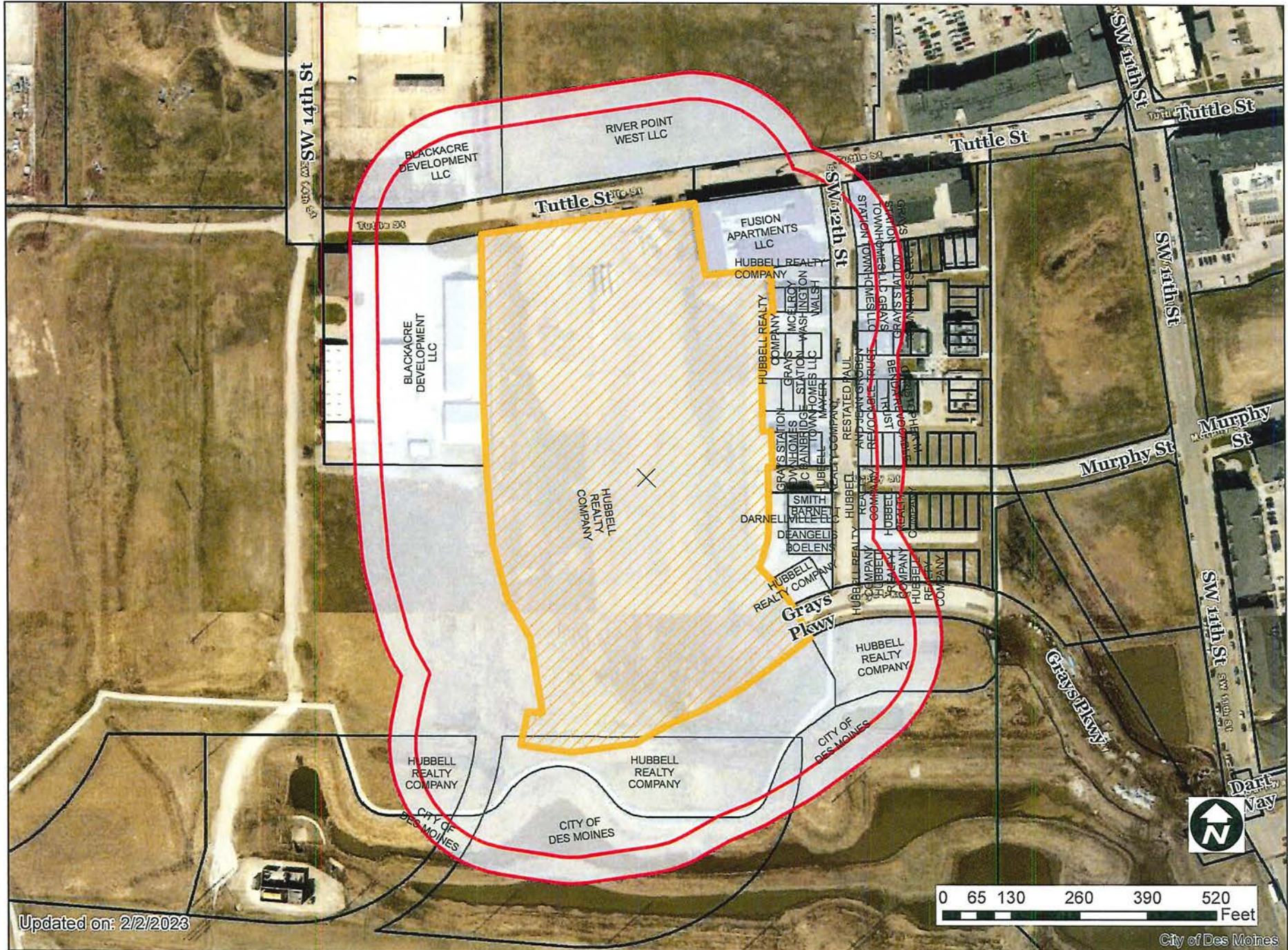




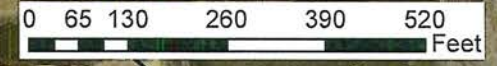
Updated on: 2/2/2023

1 inch = 151 feet





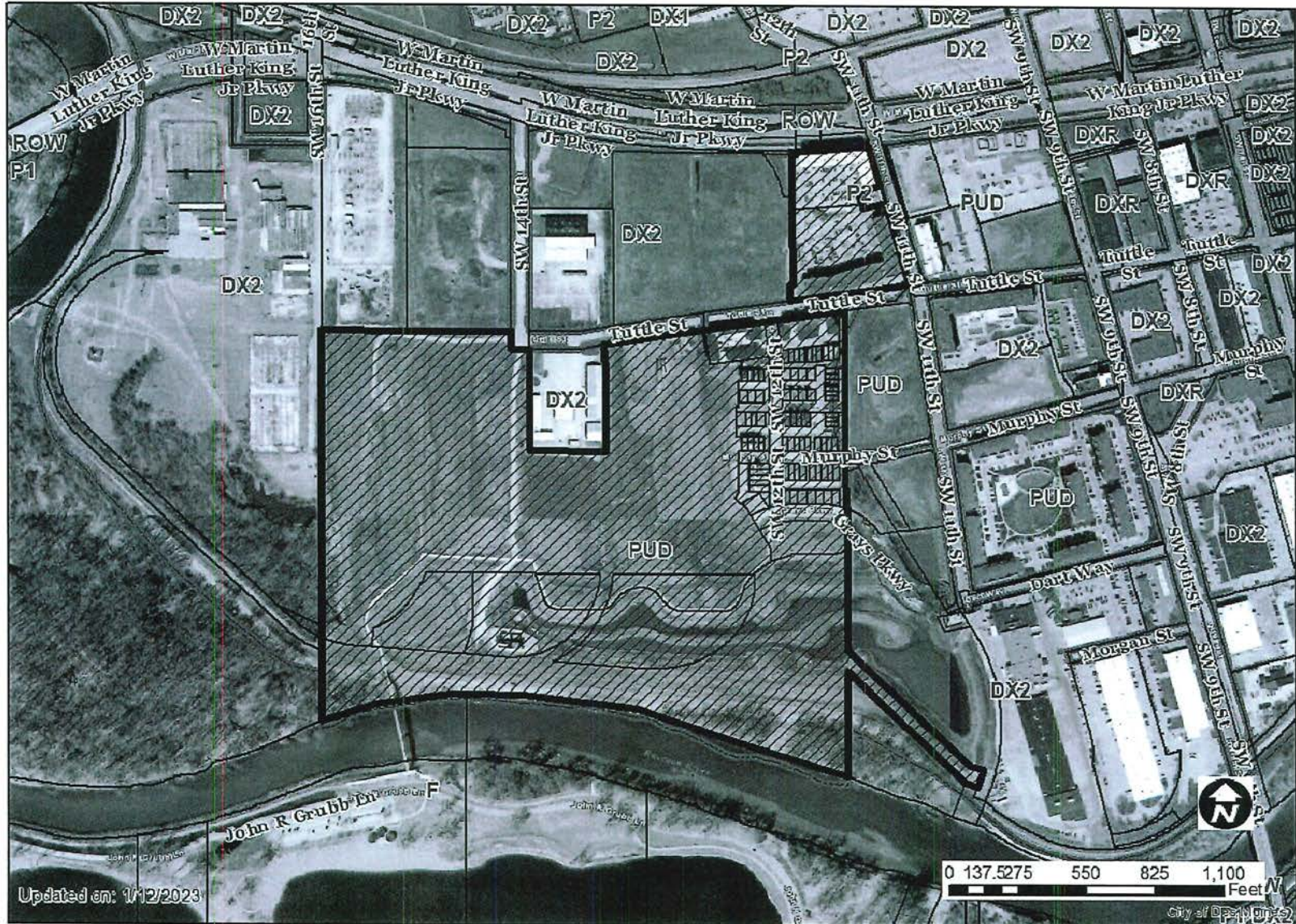
Updated on: 2/2/2023



City of Des Moines

1 inch = 243 feet





1 inch = 517 feet



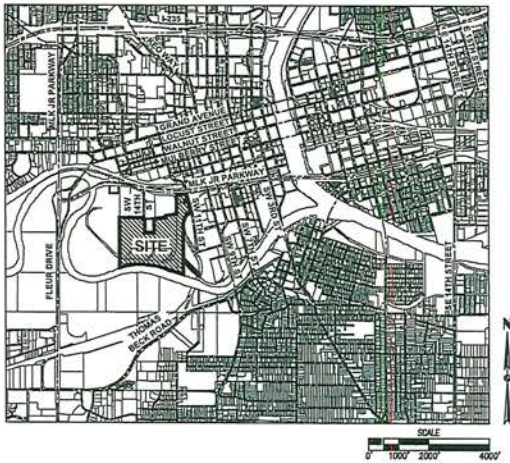




# CONCEPTUAL PLAN, FIRST AMENDMENT GRAY'S STATION

A PLANNED UNIT DEVELOPMENT IN THE CITY OF DES MOINES, COUNTY OF POLK, STATE OF IOWA

VICINITY MAP (1"=2000')



**DEVELOPER**

HUBBELL REALTY COMPANY  
CONTACT: JOE PIETRUSZEWSKI  
3690 WESTON PARKWAY  
WEST DES MOINES, IOWA 50266  
PH. (515) 243-3228

**OWNER**

NORFOLK SOUTHERN RY  
TAX DEPT  
3 COMMERCIAL PLACE, SUITE 29  
NORFOLK, VIRGINIA 23510

CITY OF DES MOINES  
400 ROBERT D RAY DRIVE  
DES MOINES, IOWA 50309

**SHEET INDEX**

- 1 PLANNED UNIT DEVELOPMENT (COVER SHEET)
- 2 PLANNED UNIT DEVELOPMENT (BASE MAP)

**CITY PRE-APPLICATION MEETING DATE**

MAY 2, 2017

**ZONING**

- EXISTING:**
- C-3B CENTRAL BUSINESS MIXED USE DISTRICT
  - FW FLOODWAY DISTRICT
  - DOWNTOWN OVERLAY DISTRICT
  - GAMBLING GAMES PROHIBITION OVERLAY DISTRICT

**PROPOSED:**

- P.U.D.

**PLAN DSM LAND USE**

DOWNTOWN MIXED USE

**ENGINEER**

CIVIL DESIGN ADVANTAGE, LLC  
CONTACT: RYAN HARDISTY  
4121 NW URBANDALE DRIVE  
URBANDALE, IOWA 50222  
PH. (515) 369-4400

**NEIGHBORHOOD MEETINGS & MEETING DATE**

The Gray's Station site is located such that it does not have many citizen neighbors to speak of. Hubbell Realty Company determined that the best way to solicit input related to the potential vision and development of the Gray's Station neighborhood was to invite community leaders to participate in a Steering Committee for the neighborhood. This Steering Committee included members of City Staff, members of the Urban Land Institute Iowa District Council, downtown neighborhood associations, a City Council member, and other individuals whose input would assist Hubbell Realty Company to create a unique new neighborhood for Des Moines. This group met twice in the Summer of 2016 to engage in making the vision for the neighborhood and to review early concepts for Gray's Station. Their participation has influenced the resulting Conceptual Development Plan for Gray's Station.

Steering Committee Meeting #1 - June 14, 2016

- Location: RDC Planning & Design 301 and Avenue Des Moines, Iowa 50309
- Time: 2 PM - 4 PM

Steering Committee Meeting #2 - August 15, 2016

- Location: RDC Planning & Design 301 and Avenue Des Moines, Iowa 50309
- Time: 2 PM - 4 PM

Neighborhood Open House - August 15, 2016

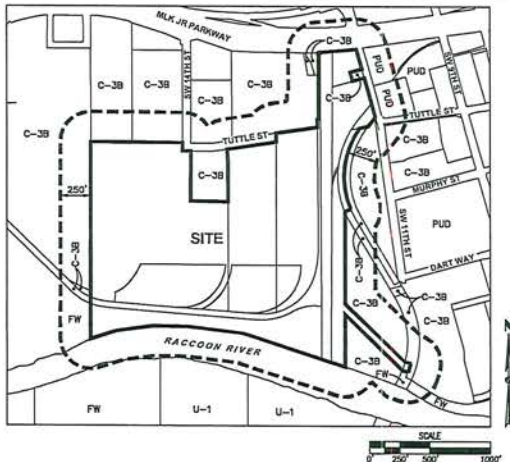
- Location: Cityville on 6th/ Common Room 550 SW 6th/ ST Des Moines, Iowa 50309
- Time: 5:30 PM - 8:30 PM

**P.U.D. PROPERTY DESCRIPTION**

OUTLOTS Y AND Z AND A PART OF LOTS B, 7 AND 8, CENTRAL DES MOINES INDUSTRIAL PARK, AN OFFICIAL PLAT AND PART OF LOTS B1, B2, B5 AND B6 IN FACTORY ADDITION, AN OFFICIAL PLAT ALL BEING IN THE CITY OF DES MOINES, POLK COUNTY, IOWA AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF SAID LOT 9, CENTRAL DES MOINES INDUSTRIAL PARK HAVING A STATE PLANE IOWA SOUTH COORDINATE VALUE OF NORTHERLY 570161.83 AND EASTING 1602813.07; THENCE SOUTH 89° (DEGREES) 40' (MINUTES) 50" (SECONDS) EAST ALONG THE NORTHERLY LINE OF SAID LOT 9, A DISTANCE OF 338.63 FEET TO THE SOUTHWEST CORNER OF LOT 2, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE SOUTH 69°46'52" EAST CONTINUING ALONG SAID NORTHERLY LINE, 419.55 FEET TO THE SOUTHEAST CORNER OF SAID LOT 2 ALSO BEING A POINT ON THE WEST RIGHT OF WAY LINE OF SW 14TH STREET; THENCE SOUTH 00°01'02" EAST CONTINUING ALONG SAID NORTHERLY LINE, 69.98 FEET TO THE SOUTHWEST CORNER OF STREET LOT 8, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE SOUTH 89°35'21" EAST CONTINUING ALONG SAID NORTHERLY LINE, 70.00 FEET TO THE NORTHWEST CORNER OF LOT 6, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE SOUTH 00°08'09" WEST CONTINUING ALONG SAID NORTHERLY LINE, 423.70 FEET TO THE SOUTHWEST CORNER OF SAID LOT 8; THENCE SOUTH 89°19'16" EAST CONTINUING ALONG SAID NORTHERLY LINE, 307.20 FEET TO THE SOUTHEAST CORNER OF SAID LOT 8, ALSO BEING THE NORTHEAST CORNER OF SAID LOT 9; THENCE NORTH 00°09'11" WEST ALONG THE WESTERLY LINE OF SAID LOT 7, CENTRAL DES MOINES INDUSTRIAL PARK, 333.39 FEET; THENCE NORTHERLY CONTINUING ALONG SAID WESTERLY LINE, 15.78 FEET TO THE NORTHWEST CORNER OF SAID LOT 7 ALSO BEING THE SOUTH RIGHT OF WAY LINE OF TUTTLE STREET; THENCE NORTH 81°10'49" EAST ALONG THE NORTH LINE OF SAID LOT 7, A DISTANCE OF 411.78 FEET TO THE NORTHEAST CORNER OF SAID LOT 7; THENCE NORTH 00°04'48" WEST ALONG THE WEST LINE OF SAID LOT 6, CENTRAL DES MOINES INDUSTRIAL PARK, 72.11 FEET TO THE NORTHWEST CORNER OF SAID LOT 6; THENCE NORTH 81°52'47" EAST ALONG THE NORTH LINE OF SAID LOT 6, A DISTANCE OF 347.62 FEET TO THE SOUTHEAST CORNER OF LOT 4, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE NORTH 00°31'40" WEST ALONG THE WESTERLY LINE OF SAID OUTLOT Y, CENTRAL DES MOINES INDUSTRIAL PARK, 63.43 FEET; THENCE NORTH 87°29'10" EAST CONTINUING ALONG SAID WESTERLY LINE, 10.37 FEET; THENCE NORTH 00°24'49" WEST CONTINUING ALONG SAID WESTERLY LINE, 554.59 FEET TO THE NORTHEAST CORNER OF LOT 5, SAID CENTRAL DES MOINES INDUSTRIAL PARK AND HAVING A STATE PLANE IOWA SOUTH COORDINATE VALUE OF NORTHERLY 575907.16 AND EASTING 1604688.46; THENCE SOUTH 89°04'27" EAST ALONG THE NORTH LINE OF SAID OUTLOT Y, 15.36 FEET TO THE NORTHEAST CORNER OF SAID OUTLOT Y; THENCE NORTH 00°21'12" WEST ALONG THE WEST LINE OF SAID LOT 85, FACTORY ADDITION, 8.59 FEET TO THE SOUTHERLY RIGHT OF WAY LINE OF MARTIN LUTHER KING JR PARKWAY; THENCE NORTH 89°10'40" EAST ALONG SAID SOUTHERLY RIGHT OF WAY LINE, 100.23 FEET; THENCE NORTH 84°40'04" EAST CONTINUING ALONG SAID SOUTHERLY RIGHT OF WAY LINE, 184.44 FEET TO THE WESTERLY RIGHT OF WAY LINE OF SW 11TH STREET; THENCE SOUTH 15°27'22" EAST ALONG SAID WESTERLY RIGHT OF WAY LINE, 173.21 FEET; THENCE SOUTH 71°42'29" WEST, 98.77 FEET; THENCE SOUTH 15°25'34" EAST, 75.29 FEET; THENCE NORTH 74°32'27" EAST, 98.61 FEET TO THE WESTERLY RIGHT OF WAY LINE; THENCE SOUTH 15°27'22" EAST ALONG SAID WESTERLY RIGHT OF WAY LINE, 344.37 FEET TO A CORNER ON THE LINE; THENCE SOUTH 38°13'02" WEST ALONG SAID EASTERLY LINE, 87.08 FEET; THENCE EASTERLY LINE OF SAID LOT 81; THENCE SOUTH 38°13'02" WEST ALONG SAID EASTERLY LINE, 87.08 FEET; THENCE SOUTH 79°27'19" WEST, 73.58 FEET TO THE EASTERLY LINE OF THE WEST HALF OF SAID LOT 81; THENCE SOUTHERLY ALONG SAID EASTERLY LINE AND A CURVE CONCAVE EASTERLY WHOSE RADIUS IS 718.25 FEET, WHOSE ARC LENGTH IS 51.41 712.02 FEET AND WHOSE CHORD BEARS SOUTH 00°20'51" WEST, 683.08 FEET; THENCE SOUTH 67°48'14" WEST, 51.41 FEET TO A POINT ON THE EAST LINE OF THE WEST 100 FEET OF SAID LOT 80, FACTORY ADDITION; THENCE SOUTH 00°01'40" EAST ALONG SAID EAST LINE, 761.72 FEET; THENCE SOUTH 45°06'19" EAST, 692.09 FEET TO THE WESTERLY LINE OF SAID LOT 81; THENCE SOUTH 76°02'29" EAST, 20.00 FEET TO SAID EASTERLY LINE OF THE WEST HALF OF LOT 81, HAVING A STATE PLANE IOWA SOUTH COORDINATE VALUE OF NORTHERLY 573317.87 AND EASTING 1605481.84; THENCE SOUTHERLY ALONG SAID EASTERLY LINE AND A CURVE CONCAVE WESTERLY WHOSE RADIUS IS 740.85 FEET, WHOSE ARC LENGTH IS 78.04 FEET AND WHOSE CHORD BEARS SOUTH 17°00'50" WEST, 76.01 FEET; THENCE NORTH 79°20'25" WEST, 50.00 FEET TO SAID WESTERLY LINE OF LOT 81; THENCE NORTH 45°36'19" WEST, 683.55 FEET TO SAID EAST LINE OF THE WEST 100 FEET OF LOT 80; THENCE SOUTH 00°01'40" EAST ALONG SAID EAST LINE, 426.00 FEET TO THE NORTH BANK AND MEANDER LINE OF THE RACCOON RIVER; THENCE NORTH 71°44'57" WEST AND ALONG SAID MEANDER LINE AND THE SOUTHERLY LINE OF SAID OUTLOT Z, CENTRAL DES MOINES INDUSTRIAL PARK, 888.50 FEET; THENCE NORTH 83°01'49" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 463.82 FEET; THENCE NORTH 89°16'53" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 291.24 FEET; THENCE SOUTH 82°06'06" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 296.82 FEET; THENCE SOUTH 72°02'02" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 228.32 FEET TO THE SOUTHWEST CORNER OF SAID OUTLOT Z; THENCE NORTH 00°03'17" EAST ALONG THE WEST LINE OF SAID CENTRAL DES MOINES INDUSTRIAL PARK, 1847.03 FEET TO THE POINT OF BEGINNING AND CONTAINING 63.73 ACRES (3,447,186 SQUARE FEET) AND IS SUBJECT TO ANY AND ALL EASEMENTS OF RECORD.

ZONING OF ADJACENT PROPERTIES (1"=500')



**NOTES**

- THIS SITE IS SUBJECT TO THE TREE REMOVAL AND MITIGATION ORDINANCE OF THE CITY CODE. HUBBELL REALTY COMPANY IS ENTERING INTO A DEVELOPMENT AGREEMENT WITH THE CITY OF DES MOINES THAT OUTLINE A CUSTOM APPROACH TO TREE REMOVAL AND MITIGATION FOR THE GRAY'S STATION PLANNED UNIT DEVELOPMENT. NO TREE SURVEY OR CANOPY AREA METHOD INFORMATION WAS SUBMITTED FOR REVIEW. THIS INFORMATION WILL BE PROVIDED WITH THE FINAL DEVELOPMENT PLAN.
- ALL REQUIRED STORM-WATER MANAGEMENT CAN BE ACHIEVED UNDER THE PROPOSED CONCEPT. (NO DESIGN SUBMITTED)
- ANY DEVELOPMENT PLAN REVIEW IS SUBJECT TO PROVIDING A TRAFFIC IMPACT ANALYSIS REVIEWED BY THE CITY TRAFFIC ENGINEER.
- THE SITE SHALL BE PLATTED TO PRESERVE PUBLIC TRANSPORTATION CORRIDORS IN GENERAL CONFORMANCE WITH THE MASTER PLAN.
- ALL SANITARY SEWER CONNECTIONS ARE AVAILABLE TO SERVICE THE PROPOSED CONCEPT.
- NO LESS THAN SEVENTY-FIVE PERCENT (75%) OF THE LOW-MEDIUM DENSITY RESIDENTIAL UNITS SHALL HAVE A MINIMUM OF 2 STORIES.
- NO MORE THAN TWENTY-FIVE PERCENT (25%) OF THE LOW-MEDIUM DENSITY RESIDENTIAL UNITS SHALL HAVE A MINIMUM HEIGHT OF 1 STORY.
- NO MORE THAN TWO 1-STORY LOW-MEDIUM DENSITY RESIDENTIAL UNITS SHALL BE CONSTRUCTED ON ADJOINING SITES.
- THE 75%-25% MINIMUM HEIGHT RATIO FOR THE LOW-MEDIUM DENSITY RESIDENTIAL UNITS SHOULD BE ACHIEVED WITH EACH PHASE OF THE PUD PROJECT. SOME VARIATION CAN BE CONSIDERED WITH EACH DEVELOPMENT PLAN.
- AN EMPHASIS ON TALLER BUILDING HEIGHTS SHALL BE PROVIDED FOR LOW-MEDIUM DENSITY RESIDENTIAL UNITS THAT FRONT PRIMARY STREETS.

**PUD CONCEPTUAL PLAN APPROVAL**

REVIEWED BY THE PLAN & ZONING COMMISSION ON JULY 6, 2017, AND JULY 20, 2017.

REVIEWED BY THE CITY COUNCIL ON JULY 24, 2017 (ROLL CALL NUMBER 17-1295), AND AUGUST 2, 2017 (ROLL CALL NUMBER 17-1307).

APPROVED BY THE CITY COUNCIL ON AUGUST 14, 2017, BY ROLL CALL NUMBER 17-1407, ORDINANCE NO. 18,500.

IN ACCORDANCE WITH SECTION 134-895 OF THE DES MOINES CODE, AS AMENDED.

DATE: \_\_\_\_\_ PLANNING DIRECTOR

I HEREBY CERTIFY THAT THIS ENGINEERING DOCUMENT WAS PREPARED BY ME OR UNDER MY DIRECT PERSONAL SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF IOWA.

RYAN A. HARDISTY, P.E. DATE: \_\_\_\_\_

20811

MY LICENSE RENEVAL DATE IS DECEMBER 31, 2023  
PAGES OR SHEETS COVERED BY THIS SEAL:  
SHEETS 1 & 6

DATE: \_\_\_\_\_

PREPARED BY: \_\_\_\_\_

TECH: \_\_\_\_\_

ENGINEER: \_\_\_\_\_

4121 NW URBANDALE DRIVE  
URBANDALE, IOWA 50222  
PHONE: (515) 369-4400 FAX: (515) 369-4410

**CIVIL DESIGN ADVANTAGE**

**GRAY'S STATION**  
PLANNED UNIT DEVELOPMENT (COVER SHEET)

DES MOINES, IOWA

1 / 2  
2202.113



# Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa Sheet 2A of 20



## DEVELOPMENT NARRATIVE:

The vision for Gray's Station is that of a vibrant and vital neighborhood where people enjoy a unique and essential combination of urban and natural amenities every day. With a focus on wellness the Gray's Station neighborhood will be built on a green framework that promotes the safe movement of pedestrians and bicyclists over the efficient movement of motorists. Physical and social connection among all people is fostered through the integration of lush greenways, ample neighborhood parks, and intimate passages throughout the neighborhood.

Long-time residents and those passing through on their way to Gray's Lake or Downtown Des Moines will stroll and bike along a naturalized detention area that has been engineered to clean and cool the stormwater that falls within the watershed that encompasses the Western Gateway area of Downtown and the Neighborhood. While this feature will be skillfully designed, most of those who linger along its edge and enjoy the summer evening song of the wealth of wildlife who call it home will believe it is a wetland that Gray's Station was delicately built around.

Gray's Station will be known as the neighborhood with the best views in town. The golden dome of the Iowa State Capitol in the morning, the verdant wetland, Raccoon River, and Gray's Lake throughout the day, and the striking Downtown Skyline at night will all contribute to this reputation. Whether walking on foot, riding on a bike, in a bus, or in a car each route for mobility has been laid out to provide optimal aesthetic views to points of interest that are both urban and natural. Whether in your first floor living room or gazing out of your home many stories above the plaza below there will always be something to catch your eye.

The abundance of walking paths, sidewalks, and bike facilities within the neighborhood combined with the strong connections to the Downtown and Gray's Lake will ensure a constant flow of people on foot and bike. This stream of children, women, and men will ensure interactions between people of all kinds and that is as it should be. These interactions mixed with the many positive attributes of the neighborhood will imbue Gray's Station with the sense of meaning necessary to solidify it as the Place for people who want to live well in Des Moines.

## LAND DEDICATION & ANTICIPATED LONG TERM MAINTENANCE:

- Enhanced Stormwater Basin
  - o Dedicated to City of Des Moines;
  - o Maintained by City of Des Moines.
- Raccoon River Levy and Shoreline
  - o Dedicated to City of Des Moines;
  - o Maintained by City of Des Moines.
- Pedestrian Bridge Landing Area
  - o Dedicated to City of Des Moines;
  - o Maintained by City of Des Moines.
- Pedestrian Bridge Landing Plaza
  - o Maintained by Commercial Association.
- Neighborhood Parks
  - o Maintained by Home Owner's Association
  - o Neighborhood Greenways
  - o Maintained by Home Owner's Association.

PLAN NARRATIVE & DESIGN GUIDELINES  
OCTOBER 19, 2022

## DESIGN GUIDELINES

- I. Overall Neighborhood Character
  - A. Development shall establish a new "low-rise" mixed use urban neighborhood including medium density residential, high density residential, and vertically mixed residential uses. The area east to the bridge landing and east of the intersection of 5th St and Martin Luther King Parkway shall accommodate residential and commercial uses.
  - B. Development shall create a sense of neighborhood and allow for flexibility in use to the downtown.
  - C. The neighborhood shall connect to a pedestrian bridge and landing point to Gray's Lake.
  - D. The development shall utilize methods to minimize the impact on existing infrastructure, including but not limited to existing street crossings, sewer lines, water mains, sanitary sewer mains, storm sewers, and storm water basins and artificial structures.
  - E. Development shall use public and private partnerships to modify existing storm water basins and turn them into public amenities located on water quality and site employment.
  - F. Housing product diversity in all forms and scale that are appropriate in an urban setting is encouraged. There shall be varying urban densities with compatible forms in a blended neighborhood, encouraging a mix of households.
  - G. Situated urban townhomes shall be concentrated along green space areas and neighborhood park areas and be limited to the urban areas as illustrated on the Conceptual Illustrative Master Plan in Sheet 8.
  - H. Green construction an encouragement to minimize impact of storm water, provide urban pathways for wildlife, and promote recreation, wellness, and pedestrian walk mobility.
  - I. Development design shall encourage proximity of uses to each other to create an urban form.
  - J. Development shall provide an opportunity for housing that could include proposed garages, larger walk, small parks, tree gardens, break way living spaces, and other landscape amenities.
  - K. Housing that offers sweeping views towards the river, watermarks park, Gray's Lake, and downtown is encouraged.
  - L. Development shall promote shared responsibility to encourage walking and cycling near a road network for motor vehicles.
  - M. Development shall create a unique photo-taking identity for the neighborhood through signage design in both the private and public realm.
  - N. All components and phases shall relate to, and integrate with, each other and previous components.
  - O. Development shall establish vehicular, mass transit, bicycle, and pedestrian linkages with the following:
    1. Downtown Central Business District.
    2. Gray's Lake.
    3. Development of the DCEO site.
  - P. Each project phase/development shall provide consistency and compatibility of character across the development.
- II. Street Character
  - A. All components of development shall provide a unified streetscape system that addresses the same materials, design elements, and equipment throughout the redevelopment area. New materials are allowed from street to street to create street identity or highlight local uses of the street (i.e., the streetscape along the Pedestrian Bridge Landing Plaza), but the overall design of each street shall still contribute to a neighborhood identity.
  - B. Development shall provide a street system primarily aligned to downtown street grid that provides safety and ease of use by vehicle users:
    1. Public streets shall not have no-access or right turning curves.
    2. Development shall include human scaled, street lighting elements.
    3. Development shall provide a streetscape system that is walkable and that provides safety, comfort, and convenience for pedestrians.
  - C. Development shall include a pedestrian friendly environment along 7th and 5th Sts that provides clear connections between the residential, mixed use, and commercial uses.
  - D. Street cross section configurations shall be as detailed in the approved Conceptual Development Plan and the Planned Unit Development (PUD) - Conceptual Plan Submitted, Sheet 10 Conceptual Street & Alley Sections.
    1. Street cross sections may vary where existing or planned utility infrastructure will contain and/or limit them.
- III. Building Character
  - A. Buildings shall be developed for each project phase/development and provide consistency and compatibility of character across the development while providing for flexibility in individual building design and building context.
    1. Building volumes in areas A, B, H, N, O, W, X, Y and Z shall be due to durable materials such as brick, masonry, stone, cement board, etc.
    2. Exterior insulation systems shall not be used.
    3. Industrial pre-engineered buildings shall not be allowed.
    4. Rooftop mechanical equipment shall be enclosed or otherwise screened from public rights of way.
  - B. A minimum of 75% of the surface area (including windows and doors) of building housing and perpendicular to a public street must be glass, brick, concrete panels, architectural concrete blocks (such as split-face or burnished block), architectural metal panels or stone. Fiber cement or wood panels are also acceptable, if well proportioned.
  - III. Building Massing, Placement, Density, Arrangement (See approved Conceptual Development Plan for Land Use Classifications)
    - A. Primary features of medium and commercial structures shall be primarily parallel to the public right-of-way except in the address of W and X.
    - B. Open space between building facades along 7th St shall include a human scale.
    - C. Overhead utility lines shall be allowed where they intersect residential areas.
      1. Down link housing shall not be allowed.
      2. Utility housing shall not be allowed.
      3. Utility housing shall not be allowed at grade or on a rooftop except for an accent in a terrace or wall.
    - D. Attached and Detached Residential uses shall have minimal setbacks from public streets to encourage neighborhood interaction and define the streetscape edge.
      1. Attached and Detached Residential: Side and Corner Lots
        - a. Lots with side elevations at block corners shall include enhancements to foundation and details distinct from side elevations adjacent to the block.
        - b. Lots with side elevations at block corners, public streets or open spaces shall include enhancements to foundation, details and roof forms or existing exterior form side elevations adjacent to the block and distinct from side elevations adjacent to the edge of the public realm.
    - E. Attached and Detached Residential: Side and Corner Lots
      1. Lots with side elevations at block corners shall include enhancements to foundation and details distinct from side elevations adjacent to the block.
      2. Lots with side elevations at block corners, public streets or open spaces shall include enhancements to foundation, details and roof forms or existing exterior form side elevations adjacent to the block and distinct from side elevations adjacent to the edge of the public realm.
    - F. Detached Residential uses on narrow lots shall have minimal side yard setbacks from adjacent structures to promote density and define the edge of the public realm.
    - G. Building Height
      1. Downtown Mixed Use
        - a) Maximum height for all uses shall be two stories.
      2. High Density Residential
        - a) Maximum height for all uses shall be as follows:
          - (1) Three stories for Subareas B, H, N, O and Z
          - (2) Eight stories for Subareas Y and Z
      3. Medium Density Residential
        - a) Maximum height for all uses shall be one story except as indicated on the Conceptual Illustrative Master Plan (Sheet 8).
    - H. Building Setbacks
      1. For all residential buildings, a maximum setback of 15 feet from the public right-of-way is permitted unless constrained by utility easements.
        2. Medium Density Residential
          - a) Attached and Detached residential buildings shall have side-yard building separation(s) (i.e., building toe-to-building toe) that are at minimum 5 feet and at maximum 10 feet except where walking paths occur between buildings.
            - (1) Where walking paths occur between buildings the maximum width of building toe-to-building toe shall increase above the maximum separation by the width of the walking path.
    - I. Building Entrances
      1. Downtown Mixed Use
        - a) Buildings shall have entrances oriented toward primary street(s) and public plaza(s).
- IV. Building Form
  1. All buildings shall have a parapet or flat roof. Tower elements are allowed.
  2. High Density Residential
    - a) All buildings shall have a parapet or flat roof. Tower elements are allowed.
  3. Medium Density Residential
    - a) All building roofs shall have a contemporary, urban character.
- V. Signs (See approved Conceptual Development Plan for Land Use Classifications)
  - A. Definitions
    1. Sign means any structure, including but not limited to a device or display, other than buildings or landscaping, used primarily for visual communication for the purpose of having the result of bringing the subject thereof to the attention of a person, group of persons, or the public generally. The term "sign" includes but is not limited to any and all reading matter, letters, numerals, pictorial representations, emblems, trademarks, inscriptions, and patterns, whether affixed to a building, painted or otherwise displayed on a building, or appears from any building, having the character of being intended to be readily intelligible or recognizable advertising or any sign on which commercial advertising is permitted.
    2. Sign area means the total area contained within the limits of a sign, provided, however, that the area of a sign encompasses the sign letters or signs signs with an overall height of 40 inches or less, attached to a single supporting structure, shall be the area of the larger separate sign face. The area of a sign composed of characters or words attached to a building or wall surface is the smallest rectangle which encloses the sign.
    3. Sign, lettering or wall surface, a sign, other than a roof sign, which is supported by a building or wall, and is further defined as follows:
      - a) Sign, facade means a single-faced building or wall sign which is parallel to the supporting wall.
      - b) Sign, projecting means a building or wall sign other than a facade sign.
      - c) Sign, directional means a sign for the purpose of traffic control which is located on private property.
      - d) Sign, directory means a sign containing the name of a building, complex or center and box or more identification signs on the same site, either on private property, located on the sign on the low identification sign per occupancy.
      - e) Sign, character display means any portion of a sign upon which alphabetic, pictographic or symbolic information is contained that is designed to attract attention to a display screen computer or light emitting diode (LED), thin plate, light tube or other illumination devices, including but not limited to programmable microprocessor controlled electronic displays, and the projection of images or messages with these characteristics by any other means with the sign face.
      - f) Sign, face means all of the surface of a sign used or designed for the presentation of a single visual display, including signs, letters and words, and excluding the supporting structure. Each separate surface of a sign used or designed for the presentation of a separate visual display shall constitute a separate sign face.
      - g) Sign, branding means a sign not attached to any building and is further defined as follows:
        - a) Sign, portable means a sign not permanently attached to a structure.
        - b) Sign, monument means a sign affixed to a structure such as grade, having a solid opaque base, constructed of brick, stone, concrete block or other durable material matching the exterior of the primary building and extending from grade to the bottom of the sign face across the entire width of the sign face. The height of the sign face must not be less than the larger of two feet or 25 percent of the total sign height. Monument signs are prohibited in the character of a monument sign.
      - h) Sign, pole means a freestanding sign which is not a portable sign nor a monument sign.
    4. Sign, identification means as follows:
      - a) Type size means an alphanumeric sign that displays no more than the name, numerical address, and color or message of an occupant of the premises.
      - b) Type size means an alphanumeric sign that displays no more than the name, occupation or profession of an occupant of the premises, name of any building on the premises or the trademark of the occupant.
      - c) Sign, identify (typed) means a sign illuminated by artificial lighting extending from the sign face, the light source not visible from any adjacent right-of-way.
      - d) Sign, illumination (backlit) means an alphanumeric sign containing a surface area upon which is displayed the name of a religious institution, school, library, community center or similar institution and the announcement of its services or activities.
      - e) Sign, internally illuminated means a sign illuminated by an artificial light source which is not visible but which reflects the sign through a diffusing medium.
      - f) Sign, message means a sign attached to and contained within the perimeter of the face or surface of a structure.
      - g) Sign, medium display means any portion of a sign where the display surface is comprised of rotating materials that permit the display of different messages by the rotation of the elements.
      - h) Sign, off-premise advertising means a sign which identifies or directs attention to:
        - a) A profession, business, service, activity, product, campaign or structure manufactured, sold, offered or conducted at a location other than upon the premises where such sign is located, and/or
        - b) The biological or non-commercial views of a party who is not an occupant of the premises.







# Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa

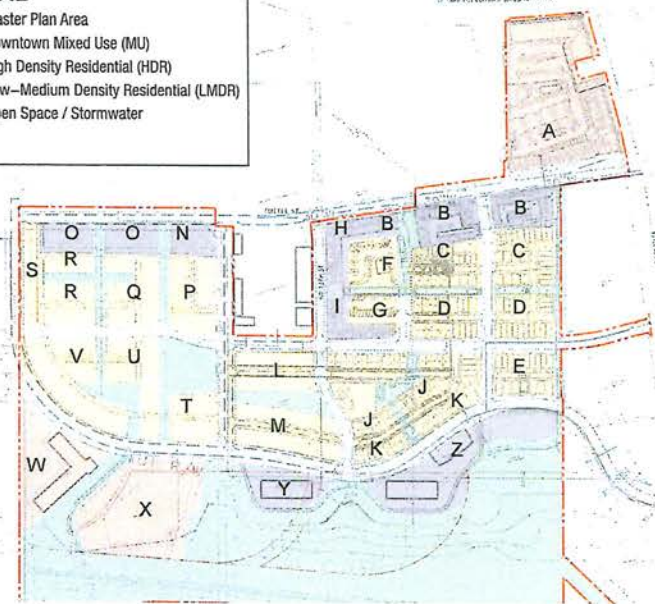
## Sheet 3 of 20



GRAY'S STATION

**LEGEND**

- Master Plan Area
- Downtown Mixed Use (MU)
- High Density Residential (HDR)
- Low-Medium Density Residential (LMDR)
- Open Space / Stormwater



Principal Uses	Plan Subarea																						
	MU	HDR	MDR	MDR	MDR	MDR	MDR	HDR	MDR	MDR	MDR	MDR	HDR	HDR	MDR	MDR	MDR	MDR	MDR	MU	MU	HDR	HDR
<b>Residential Use Category</b>																							
1-unit dwelling			X	X	X	X	X																
2-unit dwelling			X	X	X	X	X																
3-unit dwelling			X	X	X	X	X																
Multi-unit dwelling (6+) units			X	X	X	X	X																
Efficiency/Micro Units			X	X	X	X	X																
Bed & Breakfast			X	X	X	X	X																
Group Living			X	X	X	X	X																
Group Living for Health Reasons			X	X	X	X	X																
Hotel/Apartment Hotel			X	X	X	X	X																
Residential Care, Large			X	X	X	X	X																
Residential Care, Small			X	X	X	X	X																
<b> Civic &amp; Institutional Category</b>																							
Assembly, Neighborhood (<10,000 sf)			X	X																			
Assembly, General (>10,000 sf)			X	X																			
Government/Higher Education/Hospital			X	X																			
Library/Museum			X	X																			
Police/Post			X	X																			
School: Pre-K, elementary, Intermediate			X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Transit Station			X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
<b>Open Space Category</b>																							
Community Garden			X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Intensive Park Uses			X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Park			X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Urban Farm			X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
<b>Retail Use Category</b>																							
Neighborhood Retail			X	X																			
General Retail			X	X																			
Beer/Wine/Liquor Sales			X	X																			
Convenience Store			X	X																			
<b>Service Use Category</b>																							
Neighborhood Service			X	X																			
General Service			X	X																			
Adult Day Care			X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Child Day Care (>12 children)			X	X																			
Community Services			X	X																			
Drinking Places			X	X																			
Eating Places			X	X																			
Entertainment Assembly			X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Private Club			X	X																			
Tattoo/Piercing Parlor			X	X																			
<b>Employment Use Category</b>																							
Office			X	X																			
Craftsman/Studio			X	X																			

X Permitted  
All uses not listed on this sheet are prohibited.

**Accessory & Temporary Uses**

Use	Plan Subarea																						
	MU	HDR	MDR	MDR	MDR	MDR	MDR	HDR	MDR	MDR	MDR	MDR	HDR	HDR	MDR	MDR	MDR	MDR	MDR	MU	MU	HDR	HDR
<b>Residential Accessory Use</b>																							
Accessory Dwelling Unit			X	X	X	X	X																
Home Child Care			X	X	X	X	X																
Home Occupation			X	X	X	X	X																
Room Rental - Long-Term (> 3 months)			X	X	X	X	X																
Room Rental - Short-Term (< 21 days)			X	X	X	X	X																
<b>Commercial Accessory Use</b>																							
Drive-Through Facility			X	X																			
Outdoor Cafe			X	X																			
Outdoor Display			X	X																			
Parking			X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
<b>Temporary Use Category</b>																							
Voting Place			X	X																			
Temporary Outdoor Event			X	X																			
Farmers Market			X	X																			

X Permitted  
All uses not listed on this sheet are prohibited.

PERMITTED LAND USES  
OCTOBER 19, 2022



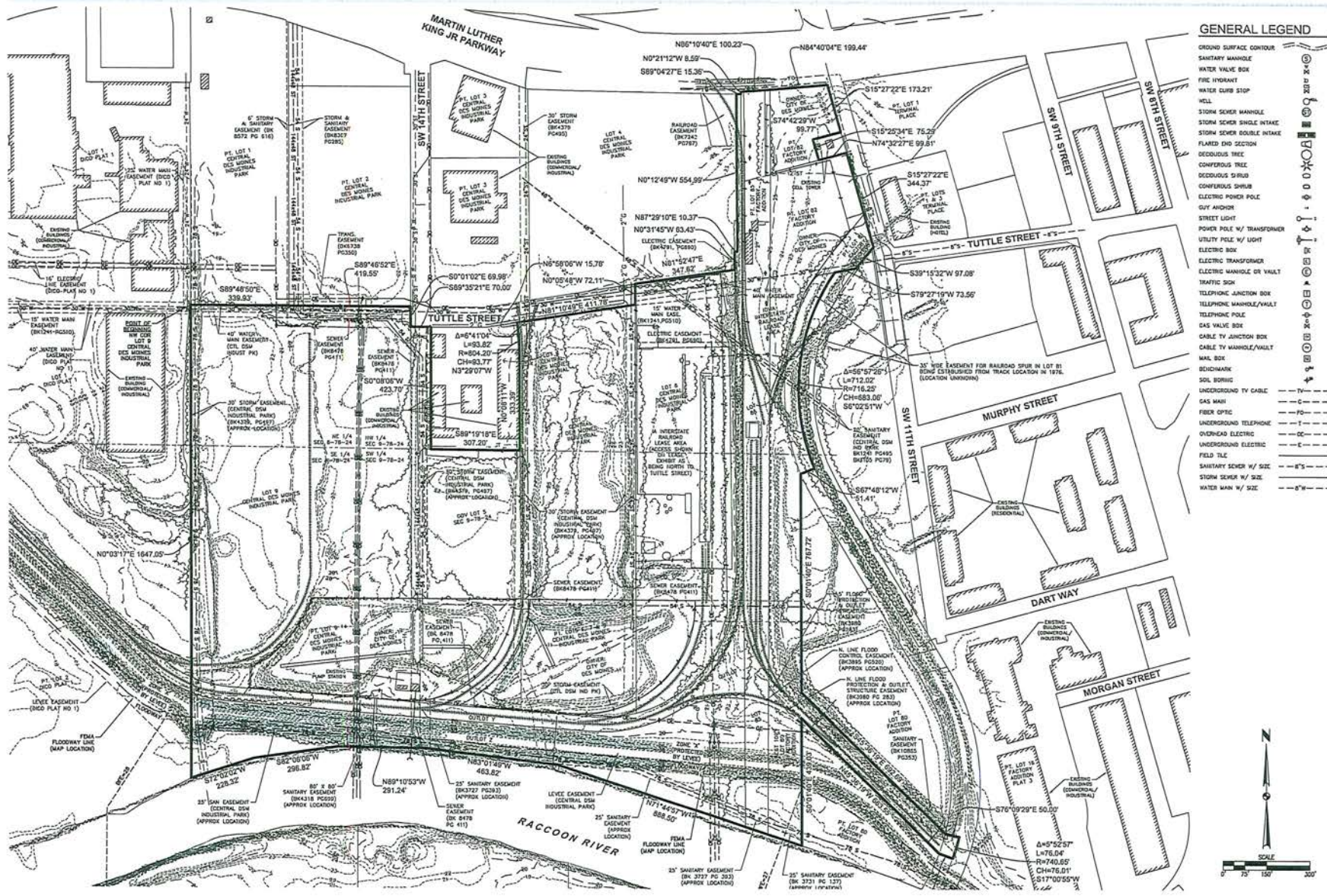




Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
 Sheet 5 of 20



GRAY'S STATION



**GENERAL LEGEND**

- GROUND SURFACE CONTOUR
- SANITARY MANHOLE
- WATER VALVE BOX
- FIRE HYDRANT
- WATER CURB STOP
- WELL
- STORM SEWER MANHOLE
- STORM SEWER SINGLE INTAKE
- STORM SEWER DOUBLE INTAKE
- FLAMED END SECTION
- DECIDUOUS TREE
- CONIFEROUS TREE
- DECIDUOUS SHRUB
- CONIFEROUS SHRUB
- ELECTRIC POWER POLE
- GUY ANCHOR
- STREET LIGHT
- POWER POLE W/ TRANSFORMER
- UTILITY POLE W/ LIGHT
- ELECTRIC BOX
- ELECTRIC TRANSFORMER
- ELECTRIC MANHOLE OR VAULT
- TRAFFIC SIGN
- TELEPHONE JUNCTION BOX
- TELEPHONE MANHOLE/VAULT
- TELEPHONE POLE
- GAS VALVE BOX
- CABLE TV JUNCTION BOX
- CABLE TV MANHOLE/VAULT
- MAN. BOX
- BOUNDARY
- SOIL BORING
- UNDERGROUND TV CABLE
- GAS MAIN
- FIBER OPTIC
- UNDERGROUND TELEPHONE
- OVERHEAD ELECTRIC
- UNDERGROUND ELECTRIC
- FIELD TILE
- SANITARY SEWER W/ SIZE
- STORM SEWER W/ SIZE
- WATER MAIN W/ SIZE

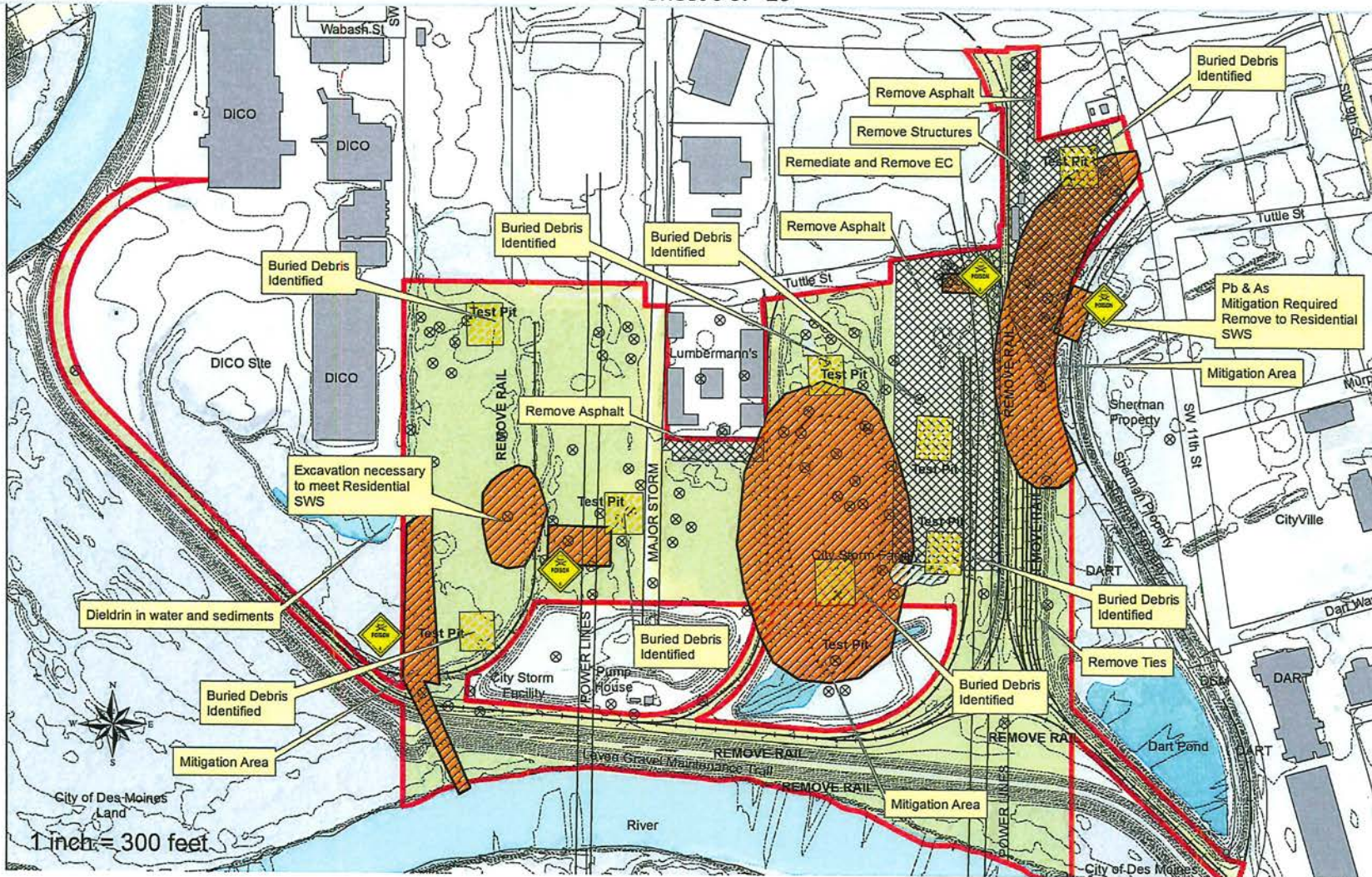


PLAN MAP:  
 EXISTING CONDITIONS  
 MAY 3, 2017





Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
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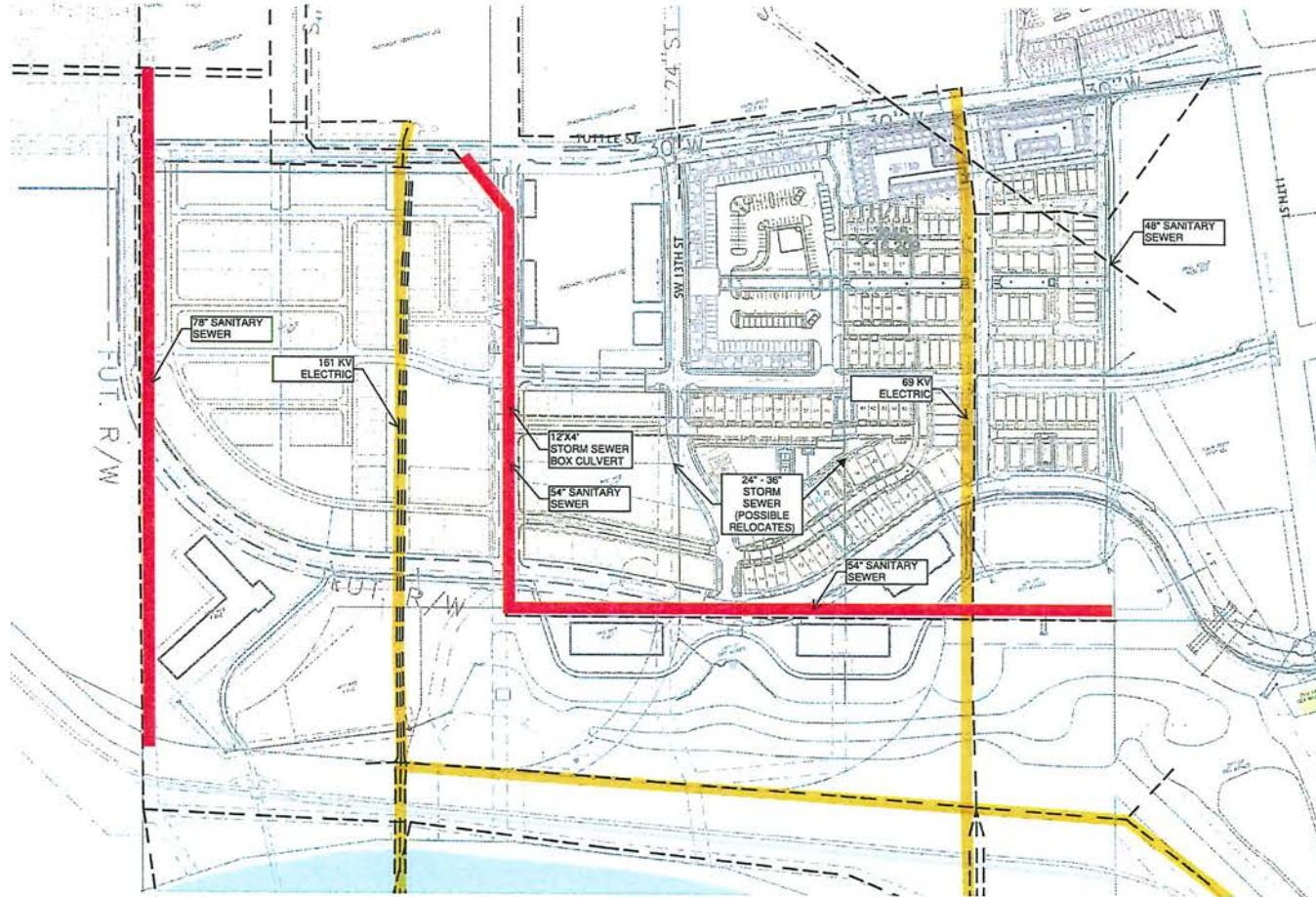
NOTES:  
 1. Refer to supplemental digital content provided to the City of Des Moines for additional environmental conditions findings.

ENVIRONMENTAL CONDITIONS  
 OCTOBER 19, 2022





Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
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**LEGEND:**  
— EXISTING SANITARY LINE TO REMAIN  
- - - EXISTING OVERHEAD POWER LINES TO REMAIN

**NOTES:**  
 1. The conceptual plan takes into account significant existing infrastructure features that are currently infeasible to move based on economic factors. Unless economic conditions change, future development plans should respect and sensitively integrate the existing infrastructure highlighted in the adjacent diagram and attached legend.

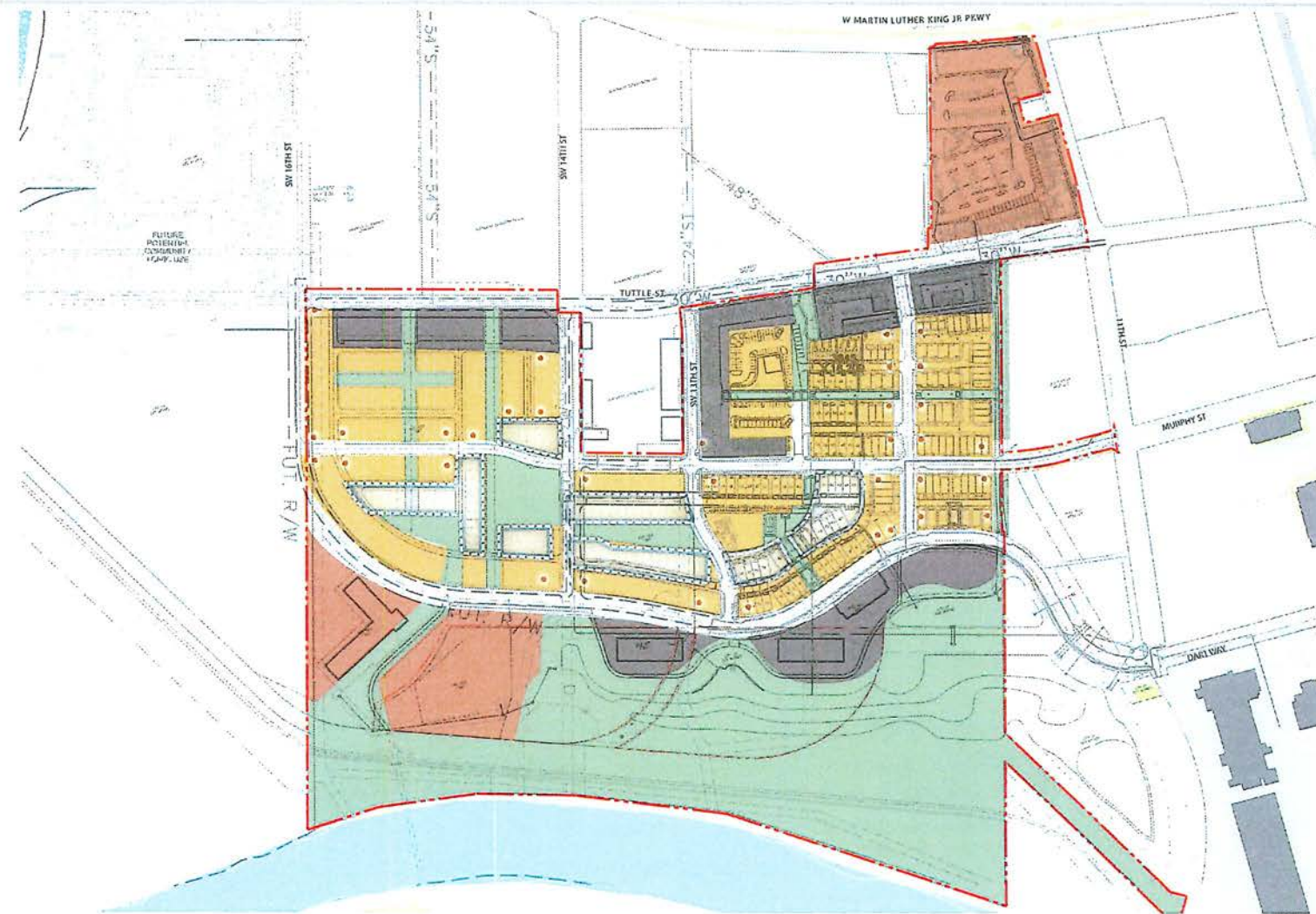
EXISTING INFRASTRUCTURE CONSTRAINTS  
 OCTOBER 19, 2022

0 | 120' | 240' | 480' | NORTH





Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
 Sheet 8 of 20



**NOTES:**  
 1. The master plan at left is for illustrative purposes only and is provided to show general, conceptual intent and character of the development. Individual parcels, building footprints, landscape design, open space design and parking design will be determined and designed as a part of future development plans for each phase.

**LEGEND**

- Master Plan Area
- Downtown Mixed Use (MU)
- High Density Residential (HDR)
- Low-Medium Density Residential (LMDR)
- Open Space / Stormwater
- 2 story minimum building height at corner lots where indicated with orange circle
- Detached Urban Townhomes Subarea (Other uses permitted as per the Permitted Land Uses Chart)

CONCEPTUAL ILLUSTRATIVE MASTER PLAN  
 OCTOBER 19, 2022





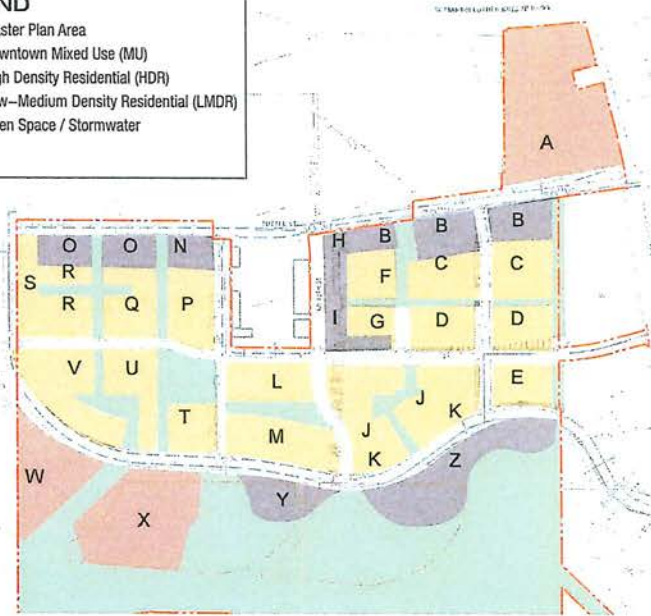
Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
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GRAY'S STATION

**LEGEND**

- Master Plan Area
- Downtown Mixed Use (MU)
- High Density Residential (HDR)
- Low-Medium Density Residential (LMDR)
- Open Space / Stormwater



*Hubbell Gray's Station*  
Master Plan Concept  
Conceptual Land Use Summary & Estimated Residential Units

AREA NAME	LAND USE	ACRES	MINIMUM DENSITY (QU/AD)	MAXIMUM DENSITY (QU/AD)
A	Mixed Use	5.3	30	50
B	HDR	1.9	30	50
C	MDR	2.0	8	35
D	MDR	1.9	8	35
E	MDR	1.0	8	35
F	MDR	0.7	8	35
G	MDR	0.7	8	35
H	HDR	0.1	18	30
I	MDR	0.8	8	35
J	MDR	1.2	7	22
K	MDR	1.7	7	22
L	MDR	1.6	8	35
M	MDR	1.6	8	35
N	HDR	0.4	18	30
O	HDR	1.2	40	55
P	MDR	1.0	8	35
Q	MDR	1.2	8	35
R	MDR	1.1	8	35
S	MDR	0.7	8	35
T	MDR	0.7	8	35
U	MDR	1.1	8	35
V	MDR	2.0	8	35
W	Mixed Use	2.0	30	50
X	Mixed Use	3.5	30	50
Y	HDR	1.0	50	70
Z	HDR	2.7	50	70
<b>TOTAL</b>		<b>39.2</b>		

**Illustrative Estimate**

Illustrated Units	Illustrated Net Density	Assumed Multi-family residential floors	City Classification
193	37	4	High
79	41	3	High
34	17		High
34	18		High
16	16		Med
10	14		Med
11	16		Med
5	35	3	High
12	16		Med
13	11		Low-Med
22	13		Med
32	19		High
32	20		High
15	33	3	High
65	54	3	High
22	22		High
22	18		High
24	21		High
16	22		High
10	14		Med
18	16		Med
38	19		High
85	42	4	High
142	41	4	High
55	54	5	High
148	55	5	High
<b>TOTAL</b>	<b>1151</b>	<b>29.3</b>	

Gray's Station will achieve a minimum of 1,100 dwelling units.

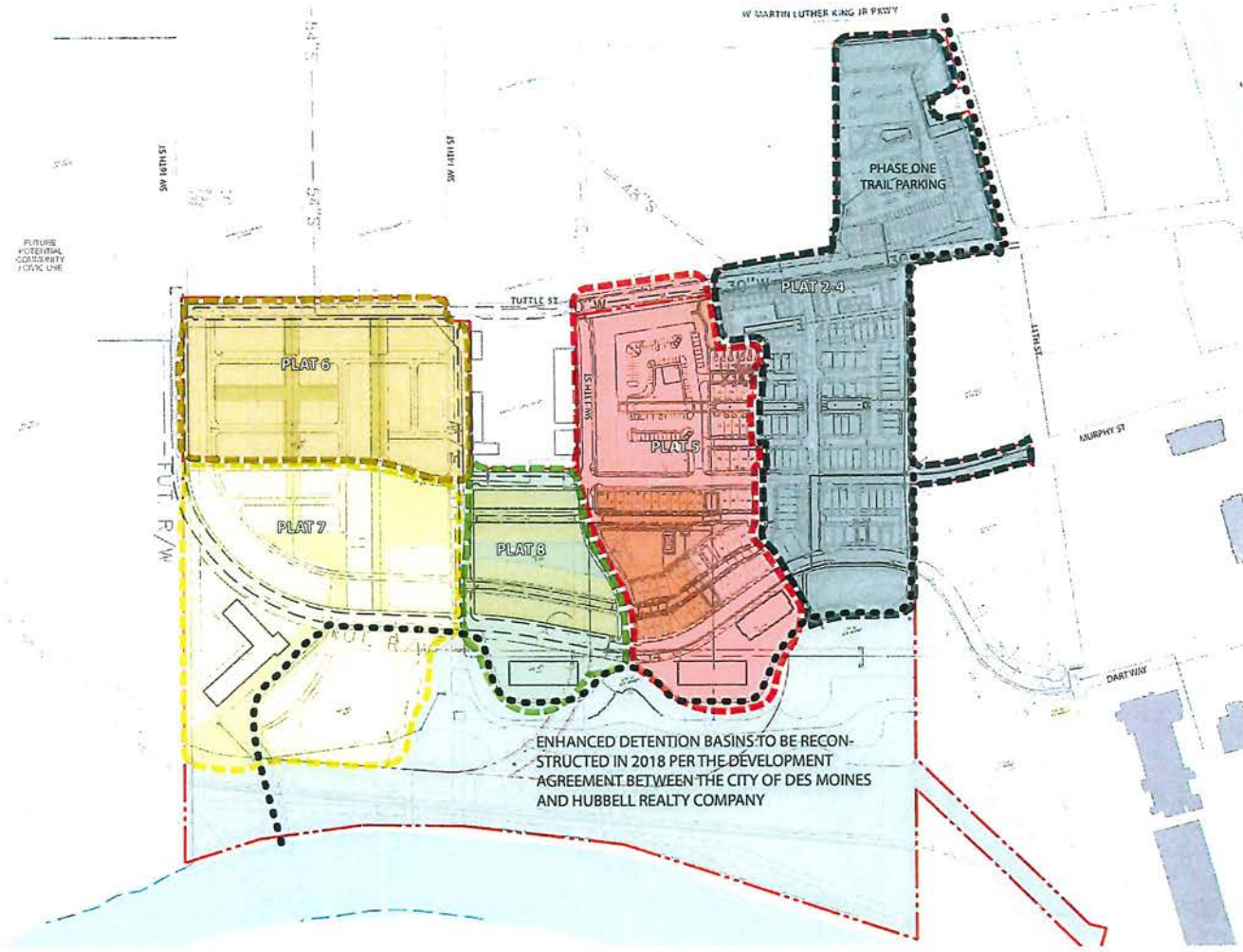
MINIMUM NET DENSITY (EXCLUDING SIGNIFICANT OPEN SPACE, ROADS) **28**

**Notes:** 1. The assumed number of multi-family units is based on an average 1000 sf dwelling unit.





Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
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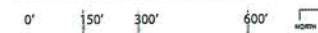
NOTES:  
 Variables that will impact the anticipated phasing of the Gray's Station plan include the timing of:  
 1) Redevelopment of the DICO site, and  
 2) Infrastructure, improvements and street extensions completed by the City of Des Moines.

**LEGEND**

- Master Plan Area
- Phase One Trail - Conceptual Alignment

Anticipated Phasing:

- Plat 2-4
- Plat 5
- Plat 8
- Plat 6
- Plat 7



CONCEPTUAL PHASING DIAGRAM  
 OCTOBER 19, 2022



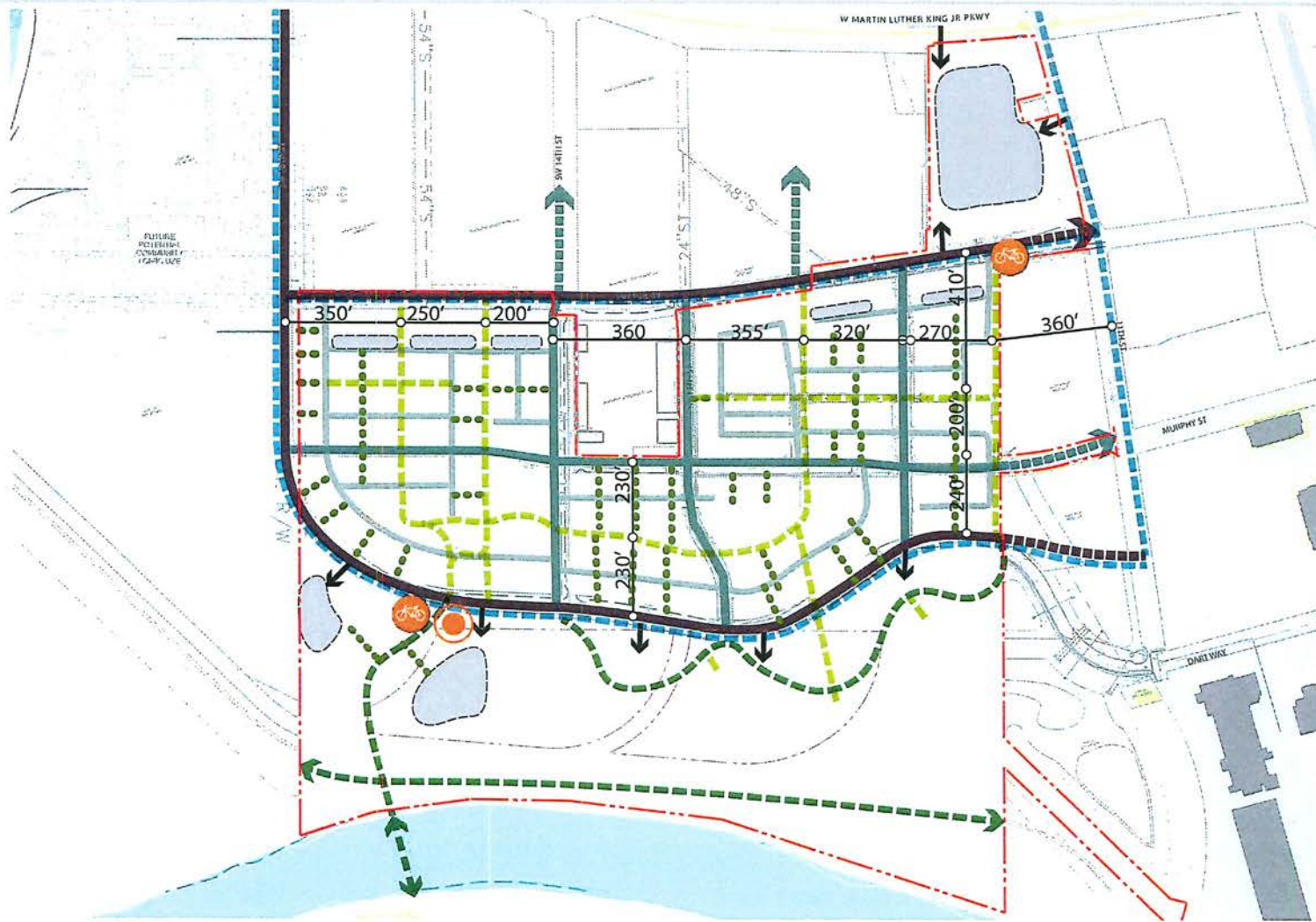
Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
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GRAY'S STATION

NOTES:

- The location of the following items are approximate and provided to show general plan intent of the circulation network:
  - Block lengths between streets and greenways;
  - Greenway locations;
  - Road centerlines;
  - Alley network;
  - Access drive location;
  - Bus stop locations;
  - Bike share locations;
  - Off-street parking areas.
- The proposed circulation network outside of the master plan area is shown for illustrative purposes and to demonstrate the development's intent to tie into future improvements outside the scope of this master plan.
- The Conceptual Plan was completed while the City's Transportation Master Plan (MOVE DSM) was under development. The intent of this Conceptual Plan is to comply with the MOVE DSM plan based on the preliminary goals that have been publicly presented such as:
  - Support other City goals of safety, quality of life, and economic development.
  - Ensure that Des Moines is best positioned to provide transportation options for all citizens in every stage of life.



**LEGEND**

- Master Plan Area
- Proposed Collector Street  
*Anticipated 2 lanes travel, 2 lanes parking, bicycle facilities*
- Proposed Local Street  
*Anticipated 2 lanes travel, 1 lane parking*
- Proposed Alley
- Proposed Bicycle/Pedestrian Route – Primary
- Proposed Greenways
- Proposed On-Street Bike Facility
- Existing Regional Trail
- Existing Local Trail
- Proposed Access Drive
- Proposed Off-street Surface Parking Area
- Proposed Bus Stop
- Proposed Bike Share

CONCEPTUAL CIRCULATION DIAGRAM  
 OCTOBER 19, 2022





Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
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GRAY'S STATION

CONCEPTUAL STREET SECTION: SOUTHERN ROAD/16TH STREET (COLLECTOR STREET)



CONCEPTUAL STREET SECTIONS: TUTTLE STREET - CYCLE TRACK (COLLECTOR STREET)



CONCEPTUAL ALLEY SECTION



CONCEPTUAL STREET SECTION: LOCAL STREET AT 14TH STREET



CONCEPTUAL STREET SECTION: LOCAL STREETS (EXCLUDING 14TH STREET)

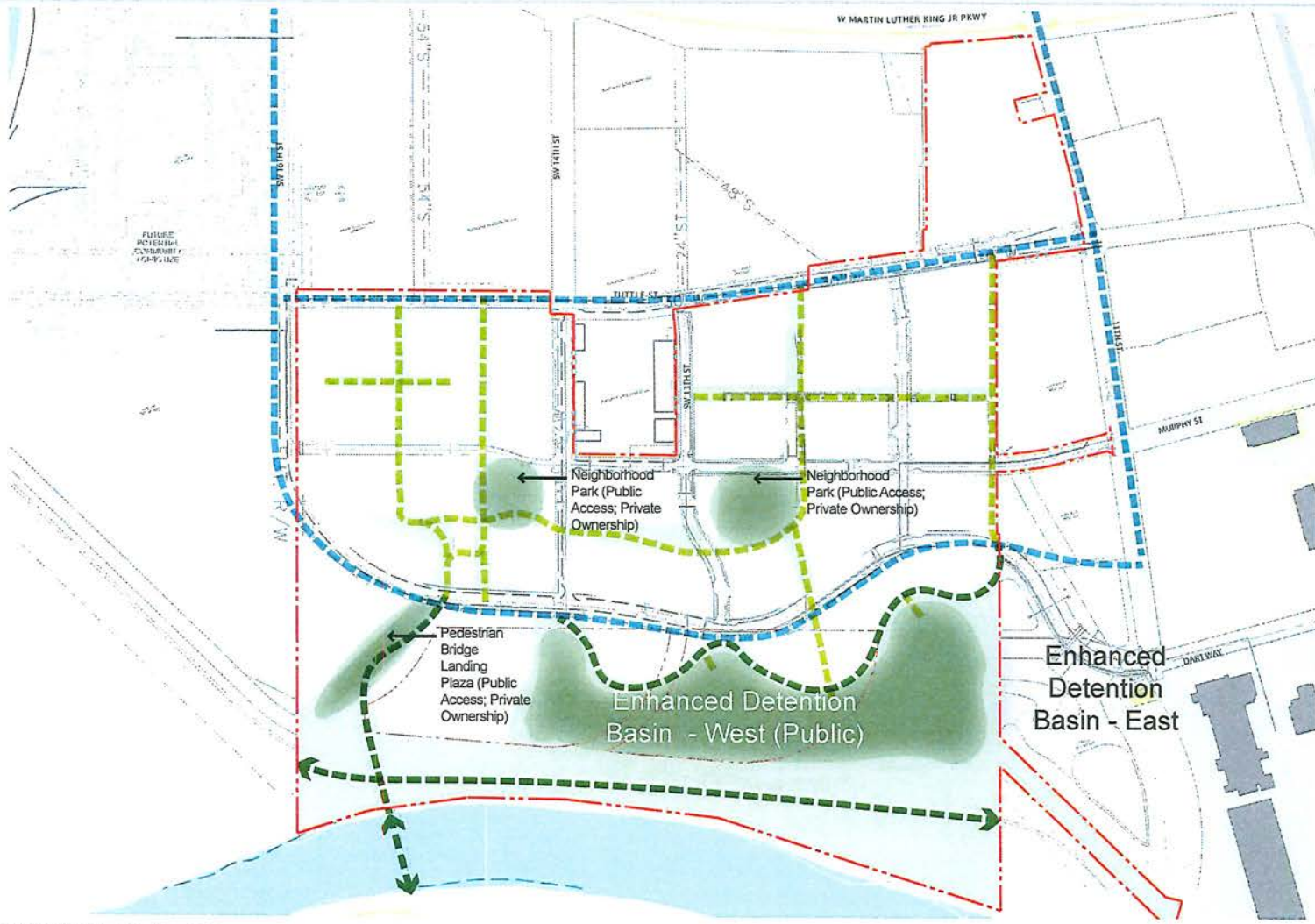


NOTE: BUILDINGS SHOWN IN ALL SECTIONS FOR GENERAL MASSING ONLY. ARCHITECTURAL DESIGN IS YET TO BE DETERMINED. FINAL DEVELOPMENT PLANS TO BE REVIEWED AND

CONCEPTUAL STREET AND ALLEY SECTIONS  
 OCTOBER 19, 2022



Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
 Sheet 13 of 20

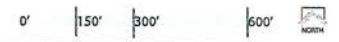


**LEGEND**

- Master Plan Area
- Proposed Public or Public Access Area
- Proposed Bicycle/Pedestrian Route – Primary
- Proposed Greenways
- Proposed On-Street Bike Facility
- Existing Regional Trail
- Existing Local Trail

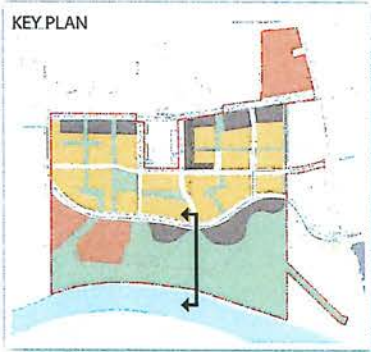
NOTE: See Sheet 2A for land dedication and maintenance plans for open space areas.

CONCEPTUAL OPEN SPACE PLAN  
 OCTOBER 19, 2022





Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
Sheet 14 of 20



NOTE: BUILDINGS SHOWN FOR GENERAL MASSING ONLY. ARCHITECTURAL DESIGN IS YET TO BE DETERMINED. SUBJECT TO INDIVIDUAL FINAL DEVELOPMENT PLANS TO BE REVIEWED AND APPROVED BY THE PLANNING & ZONING COMMISSION AND CITY COUNCIL.

CONCEPTUAL SECTION ELEVATION THROUGH ENHANCED DETENTION BASIN  
OCTOBER 19, 2022





# Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa

## Sheet 15 of 20



GRAY'S STATION

### OPEN SPACE CHARACTER

The open space system at Gray's Station is intended to support a diverse, urban population with a focus on wellness; opportunities for social and physical connections; and areas for connections to nature and solitude.

### PEDESTRIAN BRIDGE LANDING PLAZA

The following images and conceptual programming represent the intended character and feel of the plaza area anticipated in the Gray's Station neighborhood, but detailed programming and design of the park systems will be completed during later phases of the development. Final Development Plans to be reviewed by the Planning & Zoning Commission and by City Council.

Gathering • People-Watching • Outdoor Dining • Festival Space • Bike Connectivity



### NEIGHBORHOOD PARKS

The following images and conceptual programming represent the intended character and feel of the neighborhood parks anticipated in the Gray's Station neighborhood, but detailed programming and design of the park systems will be completed during later phases of the development. Final Development Plans to be reviewed by the Planning & Zoning Commission and by City Council.

Dog Walking • Playing • Learning • Picnicking • Gathering • Community Gardening • Community Space



### GREENWAYS

The following images and conceptual programming represent the intended character and feel of the greenways anticipated in the Gray's Station neighborhood, but detailed programming and design of the park systems will be completed during later phases of the development. Final Development Plans to be reviewed by the Planning & Zoning Commission and by City Council.

Connecting • Dog Walking • Playing • Biking • Neighborhood Gatherings



### ENHANCED STORMWATER DETENTION BASINS

The following images and conceptual programming represent the intended character and feel of the enhanced regional stormwater detention basins anticipated in the Gray's Station neighborhood, but detailed programming and design of the park systems will be completed during later phases of the development. Final Development Plans to be reviewed by the Planning & Zoning Commission and by City Council.

Restoring • Connecting • Learning • Unpaved Trails • Signage • Seating

The proposed enhanced detention basins will provide an attractive, educational water feature with public access and improved water quality within the Raccoon River. It is proposed to have sufficient volume to meet the water quality volume (WQV) requirements for the proposed Gray's Station development and have enough volume to address the 100-year storm event for the upstream watershed areas under full buildout conditions.

- The East Detention Basin area, formerly known as DART pond, is off-site, but is being planned for improvements concurrently with this redevelopment area. WQV requirements for areas within the proposed development will be met through permanent pool storage in the pool and marsh zones of the basin.
- The West Detention Basin area has sufficient volume to meet WQV requirements for areas within the proposed development through permanent pool storage in the pool and marsh zones of the basin.
- Permanent and well-maintained temporary access will be provided to the stormwater pump station at all times and coordinated with the City of Des Moines to ensure operation and maintenance of the station.
- A new security fence will be constructed around the stormwater pump station.

Though its primary purpose is to improve water quality, the enhanced detention basins also serve as a place to enhance the health of the community and to offer educational opportunities for the larger community. At a Master Plan level, the area features paved and unpaved trails, wetland overlook areas, and seating areas. The proposed inclusion of interpretive signage and art would allow people of all ages to understand the role of the area for the region. The use of micro-grading for the basins allows the area to be carefully shaped. This shaping breaks down the scale of the regional detention, creates places that are enjoyable, and removes the utilitarian feel of the existing basins.



PROPOSED OPEN SPACE CHARACTER IMAGERY  
OCTOBER 19, 2022



# Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa

## Sheet 16 of 20



GRAY'S STATION

### PRECEDENT HOUSING CHARACTER IMAGERY

As a large neighborhood, the proposed housing character for Gray's Station is intended to be diverse and urban in form/character. The following images represent the intended character and feel of the multi-family and mixed-use residential housing types. The design of the buildings including architectural details, materials, final massing/heights and colors will be completed during later phases of the development. Final Development Plans to be reviewed by the Planning & Zoning Commission and by City Council.

Each character image represents varied features as represented by the following categories that correspond to the colors under each image:

- Relationship to exterior open space (plaza, park, roof deck, etc.)
- Relationship to street
- Relationship to alley
- Building articulation/massing
- Architectural character
- Landscape character

### DOWNTOWN MIXED USE (NE CORNER)



### RESIDENTIAL



### MULTI-FAMILY / MEDIUM-HIGH DENSITY



### DOWNTOWN MIXED USE (BRIDGE LANDING)



PROPOSED BUILDING CHARACTER IMAGERY  
OCTOBER 19, 2022



# Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa

## Sheet 17 of 20



GRAY'S STATION

### PRECEDENT HOUSING CHARACTER IMAGERY

As a large neighborhood, the proposed housing character for Gray's Station is intended to be diverse and urban in form/character. The following images represent the intended character and feel of the medium density housing types. The design of the buildings including architectural details, materials, final massing/heights and colors will be completed during later phases of the development. Final Development Plans to be reviewed by the Planning & Zoning Commission and by City Council.

Each character image represents varied features as represented by the following categories that correspond to the colors under each image:

- Relationship to exterior open space (plaza, park, roof deck, etc.)
- Relationship to street
- Relationship to alley
- Building articulation/massing
- Architectural character
- Landscape character

### MEDIUM DENSITY RESIDENTIAL



PROPOSED BUILDING CHARACTER IMAGERY  
OCTOBER 19, 2022





Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
Sheet 18 of 20



DOWNTOWN MIXED USE - PROTOTYPICAL BUILDING

1. Primary facades of mixed-use and commercial structures shall be primarily parallel to the public right-of-way except in the subareas of W and X.
2. Buildings shall have entrances oriented toward primary street(s) and public plaza(s).
3. At least one building entrance for residential uses should directly access the street or public plaza when a residential use is located above street-level retail or commercial uses.
4. All buildings should have frontage on principal street(s), public plaza, park or greenway of not less than 70 percent of the lot (with exceptions as outlined in the Design Guidelines).
5. For commercial and mixed-use buildings, at least 70 percent of the building frontage should be within one foot of the property line (with exceptions as outlined in the Design Guidelines).
6. Facades of above grade structured parking (e.g., podium parking beneath commercial or residential uses) adjacent to any public right-of-way shall be architecturally and aesthetically consistent with the remainder of the building they support.
7. Minimum height for all Downtown Mixed Use buildings shall be four stories.
8. All Downtown Mixed Use buildings shall have a parapet or flat roof. Tower elements are allowed.



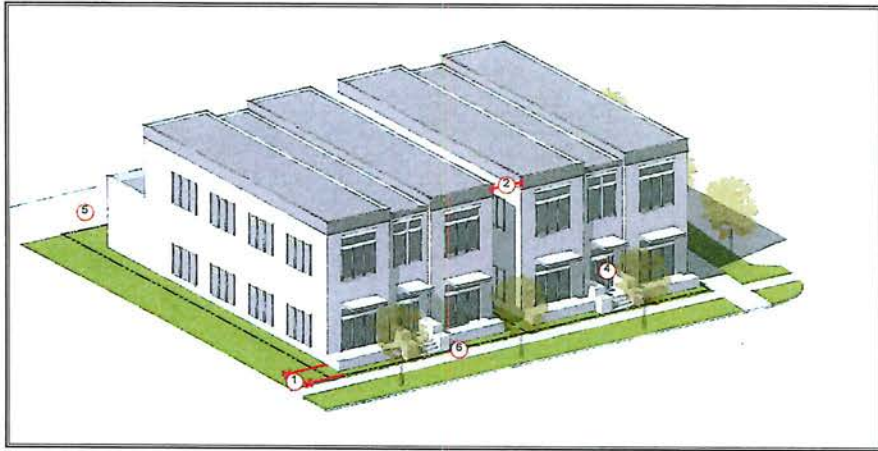
HIGH DENSITY RESIDENTIAL - PROTOTYPICAL BUILDING

1. For residential buildings, a maximum setback of 15 feet from the public right-of-way is permitted unless constrained by utility easements.
2. Buildings shall have entrances oriented toward primary street(s) and public plaza(s).
3. All buildings should have frontage on principal street(s), public plaza, park or greenway of not less than 70 percent of the lot (with exceptions as outlined in the Design Guidelines).
4. Facades of above grade structured parking (e.g., podium parking beneath commercial or residential uses) adjacent to any public right-of-way shall be architecturally and aesthetically consistent with the remainder of the building they support.
5. Minimum height for all uses shall be as follows:
  - a) Three stories for Subareas B, H, N, and O
  - b) Eight stories for Subareas Y and Z
6. All Downtown Mixed Use buildings shall have a parapet or flat roof. Tower elements are allowed.

PROPOSED PROTOTYPICAL FORMS  
OCTOBER 19, 2022







MEDIUM DENSITY RESIDENTIAL - PROTOTYPICAL MULTI-UNIT DWELLING UNIT

1. For all residential buildings, a maximum setback of 15 feet from the public right-of-way is permitted unless constrained by utility easements.
2. Attached and Detached residential buildings shall have side-yard building separation(s) (i.e., building face to building face) that are at minimum 5 feet and at maximum 10 feet except where walking paths occur between buildings.
  - a. Where walking paths occur between buildings the maximum width between buildings shall increase above the maximum separation by the width of the walking path.
3. Greenways shall include pedestrian pathways with a minimum width of 5'
4. Buildings shall have entrances oriented toward primary street(s), public plaza(s), parks, greenways or alleys.
5. All garages shall be oriented to an alley.
6. All buildings should have frontage on principal street(s), public plaza, park, greenways or alleys of not less than 70 percent of the lot (with exceptions as outlined in the Design Guidelines).



MEDIUM DENSITY RESIDENTIAL - PROTOTYPICAL DETACHED FORM (WITH FOURPLEX)

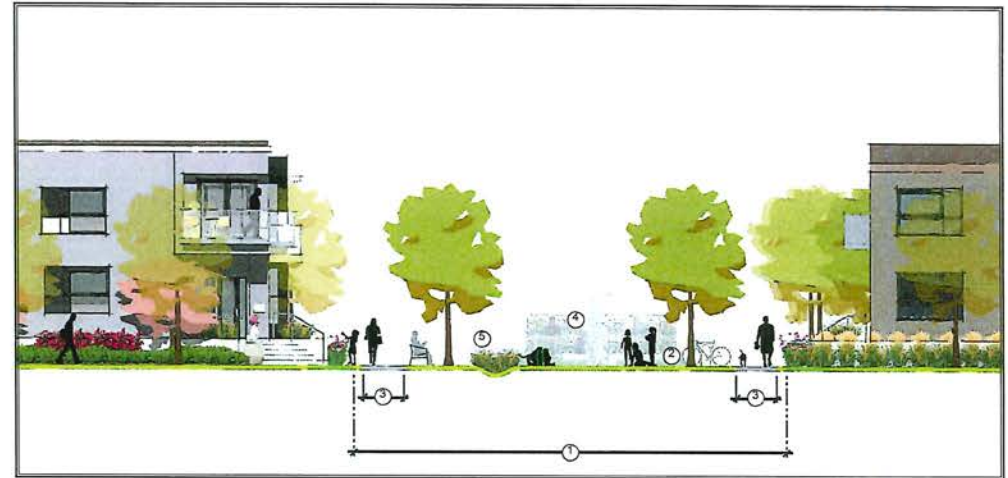
1. For all residential buildings, a maximum setback of 15 feet from the public right-of-way is permitted unless constrained by utility easements.
2. Attached and Detached residential buildings shall have side-yard building separation(s) (i.e., building face to building face) that are at minimum 5 feet and at maximum 10 feet except where walking paths occur between buildings.
  - a. Where walking paths occur between buildings the maximum width between buildings shall increase above the maximum separation by the width of the walking path.
3. Greenways shall include pedestrian pathways with a minimum width of 5'
4. Buildings shall have entrances oriented toward primary street(s), public plaza(s), parks, greenways or alleys.
5. All garages shall be oriented to an alley.
6. Accessory Dwelling Units shall be allowed.
7. All buildings should have frontage on principal street(s), public plaza, park, greenways or alleys of not less than 70 percent of the lot (with exceptions as outlined in the Design Guidelines).





MEDIUM DENSITY RESIDENTIAL - PROTOTYPICAL ATTACHED FORM

1. For all residential buildings, a maximum setback of 15 feet from the public right-of-way is permitted unless constrained by utility easements.
2. Attached and Detached residential buildings shall have side-yard building separation(s) (i.e., building face to building face) that are at minimum 5 feet and at maximum 10 feet except where walking paths occur between buildings.
  - a. Where walking paths occur between buildings the maximum width between buildings shall increase above the maximum separation by the width of the walking path.
3. Greenways shall include pedestrian pathways with a minimum width of 5'
4. Buildings shall have entrances oriented toward primary street(s), public plaza(s), parks, greenways or alleys.
5. All garages shall be oriented to an alley.
6. Accessory Dwelling Units shall be allowed.
7. All buildings should have frontage on principal street(s), public plaza, park, greenways or alleys of not less than 70 percent of the lot (with exceptions as outlined in the Design Guidelines).



GREENWAY - PROTOTYPICAL FORM

Green connections are encouraged to minimize impact of storm water, provide urban pathways for wildlife, and promote recreation, wellness, and pedestrian scale mobility.

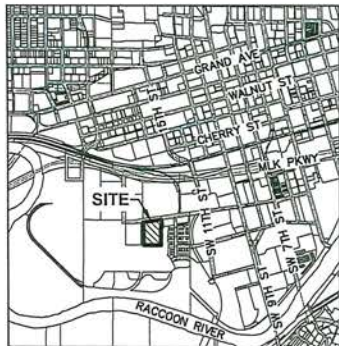
1. Greenways shall maintain an approximate width of 40' to 50'. Exceptions may occur where infrastructure requirements necessitate and will be reviewed with the Final Development Plan.
2. Greenways shall include landscaping.
3. Greenways shall include pedestrian pathways with a minimum width of 5'. Pathway location may vary depending on context.
4. Greenways may include structural (e.g., Pergola, Shade Structure, Community Garden Features, etc.) and/or artistic elements where deemed appropriate by the developer.
5. Greenways may include stormwater quality areas, where appropriate.



# SITE DEVELOPMENT PLAN FOR: GRAY'S STATION TELUS

## DES MOINES, IOWA

### VICINITY - MAP NOT TO SCALE



DES MOINES, IOWA

### OWNER

HRC NPS I LLC  
6900 WESTOWN PKWY  
WEST DES MOINES, IA 50266

### APPLICANT

HUBBELL REALTY COMPANY  
CONTACT: KRIS SADDORIS  
6900 WESTOWN PKWY  
WEST DES MOINES, IA 50266  
PH. (515) 243-3228

### ENGINEER

CIVIL DESIGN ADVANTAGE  
4121 NW URBANDALE DRIVE  
URBANDALE, IA 50322  
CONTACT: RYAN HARDISTY  
EMAIL: RYANH@CDA-ENG.COM  
PH. (515) 369-4400

### SURVEYOR

CIVIL DESIGN ADVANTAGE, LLC  
4121 NW URBANDALE DRIVE  
URBANDALE, IA 50322  
CONTACT: CHARLIE MCLOTHLEN  
EMAIL: CHARLEM@CDA-ENG.COM  
PH. (515) 369-4400

### ARCHITECT

SIMONSON & ASSOCIATES, INC.  
1717 INGERSOLL AVE SUITE #117  
DES MOINES, IA 50319  
CONTACT: STEPHANIE POOLE  
EMAIL: SPOOLE@SIMONSONASSOC.COM  
PH. (515) 440-5628

### SUBMITTAL DATES

FIRST SUBMITTAL: 10/19/2022  
SECOND SUBMITTAL: 01/04/2023

### ZONING

GRAY'S STATION PUD - ZON2017-00087

### EXISTING/ PROPOSED USE

EXISTING: UNDEVELOPED  
PROPOSED: HOUSEHOLD LIVING - 8+ HOUSEHOLDS PER LOT

### DEVELOPMENT SUMMARY

TOTAL SITE AREA: 3.48 ACRES (151,395 SF)  
EXISTING IMPERVIOUS AREA: 0.00 ACRES (0 SF)  
OPEN SPACE CALCULATION:  
TOTAL SITE: = 151,395 SF (3.48 AC.)  
BUILDINGS: = 47,725 SF  
PARKING AREAS: = 66,172 SF  
SIDEWALKS: = 14,844 SF  
OPEN SPACE PROVIDED: = 22,654 SF (15.0%)

UNITS: 84 UNITS (24.14 UNITS PER ACRE)

PARKING:  
STANDARD: 48 SPACES  
GARAGE: 84 SPACES  
ADA: 5 SPACES  
TOTAL: 137 SPACES

### DATE OF SURVEY

08/05/2022

### BENCHMARKS

- CITY BENCHMARK #725: BRASS CAP IN THE NORTHEAST TRAFFIC SIGNAL BASE AT THE SOUTHWEST CORNER OF MARTIN LUTHER KING DRIVE AND SOUTHWEST 11TH STREET. ELEVATION=28.01
- CITY BENCHMARK #5581: BRASS TABLET IN THE CONCRETE WALL 43.6 FEET WEST OF THE EAST FACE NEAR THE SANITARY SEWER PUMP STATION. ELEVATION=25.48

### LEGAL DESCRIPTION

LOT 87, GRAY'S STATION PLAT 5, AN OFFICIAL PLAT IN THE CITY OF DES MOINES, POLK COUNTY, IOWA AND CONTAINING 3.48 ACRES (151,395 S.F.).

### TAX ABATEMENT - SUSTAINABILITY

- BIKE RACKS FOR PUBLIC USE THAT PROVIDE A MINIMUM OF 10% OF THE AUTOMOBILE PARKING PROVIDED.
- 50% INCREASE IN REQUIRED LANDSCAPING.
- PRIMARY ENTRY WITHIN 1/4 MILE OF A DART TRANSIT STOP.
- REDEVELOPMENT OF A PREVIOUSLY DEVELOPED SITE.

### INDEX OF SHEETS

NO.	DESCRIPTION
CO.1	COVER SHEET
C1.0-1.2	DIMENSION PLAN
C2.0-2.6	GRADING PLAN
C3.0-3.2	UTILITY PLAN
C4.0	DETAILS
L02.02 & L02.04	RDG LANDSCAPE PLAN - SITE LAYOUT PLAN
L04.02 & L04.04	RDG LANDSCAPE PLAN - SITE PLANTING PLAN
L05.01	RDG LANDSCAPE PLAN - DETAILS

### GENERAL LEGEND

PROPOSED	EXISTING
PROPERTY BOUNDARY	SANITARY MANHOLE
SECTION LINE	WATER VALVE BOX
CENTER LINE	FIRE HYDRANT
RIGHT OF WAY	WATER CURB STOP
BUILDING SETBACK	WELL
PERMANENT EASEMENT	STORM SEWER MANHOLE
TEMPORARY EASEMENT	STORM SEWER SINGLE INTAKE
TYPE SW-501 STORM INTAKE	STORM SEWER DOUBLE INTAKE
TYPE SW-502 STORM INTAKE	FLARED END SECTION
TYPE SW-503 STORM INTAKE	DECIDUOUS TREE
TYPE SW-505 STORM INTAKE	CONIFEROUS TREE
TYPE SW-508 STORM INTAKE	DECIDUOUS SHRUB
TYPE SW-512 STORM INTAKE	CONIFEROUS SHRUB
TYPE SW-513 STORM INTAKE	ELECTRIC POWER POLE
TYPE SW-401 STORM MANHOLE	GUY ANCHOR
TYPE SW-402 STORM MANHOLE	STREET LIGHT
FLARED END SECTION	POWER POLE W/ TRANSFORMER
TYPE SW-301 SANITARY MANHOLE	ELECTRIC BOX
STORM/SANITARY CLEANOUT	ELECTRIC TRANSFORMER
WATER VALVE	ELECTRIC MANHOLE OR VAULT
FIRE HYDRANT ASSEMBLY	TRAFFIC SIGN
SIGN	TELEPHONE JUNCTION BOX
DETECTABLE WARNING PANEL	TELEPHONE MANHOLE/VAULT
WATER CURB STOP	TELEPHONE POLE
SANITARY SEWER	GAS VALVE BOX
SANITARY SERVICE	CABLE TV JUNCTION BOX
STORM SEWER	CABLE TV MANHOLE/VAULT
STORM SERVICE	MAIL BOX
WATERMAIN WITH SIZE	BENCHMARK
WATER SERVICE	SOIL BORING
SAWCUT (FULL DEPTH)	UNDERGROUND TV CABLE
SILT FENCE	GAS MAIN
USE AS CONSTRUCTED	FIBER OPTIC
MINIMUM PROTECTION ELEVATION	UNDERGROUND TELEPHONE
	OVERHEAD ELECTRIC
	UNDERGROUND ELECTRIC
	FIELD TILE
	SANITARY SEWER W/ SIZE
	STORM SEWER W/ SIZE
	WATER MAIN W/ SIZE



### UTILITY WARNING

ANY UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY AND RECORDS OBTAINED BY THIS SURVEYOR. THE SURVEYOR MAKES NO GUARANTEE THAT THE UTILITIES SHOWN COMPRISE ALL THE UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UTILITIES SHOWN ARE IN THE EXACT LOCATION SHOWN.



**CIVIL DESIGN ADVANTAGE**

4121 NW URBANDALE DRIVE, URBANDALE, IA 50322  
PH: (515) 369-4400

PROJECT NO. 2205.350

### SITE PLAN APPROVAL:

APPROVED  APPROVED WITH CONDITIONS - SEE EXHIBIT "A"

IN ACCORDANCE WITH SECTION 155-9, 2019 DES MOINES MUNICIPAL CODE, AS AMENDED.

NO CHANGES TO THIS PLAN UNLESS APPROVED IN WRITING FROM THE DEVELOPERS SERVICES DIRECTOR.

DEVELOPMENT SERVICES DIRECTOR: \_\_\_\_\_ DATE: \_\_\_\_\_

THE PROJECT REQUIRES AN IOWA NPDES PERMIT #2 AND CITY OF DES MOINES GRADING PERMIT. CIVIL DESIGN ADVANTAGE WILL PROVIDE THE PERMITS AND THE INITIAL STORM WATER POLLUTION PREVENTION PLAN (SWPPP) FOR THE CONTRACTORS USE DURING CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR UPDATING THE SWPPP THROUGHOUT CONSTRUCTION AND MEETING LOCAL, STATE AND FEDERAL REQUIREMENTS.

ALL CONSTRUCTION MATERIALS, DUMPSTERS, DETACHED TRAILERS OR SIMILAR ITEMS ARE PROHIBITED ON PUBLIC STREETS OR WITHIN THE PUBLIC R.O.W.

THE 2022 EDITION OF THE SDAS STANDARD SPECIFICATIONS, THE PUBLIC RIGHTS-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG) AND ALL CITY SUPPLEMENTALS, IF APPLICABLE, SHALL APPLY TO ALL WORK ON THIS PROJECT UNLESS OTHERWISE NOTED.

I HEREBY CERTIFY THAT THIS ENGINEERING DOCUMENT WAS PREPARED BY ME OR UNDER MY DIRECT PERSONAL SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF IOWA.

RYAN HARDISTY, P.E. DATE: \_\_\_\_\_  
DESIGN NUMBER: 22011  
AN ENGINE RECORD DATE IS DECEMBER 31, 2023  
PAGES OR SHEETS COVERED BY THIS SEAL:  
ALL C SERIES SHEETS



DATE PLOTTED: 01/04/2023 10:00 AM. PLOTTER: HP PLOTTER. PLOT SIZE: 36" X 48" (1118 X 1219 mm). PLOT SCALE: 1" = 100'.

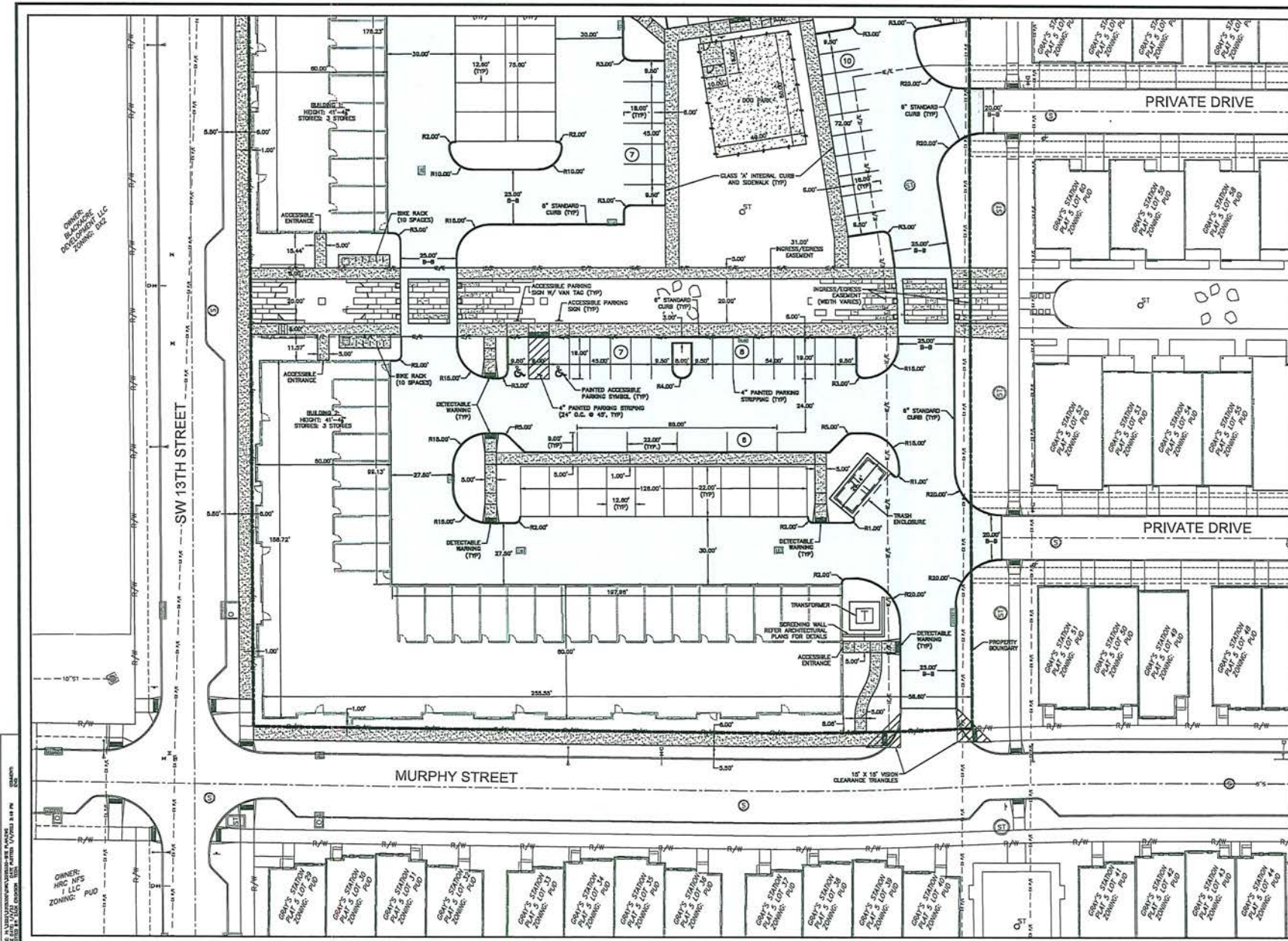












OWNER: HRC NPS I LLC ZONING: PUD

OWNER: BLACKBURN DEVELOPMENT, LLC ZONING: S2

SW 13TH STREET

MURPHY STREET

PRIVATE DRIVE

PRIVATE DRIVE



DATE	REVISIONS
10/19/2020	SECOND SUBMITTAL
10/19/2020	FIRST SUBMITTAL

4121 NW URBANDALE DRIVE  
 URBANDALE, IA 50002  
 PHONE: (515) 588-4400



**GRAY'S STATION TELUS**  
 DIMENSION PLAN

C1.2

2205.350

TECH:

ENGINEER:

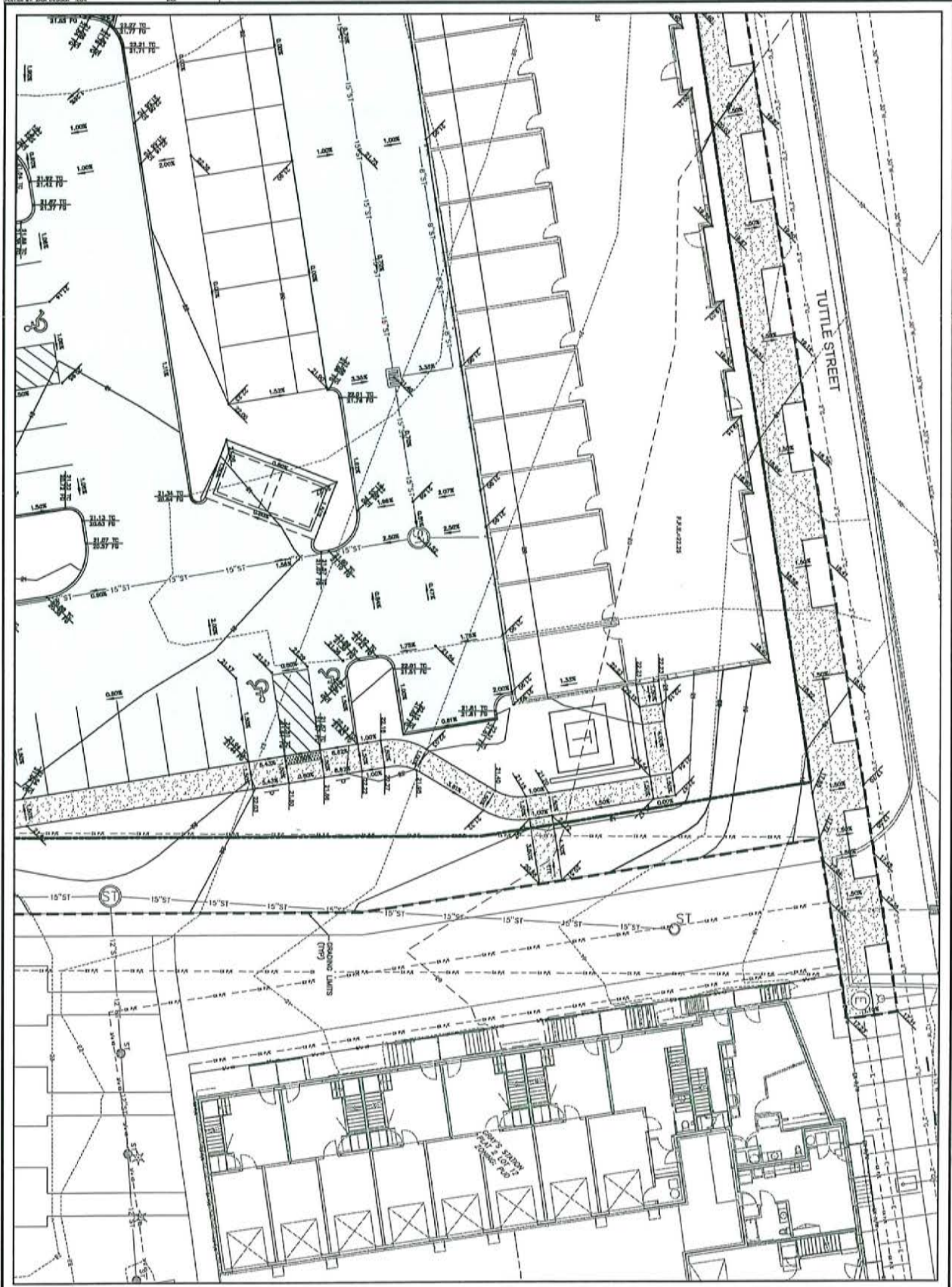
DES MOINES, IOWA

CIVIL DESIGN ADVANTAGE









**C2.2**  
**GRAY'S STATION TELUS**  
**GRADING PLAN**



4121 NW URBANDALE DRIVE  
 URBANDALE, IA 50322  
 PHONE: (515) 959-4400

REVISIONS	DATE
SECOND SUBMITTAL	9/16/2022
FIRST SUBMITTAL	10/13/2022

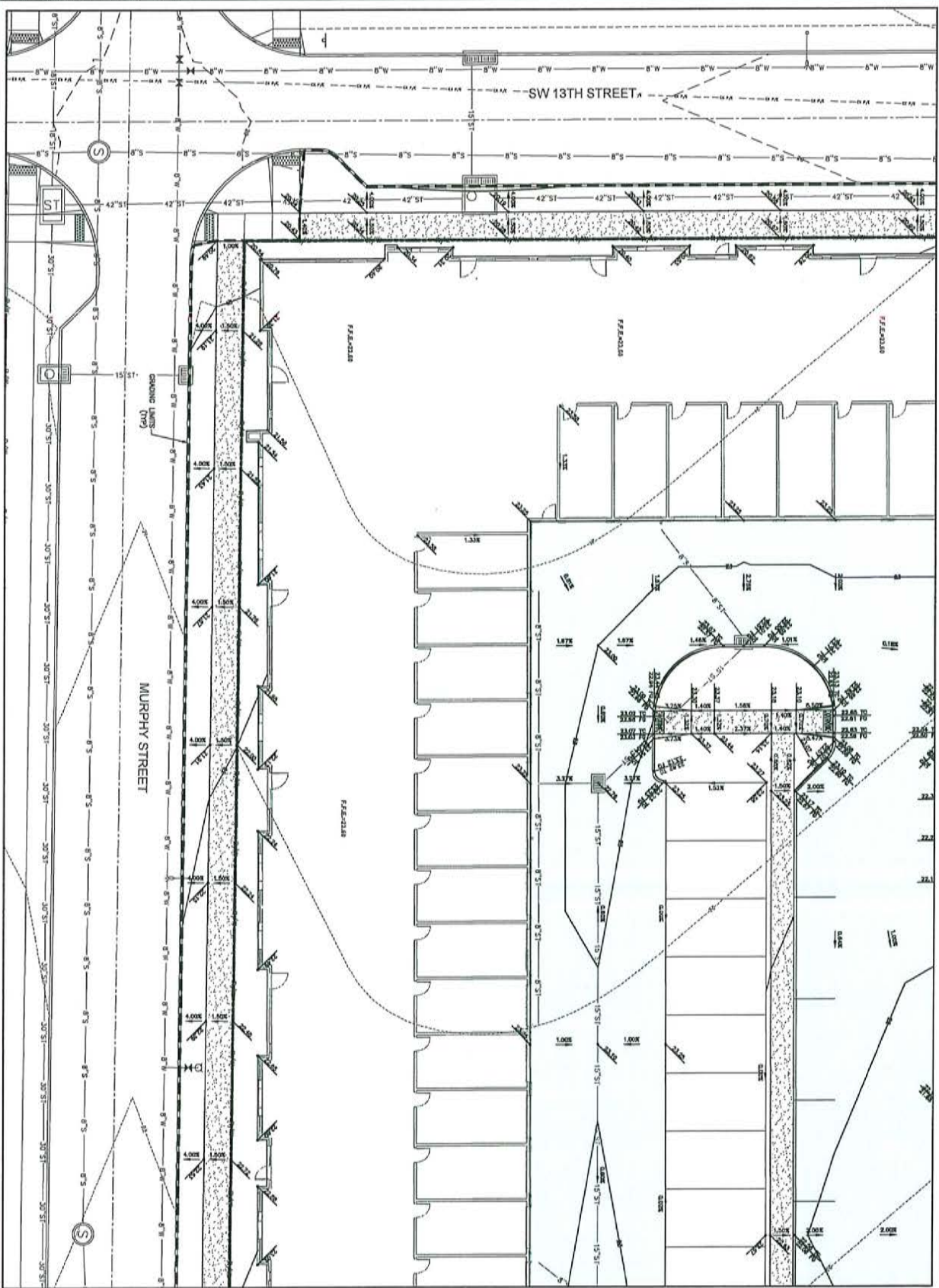




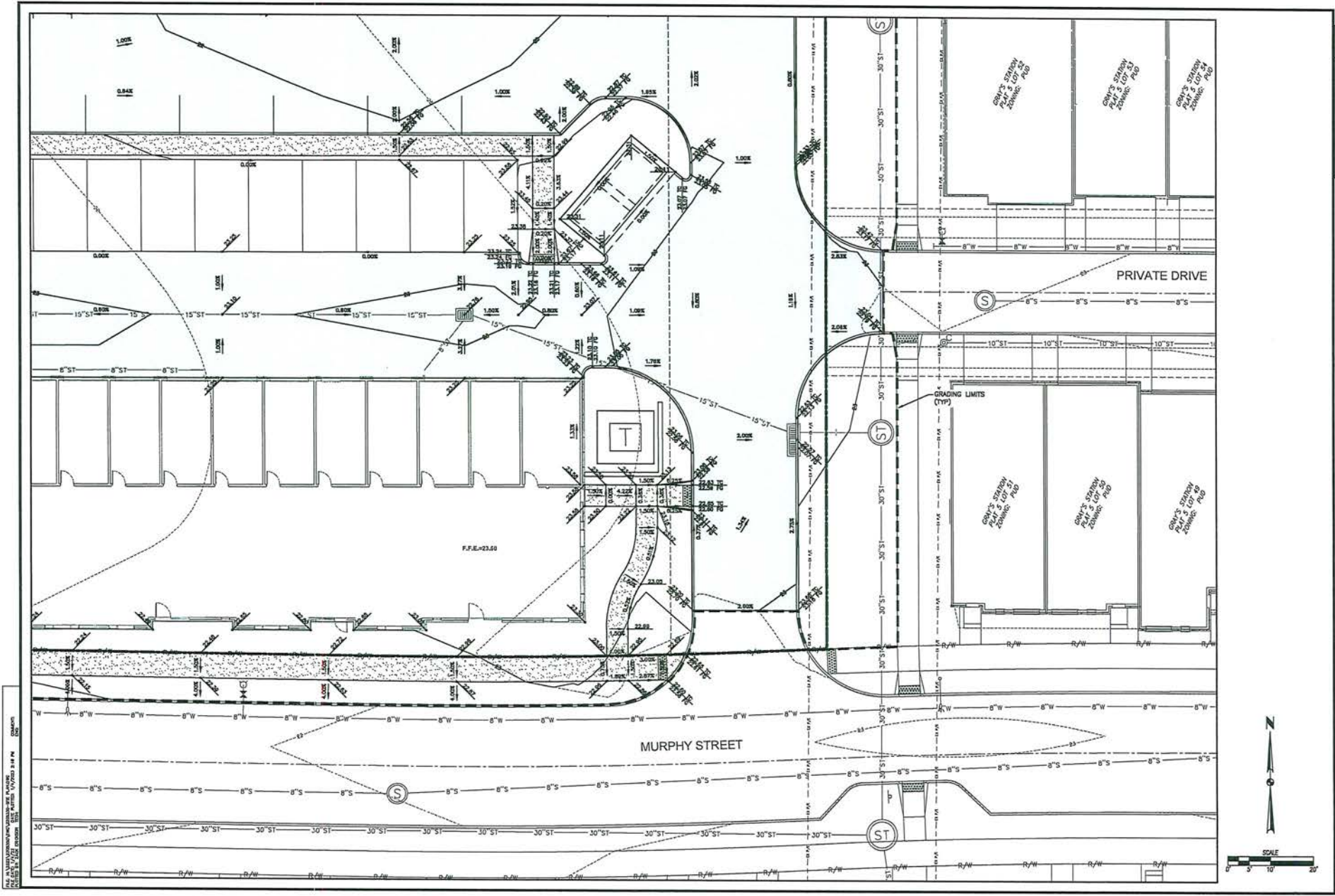












REVISIONS	DATE

4121 NW URBANDALE DRIVE  
 URBANDALE, IA 50322  
 PHONE: (515) 389-4400



**GRAY'S STATION TELUS**  
 GRADING PLAN

**C2.6**  
 2205.350

TECH: \_\_\_\_\_  
 ENGINEER: \_\_\_\_\_

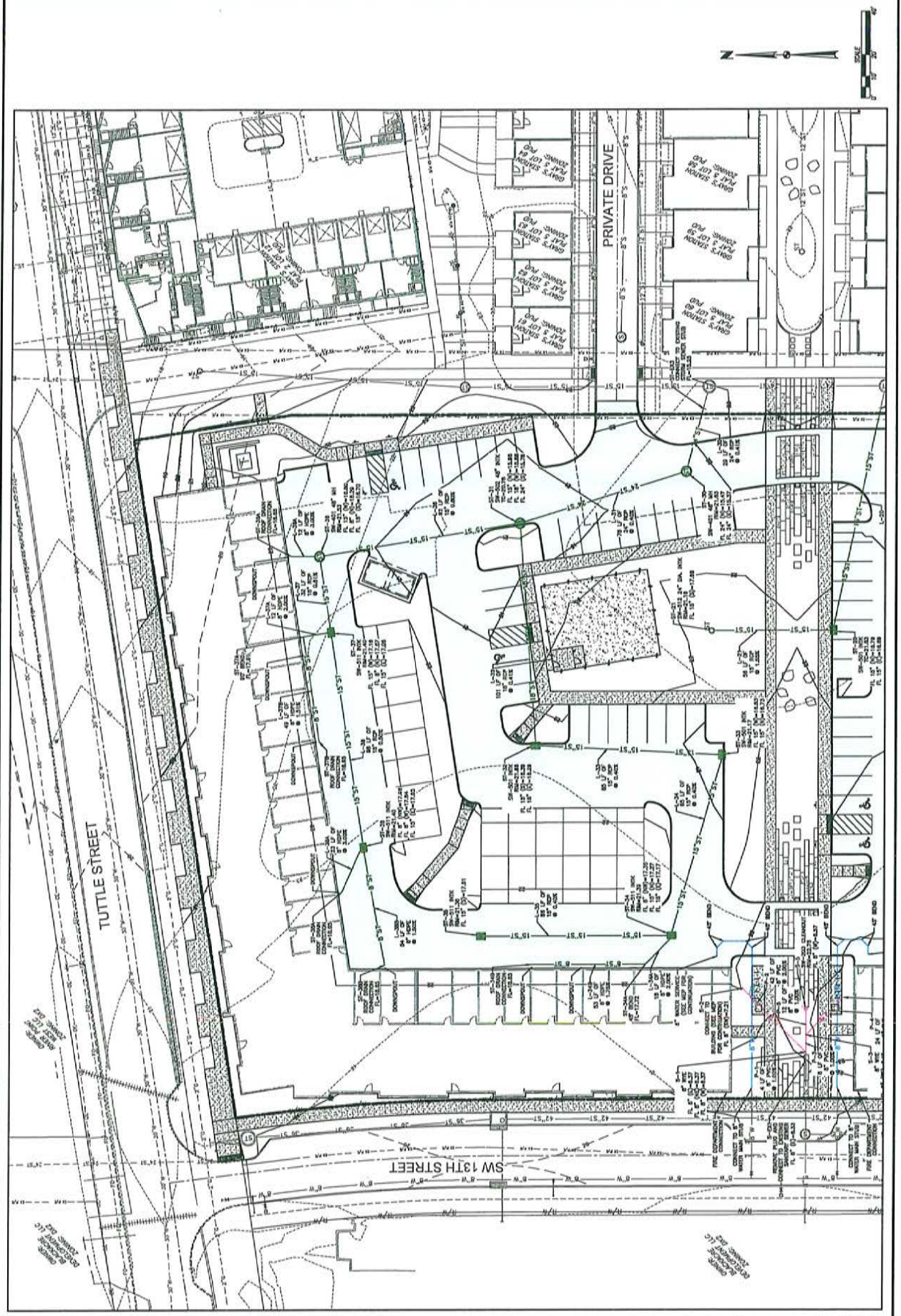
DES MOINES, IOWA

CIVIL DESIGN ADVANTAGE

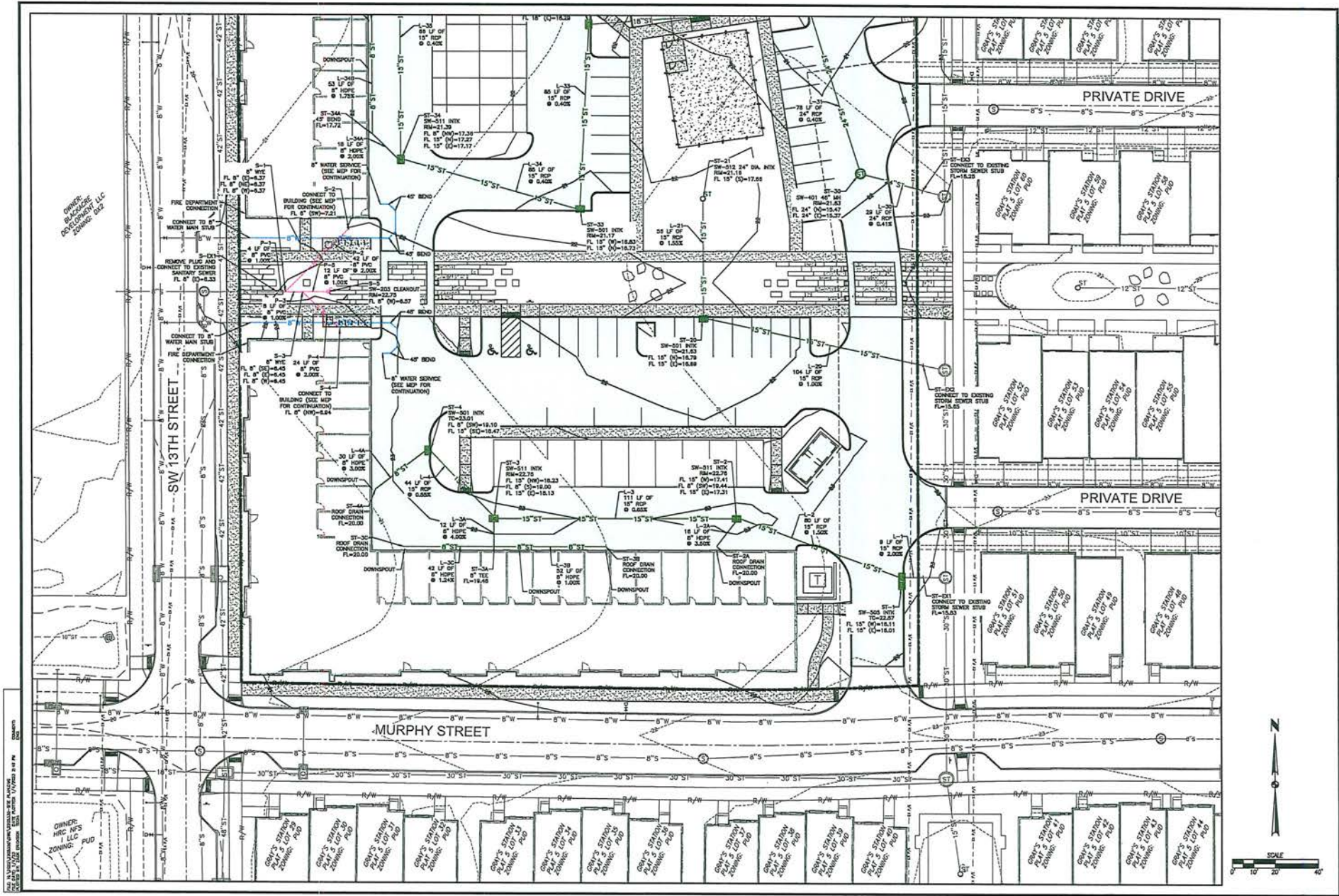
DATE: \_\_\_\_\_  
 SECOND SUBMITTAL: \_\_\_\_\_  
 FIRST SUBMITTAL: \_\_\_\_\_

10/19/2021









DATE	REVISIONS

4121 NW URBANDALE DRIVE  
 URBANDALE, IA 50322  
 PHONE: (515) 369-4400

TECH: \_\_\_\_\_  
 ENGINEER: \_\_\_\_\_



DES MOINES, IOWA  
 CIVIL DESIGN ADVANTAGE

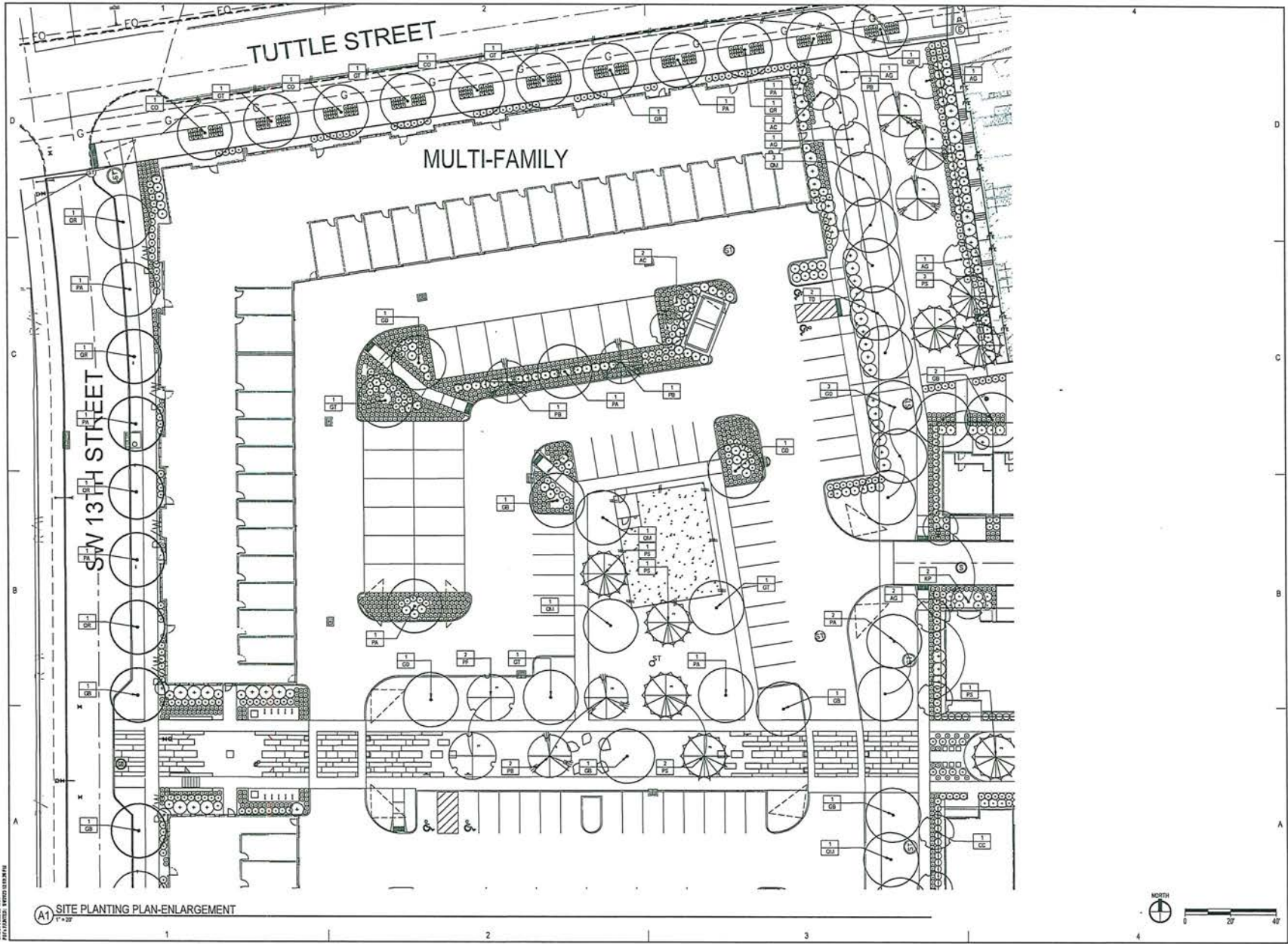
**GRAY'S STATION TELUS**  
 UTILITY PLAN

**C3.2**  
 2205.360









(A1) SITE PLANTING PLAN-ENLARGEMENT

SECTION OF PLANTING PLAN ENLARGEMENT

GRAY'S STATION PLAT 5 PRELIMINARY - NOT FOR CONSTRUCTION DESIGN DEVELOPMENT

PROJECT NO.	1000000000
DATE	01/15/2018
PROJECT NAME	GRAY'S STATION PHASE 2
CLIENT	HUBBELL REALTY
ARCHITECT	RDG ARCHITECT
LANDSCAPE ARCHITECT	RDG ARCHITECT

HUBBELL REALTY

ARCHITECT  
RDG ARCHITECT  
1000000000  
01/15/2018  
LANDSCAPE ARCHITECT  
RDG ARCHITECT  
1000000000  
01/15/2018

RDG  
PLANNING & DESIGN

L04.02

SITE PLANTING PLAN

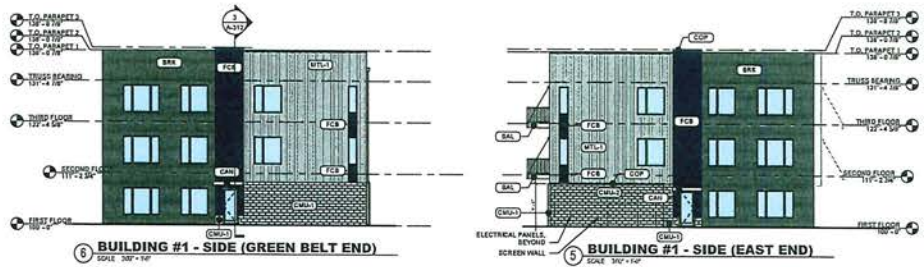






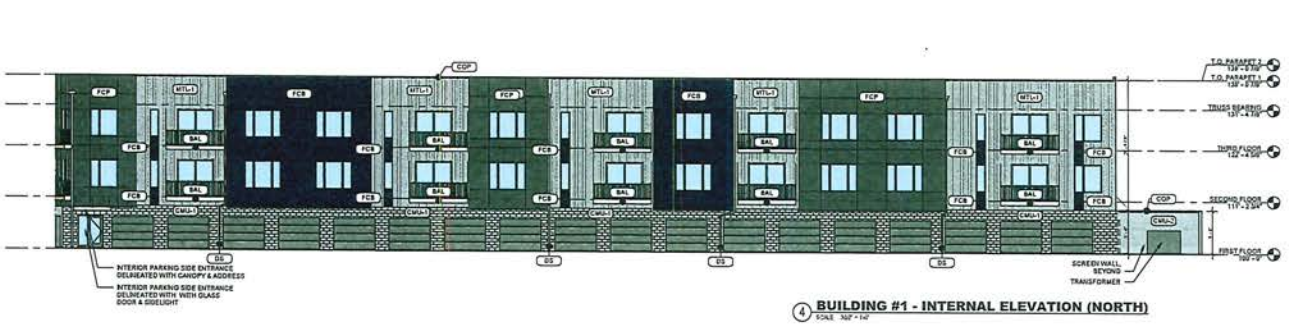
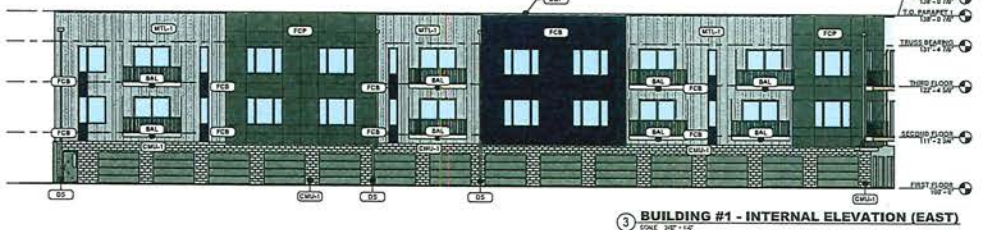
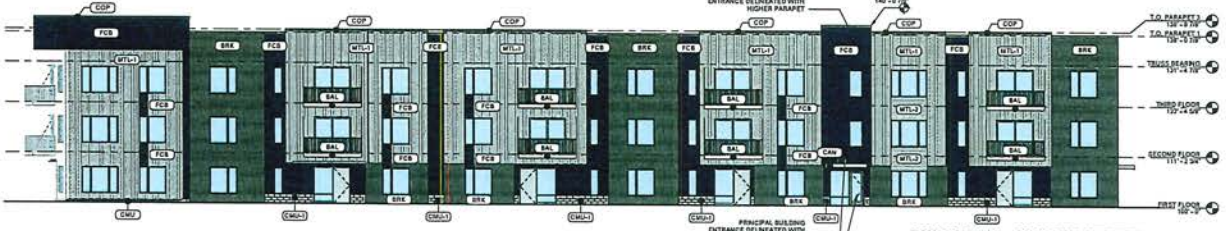






EXTERIOR MATERIAL LEGEND	
MAT. LABEL	MATERIAL DESCRIPTION
BAL	PRE-FABRICATED BALCONY, EASY MAINTENANCE
BKX	FULL DEPTH MODULAR BRICK, NATURAL VARIATION, COLOR-GRAY
CMU	8\"/>
CMW	8\"/>
CP	PRE-FABRICATED CONCRETE PANELS, NATURAL VARIATION, COLOR-GRAY
CPW	PRE-FABRICATED CONCRETE PANELS, NATURAL VARIATION, COLOR-WHITE
CS	PRE-FABRICATED CONCRETE SLAB, NATURAL VARIATION, COLOR-WHITE
FCB	PRE-FABRICATED CONCRETE PANELS, NATURAL VARIATION, COLOR-GRAY
FCW	PRE-FABRICATED CONCRETE PANELS, NATURAL VARIATION, COLOR-WHITE
FT	PRE-FABRICATED CONCRETE PANELS, NATURAL VARIATION, COLOR-GRAY
FTW	PRE-FABRICATED CONCRETE PANELS, NATURAL VARIATION, COLOR-WHITE
MTL	METAL PANEL, CONCRETE TO MATCH WITH VERTICAL REVEAL, COLOR-GRAY
MTLW	METAL PANEL, CONCRETE TO MATCH WITH HORIZONTAL REVEAL, COLOR-WHITE
TR	TRIM, ALUMINUM STAINLESS STEEL FINISH, CLEAR ANODIZED FINISH
TRW	TRIM, ALUMINUM STAINLESS STEEL FINISH, CLEAR ANODIZED FINISH
VE	8\"/>

- ### GENERAL EXTERIOR ELEVATION NOTES
- REFER TO ADDITIONAL NOTES AND REQUIREMENTS ON ALL OTHER DOCUMENTS AND OTHER DISCIPLINE DOCUMENTS.
  - ALL CORNER JOINTS & SERVICE LINES SHALL BE GROUPED TOGETHER IN A NEAT & ORDERLY MANNER AND BUSHED THROUGH THE WALL AT THE CORNER POINT AND ARE NOT TO RUN OVER THE TOP OF PARAPET. ALL OTHER CORNER JOINTS & SERVICE LINES FOR EXTERIOR FINISHES & EQUIPMENT SHALL BE FULLY CONCEALED WITHIN THE BUILDING CONSTRUCTION.
  - ALL VERTICAL TRANSITIONS BETWEEN DIFFERENT MATERIALS SHALL BE FLARED CONTIGUOUS FROM UP SIDING OVER MATERIALS LITTING OVER TO FACE OF LOWER MATERIAL WITH 8\"/>



- ### MISC EXTERIOR MATERIAL FINISHES
- TYPICAL U.O.A.
- EXPOSED CONCRETE: GRIND SMOOTH & BUSHED WITH CEMENT SLURRY
  - EXPOSED BRICK: CLEANED & PAINTED TO MATCH ADJACENT MATERIAL ABOVE
  - PAVING & DRIVEWAY: GALVANNEZED FRAMES WHITE
  - CONCRETE: GRIND SMOOTH & BUSHED WITH CEMENT SLURRY
  - CAULKS & SEALANTS: COLOR MATCHED TO ADJACENT MATERIAL
  - HOLDING METAL HAND FRAMES: PAINTED TO MATCH ADJACENT MATERIAL
  - OVERHEAD PANEL DOORS: METAL PRE-FINISHED TO MATCH ADJACENT MATERIAL
  - CONCRETE: METAL PRE-FINISHED TO MATCH GRAY
  - LIGHT FIXTURES: METAL PRE-FINISHED HOURS TO MATCH ADJ. OPENING FRAMING
  - MECHANICAL EQUIPMENT: SCREENED BY PARAPET
  - EXTERIOR UTILITY EQUIPMENT: IF ALLOWED, PAINTED TO MATCH ADJACENT MATERIAL
  - TRIM SIZES: TYP. U.O.A.
  - WINDOW & DOOR: 1-1/2\"/>

MATERIAL CALCULATIONS (CITY OF DES MOINES)		
EDGE ELEVATION (GREEN BELT)	2,098.28 SF TOTAL	W/O PENETRATIONS: 1,584.12 SF
GLASS	375.22 SF	18.0%
BRICK	116.22 SF	5.6%
FIBER CEMENT	78.54 SF	3.8%
METAL PANEL	891.46 SF	42.5%
CMU	250.81 SF	12.0%
STREET FACIUM TRANSPARENCY PERCENTAGE = 24%		
INTERNAL ELEVATION (EAST)	4,235.54 SF TOTAL	W/O PENETRATIONS: 4,283.89 SF
GLASS	1,388.88 SF	32.8%
BRICK	1,818.13 SF	43.0%
FIBER CEMENT	820.00 SF	19.4%
METAL PANEL	793.89 SF	18.8%
CMU	-	-
EDGE ELEVATION (EAST END)	2,884.31 SF TOTAL	W/O PENETRATIONS: 1,884.12 SF
GLASS	375.22 SF	13.0%
BRICK	84.24 SF	2.9%
FIBER CEMENT	217.24 SF	7.5%
METAL PANEL	822.82 SF	28.5%
CMU	290.81 SF	10.1%
TUTTLE STREET (STREET FACIUM)	15,884.79 SF TOTAL	W/O PENETRATIONS: 7,884.12 SF
GLASS	2,588.22 SF	16.3%
BRICK	2,482.31 SF	15.6%
FIBER CEMENT	2,217.24 SF	14.0%
METAL PANEL	3,324.02 SF	20.9%
CMU	1,907.02 SF	12.0%
STREET FACIUM TRANSPARENCY PERCENTAGE = 25%		
INTERNAL ELEVATION (NORTH)	3,834.14 SF TOTAL	W/O PENETRATIONS: 1,684.12 SF
GLASS	2,022.22 SF	52.8%
BRICK	82.24 SF	2.1%
FIBER CEMENT	2,217.24 SF	57.9%
METAL PANEL	2,228.02 SF	58.1%
CMU	584.22 SF	15.2%

WINDOW RECESS BY MATERIAL:

BRICK: 1-1/2\"/>

**SIMONSON**  
Architectural Firm

**HUBBELL REALTY COMPANY**

**TELUS CONDOS**  
DES MOINES, IA

DATE: 10/20/2023

PROJECT: TELUS CONDOS

JOB NO. 21162

PHASE: SP

DATE: 10/20/2023

BLDG #1 - EXTERIOR ELEVATIONS - COLORED

DATE: 10/20/2023

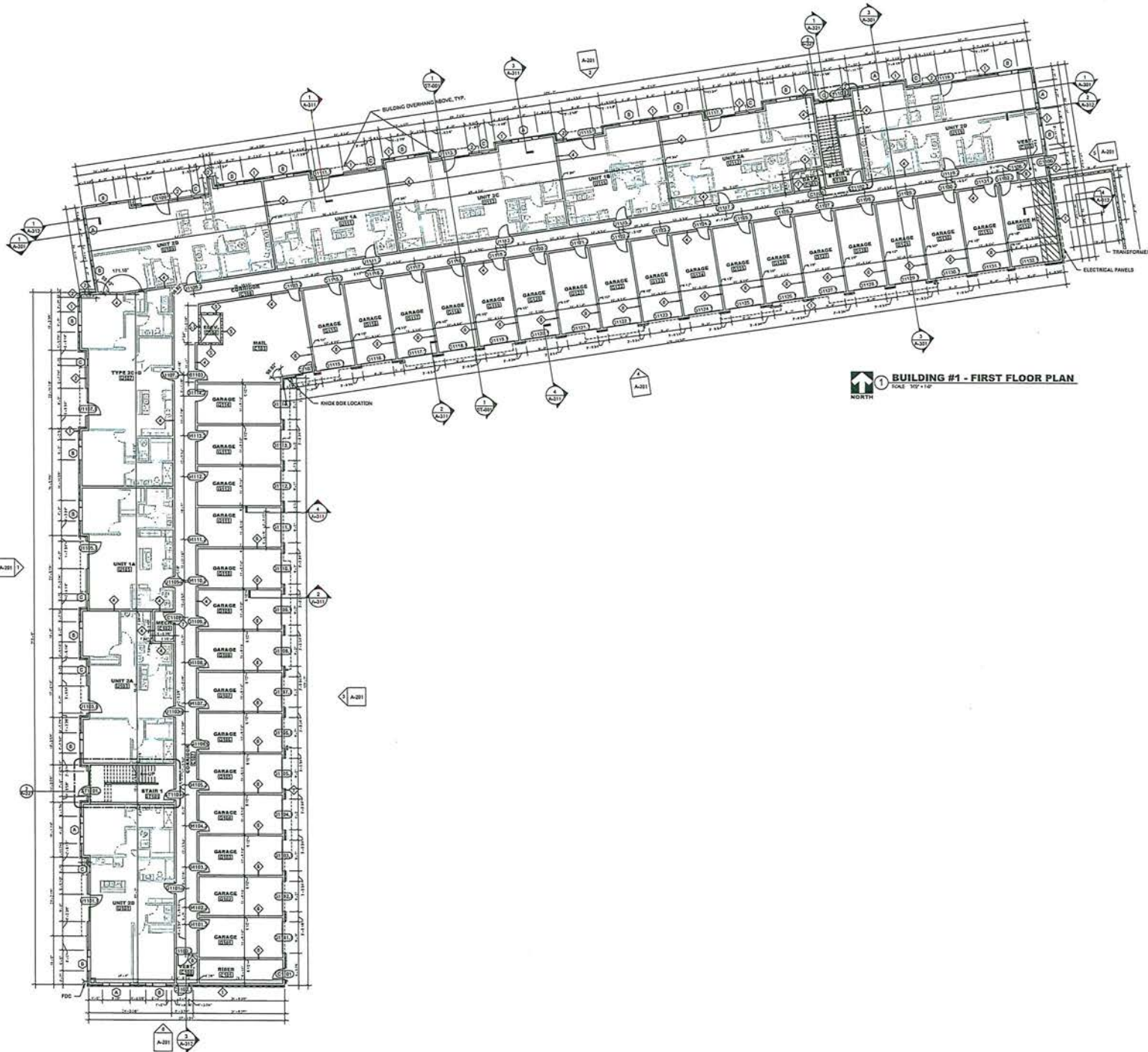
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**BUILDING #1 - FIRST FLOOR PLAN**  
SCALE: 1/8" = 1'-0"

- GENERAL FLOOR PLAN NOTES**
1. REFER TO ADDITIONAL NOTES AND REQUIREMENTS ON ALL OTHER DOCUMENTS AND OTHER SPECIFICATIONS DOCUMENTS.
  2. ALL SITE INFO, FURNITURE, FEATURES AND EQUIPMENT SHOWN SHOULD BE CONSIDERED CONCEPTUAL AND MAY HAVE TO BE PROVIDED FOR COORDINATION PURPOSES ONLY. REFER TO C.A.I., FIRE SUPPRESSION, PLUMBING, MECHANICAL, ELECTRICAL, COMMUNICATIONS, ELECTRONIC SAFETY & SECURITY DOCUMENTS FOR CONSULTANTS FOR SPECIFIC DESIGN INFORMATION & REQUIREMENTS.
  3. DIMENSIONS ARE TO FACE OF FINISH MATERIAL OR CENTER OF STRUCTURE UNLESS OTHERWISE SPECIFIED.
  4. ALL CONDUITS, PIPES, WIRING, ROUGH-IN, ETC. SHOULD BE HELD AS HIGH AS POSSIBLE TO THE ROOF OR FLOOR DECK OR SHOULD BE KEPT WITHIN INTERIOR WALL CHASES. WORKING SHALL BE BELOW THE BOTTOM EDGE OF MAIN BEARING FRAMES. NOTHING SHALL BE EXPOSED IN FINISHED SPACES. WORKING SHALL PREVENT A FINISH CONTIGUOUS FINISH WALL OR CEILING AS NOTED ON THE DRAWINGS AND NO PLUMBING RAN IN EXTERIOR WALLS UNLESS PRIOR APPROVAL IS GRANTED BY THE ARCHITECT OR OWNER. PLACE WORK COORDINATED BETWEEN TRADES IS REQUIRED.
  5. INITIAL VERTICAL EXPANSION JOINT SHALL BE LOCATED AT COLUMN/BEAM LINES AND AT THE HEAD OF SOCR AWANGSPRINGS OR AS OTHERWISE NOTED OR WHEN REQUIRED PER STANDARD MATERIAL PRACTICE TO REDUCE STRESS CONCENTR. NO CONTROL JOINT/EXPANSION JOINT IN LOAD BEARING AREA. STAY AWAY 2'-0" MIN.
  6. CARE & SEAL ALL CONTROL/EXPANSION, SAWCUT JOINTS AT ALL INTERIOR & EXTERIOR MASONRY & CONCRETE.
  7. PROVIDE LOWER LEVEL AND/OR UNDERFLOOR DRAINAGE SYSTEM TO REDUCE MOISTURE UPLIFT AND PROTECT AGAINST MOISTURE SEEPAGE. PROVIDE FOUNDATION WALL DRAINAGE SYSTEM TO REDUCE LATERAL PRESSURES AND PROTECT AGAINST MOISTURE SEEPAGE. BOTH SYSTEMS SHALL OUTLET TO APPROPRIATE RELEAS.
  8. PROVIDE POSITIVE DRAINAGE OF SURFACE WATER AWAY FROM BUILDING WITHOUT PONING OF WATER ADJACENT TO BUILDING OR ON PAVEMENTS.

**BUILDING #1 UNIT MIX:**

**FIRST FLOOR**

UNIT 1A	1
UNIT 1B	1
UNIT 1C	1
UNIT 1D	1
UNIT 1E	1
UNIT 1F	1
UNIT 1G	1
UNIT 1H	1
UNIT 1I	1
UNIT 1J	1
UNIT 1K	1
UNIT 1L	1
UNIT 1M	1
UNIT 1N	1
UNIT 1O	1
UNIT 1P	1
UNIT 1Q	1
UNIT 1R	1
UNIT 1S	1
UNIT 1T	1
UNIT 1U	1
UNIT 1V	1
UNIT 1W	1
UNIT 1X	1
UNIT 1Y	1
UNIT 1Z	1
TOTAL	20 (11 REG GARAGES, 1 ADA GARAGE)

**SECOND FLOOR**

UNIT 2A	1
UNIT 2B	1
UNIT 2C	1
UNIT 2D	1
UNIT 2E	1
UNIT 2F	1
UNIT 2G	1
UNIT 2H	1
UNIT 2I	1
UNIT 2J	1
UNIT 2K	1
UNIT 2L	1
UNIT 2M	1
UNIT 2N	1
UNIT 2O	1
UNIT 2P	1
UNIT 2Q	1
UNIT 2R	1
UNIT 2S	1
UNIT 2T	1
UNIT 2U	1
UNIT 2V	1
UNIT 2W	1
UNIT 2X	1
UNIT 2Y	1
UNIT 2Z	1
TOTAL	26

**THIRD FLOOR**

UNIT 3A	1
UNIT 3B	1
UNIT 3C	1
UNIT 3D	1
UNIT 3E	1
UNIT 3F	1
UNIT 3G	1
UNIT 3H	1
UNIT 3I	1
UNIT 3J	1
UNIT 3K	1
UNIT 3L	1
UNIT 3M	1
UNIT 3N	1
UNIT 3O	1
UNIT 3P	1
UNIT 3Q	1
UNIT 3R	1
UNIT 3S	1
UNIT 3T	1
UNIT 3U	1
UNIT 3V	1
UNIT 3W	1
UNIT 3X	1
UNIT 3Y	1
UNIT 3Z	1
TOTAL	26

**TOTALS**

UNIT 1A	10
UNIT 1B	10
UNIT 1C	10
UNIT 1D	10
UNIT 1E	10
UNIT 1F	10
UNIT 1G	10
UNIT 1H	10
UNIT 1I	10
UNIT 1J	10
UNIT 1K	10
UNIT 1L	10
UNIT 1M	10
UNIT 1N	10
UNIT 1O	10
UNIT 1P	10
UNIT 1Q	10
UNIT 1R	10
UNIT 1S	10
UNIT 1T	10
UNIT 1U	10
UNIT 1V	10
UNIT 1W	10
UNIT 1X	10
UNIT 1Y	10
UNIT 1Z	10
TOTAL	17 1-BEDROOMS = 33%

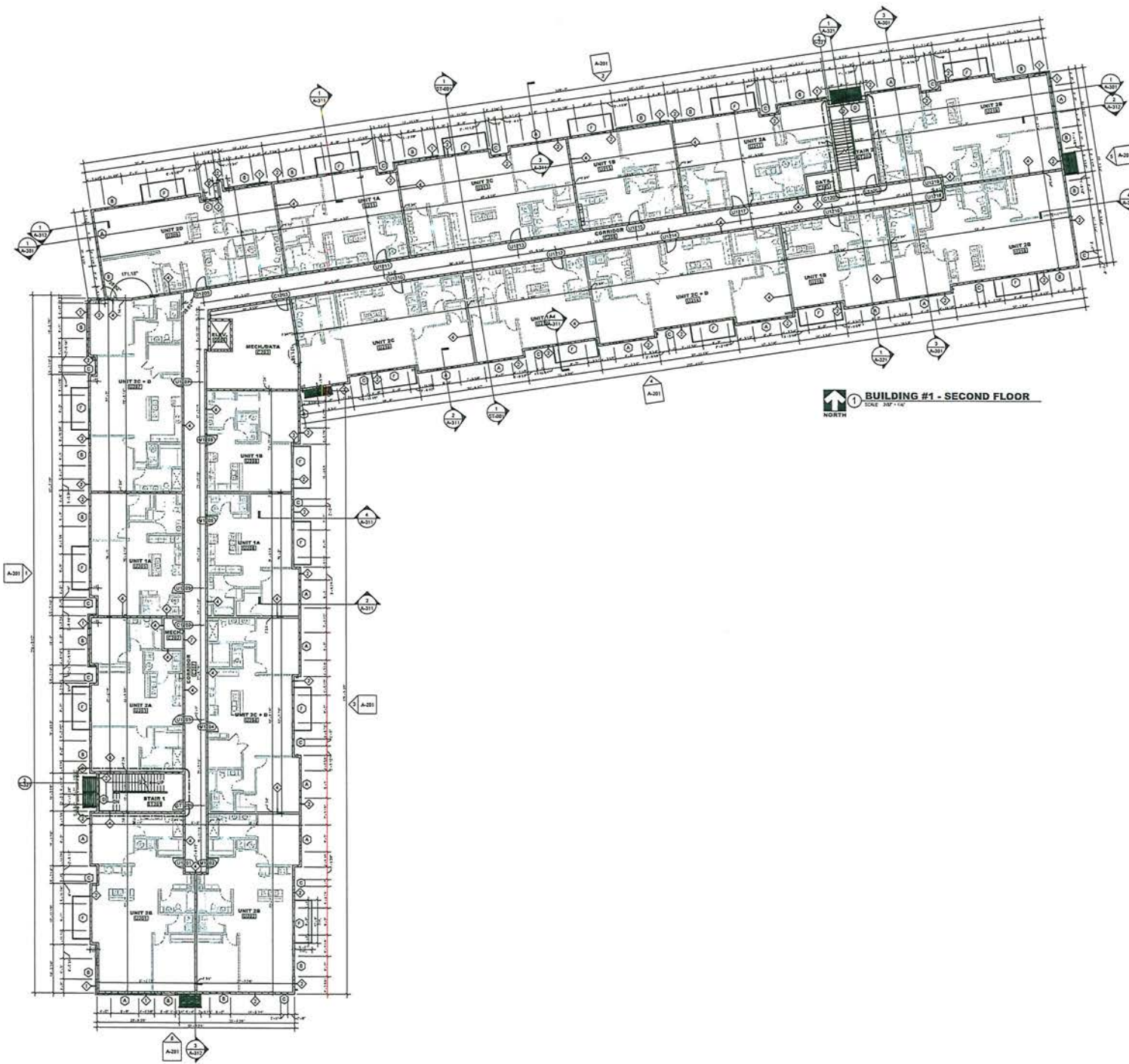


**TELUS CONDOS, IA**  
DES MOINES, IA

DATE	10-20-2021
BY	10-20-2021
REVISION	
DESCRIPTION	
DATE	
BY	
REVISION	
DESCRIPTION	
DATE	
BY	
REVISION	
DESCRIPTION	

JAN 16, 2022 PNL 404  
21 (62) SP  
DRAWN BY  
**BUILDING #1 - FIRST FLOOR PLAN**  
CHECKED BY  
DATE PLOTTED 1/11/2022 11:47 AM





**BUILDING #1 - SECOND FLOOR**  
 SCALE: 1/8" = 1'-0"  
 NORTH

**GENERAL FLOOR PLAN NOTES**

1. REFER TO ADDITIONAL NOTES AND REQUIREMENTS ON ALL OTHER DOCUMENTS AND OTHER DISCIPLINE DOCUMENTS.
2. ALL SEE INFO, FURNITURE, FIXTURES AND EQUIPMENT SHOWN SHOULD BE CONSIDERED CONCEPTUAL AND MAY HAVE BE PROVIDED FOR COORDINATION PURPOSES ONLY. REFER TO CIVIL, FIRE SUPPRESSION, PLUMBING, MECHANICAL, ELECTRICAL, COMMUNICATIONS, ELECTRONIC SAFETY & SECURITY DOCUMENTS FOR CONSTRUCTION SPECIFICATIONS AND REQUIREMENTS.
3. DIMENSIONS ARE TO FACE OF FINISH MATERIAL OR CENTER OF STRUCTURE UNLESS OTHERWISE SPECIFIED.
4. ALL CONDITES, PIPES, WIRING, RIGGING, ETC. SHOULD BE HELD AS TIGHT AS POSSIBLE TO THE ROOF OR FLOOR DECK OR SHOULD BE KEPT WITHIN INTERIOR WALL CAVITIES. NOTHING SHALL BE BEYOND THE BOTTOM EDGE OF UNIT BUILDING FRAMES. NOTHING SHALL BE EXPOSED IN FINISHED SPACES. NOTHING SHALL PREVENT A SMOOTH CONTINUOUS FINISH WALL OR CEILING AS NOTED ON THE DRAWING AND NO NUMBERING IN EXTERIOR WALLS UNLESS PRIOR APPROVAL BETWEEN TRADES IS REQUIRED.
5. INSTALL VERTICAL EXPANSION JOINT ANIMAL AT COLUMN/BEAM LINES AND AT THE HEAD OF DOOR JAMBSPARTINGS OR AS OTHERWISE NOTED OR INDICATED REQUIRED PER STANDARD MATERIAL PRACTICE TO REDUCE STRESS TRACKING, NO CONTROL, JOINT/EXPANSION JOINT IN LOAD BEARING AREAS, STAY AWAY 2'-0" MIN.
6. CARVE & SEAL ALL CONTROL-EXPANSION, SAWCUT JOINTS AT ALL INTERIOR & EXTERIOR MASONRY & CONCRETE.
7. PROVIDE LOWER LEVEL ANCHOR UNDER FLOOR DRAINAGE SYSTEM TO REDUCE HYDROSTATIC UPLIFT AND PROTECT AGAINST SEEPAGE. PROVIDE POSITIVE DRAINAGE SYSTEM TO REDUCE LATERAL PRESSURES AND PROTECT AGAINST SEEPAGE. BOTH SYSTEMS SHALL OUTLET TO APPROPRIATE RELIEF.
8. PROVIDE POSITIVE DRAINAGE OF SURFACE WATER AWAY FROM BUILDING WITHOUT PONDING OF WATER ADJACENT TO BUILDING OR ON PAVEMENTS.

**BUILDING #1 UNIT MIX:**

FIRST FLOOR	
UNIT 1A	2
UNIT 1B	1
UNIT 1C	1
UNIT 1D	1
UNIT 1E	1
UNIT 1F	1
UNIT 1G	1
UNIT 1H	1
UNIT 1I	1
UNIT 1J	1
TOTAL	13 (01 REG GARAGES, 1 ADA GARAGES)

SECOND FLOOR	
UNIT 2A	4
UNIT 2B	2
UNIT 2C	2
UNIT 2D	2
UNIT 2E	2
UNIT 2F	2
UNIT 2G	2
UNIT 2H	2
UNIT 2I	2
UNIT 2J	2
TOTAL	28

THIRD FLOOR	
UNIT 3A	4
UNIT 3B	2
UNIT 3C	2
UNIT 3D	2
UNIT 3E	2
UNIT 3F	2
UNIT 3G	2
UNIT 3H	2
UNIT 3I	2
UNIT 3J	2
TOTAL	28

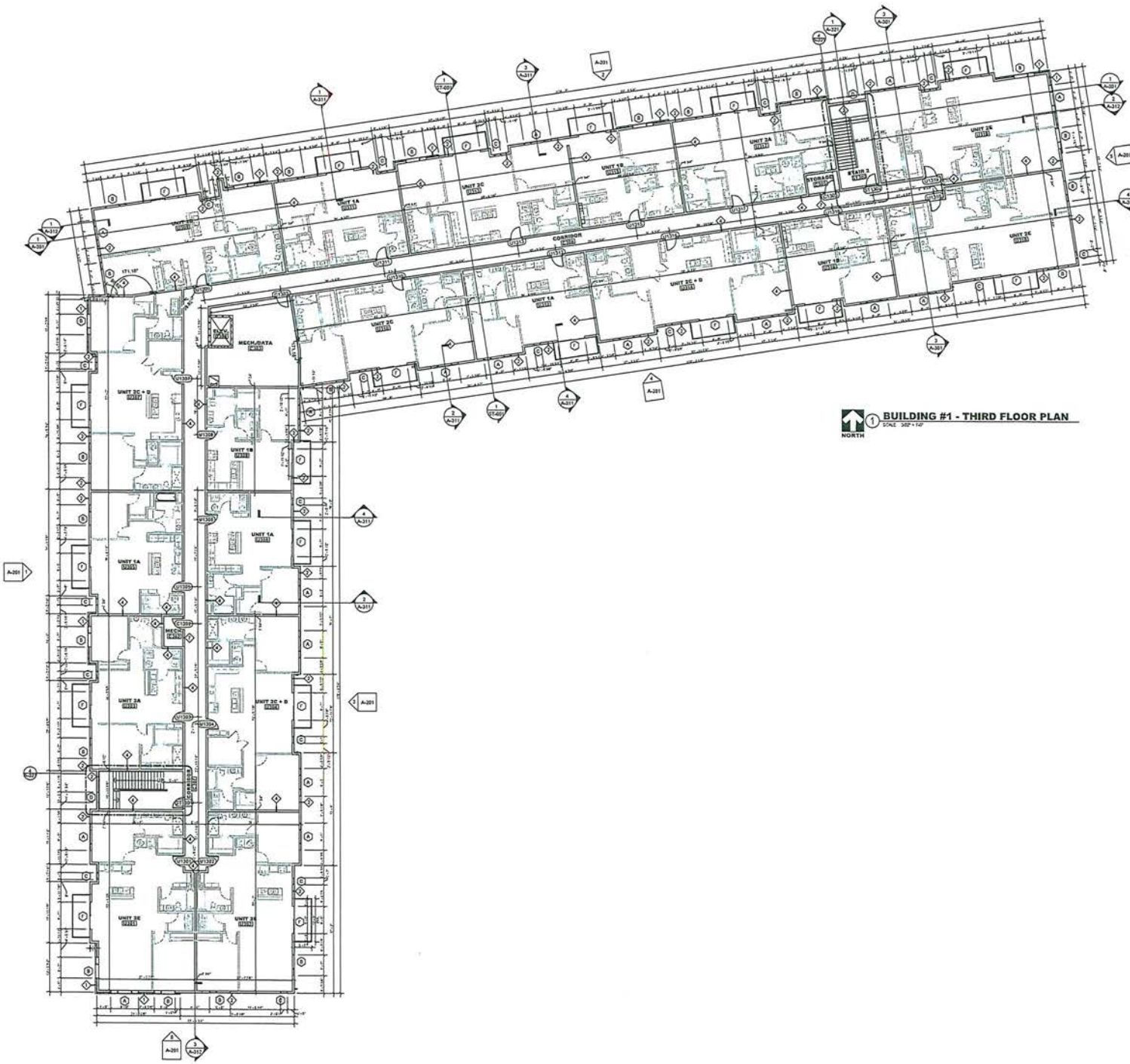
TOTALS	
UNIT 1A	2
UNIT 1B	1
UNIT 1C	1
UNIT 1D	1
UNIT 1E	1
UNIT 1F	1
UNIT 1G	1
UNIT 1H	1
UNIT 1I	1
UNIT 1J	1
UNIT 2A	4
UNIT 2B	2
UNIT 2C	2
UNIT 2D	2
UNIT 2E	2
UNIT 2F	2
UNIT 2G	2
UNIT 2H	2
UNIT 2I	2
UNIT 2J	2
UNIT 3A	4
UNIT 3B	2
UNIT 3C	2
UNIT 3D	2
UNIT 3E	2
UNIT 3F	2
UNIT 3G	2
UNIT 3H	2
UNIT 3I	2
UNIT 3J	2
TOTAL	48 (12 GARAGES)



**TELUS CONDOS**  
 DES MOINES, IA

DATE	10-20-2011
BY	MARK ELLIOTT
PROJECT	TELUS CONDOS, 1010 13TH AVE. S.E. DES MOINES, IA 50315
SCALE	AS SHOWN





**BUILDING #1 - THIRD FLOOR PLAN**  
 NORTH  
 SCALE: 3/8" = 1'-0"

- GENERAL FLOOR PLAN NOTES**
1. REFER TO ADDITIONAL NOTES AND REQUIREMENTS ON ALL OTHER DOCUMENTS AND OTHER DISCIPLINE DOCUMENTS.
  2. ALL SITE M/F/O, FURNITURE, FIXTURES AND EQUIPMENT SHOWN SHOULD BE CONSIDERED CONCEPTUAL AND MAY HAVE BE PROVIDED FOR COORDINATION PURPOSES ONLY. REFER TO CIVIL, VEE SUPPLEMENT, PLUMBING, MECHANICAL, ELECTRICAL, COMMUNICATIONS, ELECTRONIC SAFETY & SECURITY DOCUMENTS, AND CONSULTANTS FOR SPECIFIC DESIGN INFORMATION & REQUIREMENTS.
  3. DIMENSIONS ARE TO FACE OF FINISH MATERIAL OR CENTER OF STRUCTURE UNLESS OTHERWISE SPECIFIED.
  4. ALL CORNERS, PIPES, WIRING, REDUCTION, ETC. SHOULD BE HELD AS TIGHT AS POSSIBLE TO THE ROOF OR FLOOR DECK OR SHOULD BE KEPT WITHIN INTERIOR WALL CAVEATS. NOTINGS SHALL BE BELOW THE BOTTOM EDGE OF MAIN BUILDING FRAMES. NOTHING SHALL BE EXPOSED IN FINISHED SPACES. NOTHING SHALL PENETRATE A SMOKE CONTAINING TUBED WALL OR CEILING AS NOTED ON THE IS GRANTED BY THE ARCHITECT OR OWNER. FINISHMENT COORDINATION BETWEEN TRADES IS REQUIRED.
  5. INSTALL VERTICAL EXPANSION JOINT MINIMAL AT COLUMNWORK LINES AND AT THE HEAD OF SCOR JAMBOPENINGS OR AS OTHERWISE NOTED OR WHERE REQUIRED FOR STANDARD MATERIAL PRACTICE TO REDUCE STRESS STRAINS. NO CONTROL JOINT/EXPANSION JOINT IN LOAD BEARING AREAS. STAY AWAY 2'-0" MIN.
  6. CALL & SEAL ALL CONTROL-EXPANSION, SAWCUT JOINTS AT ALL INTERIOR & EXTERIOR MASONRY & CONCRETE.
  7. PROVIDE LOWER LEVEL ANCHOR UNDERFLOOR DRAINAGE SYSTEM TO REDUCE HYDROSTATIC UPLIFT AND PROTECT AGAINST MOISTURE SEEPAGE. PROVIDE FOUNDATIONAL DRAINAGE SYSTEM TO REDUCE LATERAL PRESSURES AND PROTECT AGAINST MOISTURE SEEPAGE. BOTH SYSTEMS SHALL OUTLET TO APPROPRIATE RELIEF.
  8. PROVIDE POSITIVE DRAINAGE OF SURFACE WATER AWAY FROM BUILDING WITHOUT PONDING OF WATER ADJACENT TO BUILDING OR ON PAVEMENTS.

**BUILDING #1 UNIT MIX:**

**FIRST FLOOR**

UNIT 1A	1
UNIT 1B	1
UNIT 2A	1
UNIT 2B	1
UNIT 2C-D	1
UNIT 2E	1
TOTAL	6 (2) 1-BED GARAGES, 1 ADA GARAGES

**SECOND FLOOR**

UNIT 1A	4
UNIT 1B	4
UNIT 2A	2
UNIT 2B	2
UNIT 2C-D	2
UNIT 2E	2
TOTAL	18

**THIRD FLOOR**

UNIT 1A	4
UNIT 1B	4
UNIT 2A	2
UNIT 2B	2
UNIT 2C-D	2
UNIT 2E	2
TOTAL	18

**TOTALS**

UNIT 1A	10	17 1-BEDROOMS + 30%
UNIT 1B	4	
UNIT 2A	2	
UNIT 2B	2	
UNIT 2C-D	2	
UNIT 2E	2	
TOTAL	22	(2) GARAGES)



**TELUS CONDOS**  
 DES MOINES, IA

DATE	12-28-2017
PROJECT	TELUS CONDOS
DESIGNER	ARCHITECT
CHECKED	ARCHITECT
DATE	12-28-2017
PROJECT	TELUS CONDOS
DESIGNER	ARCHITECT
CHECKED	ARCHITECT

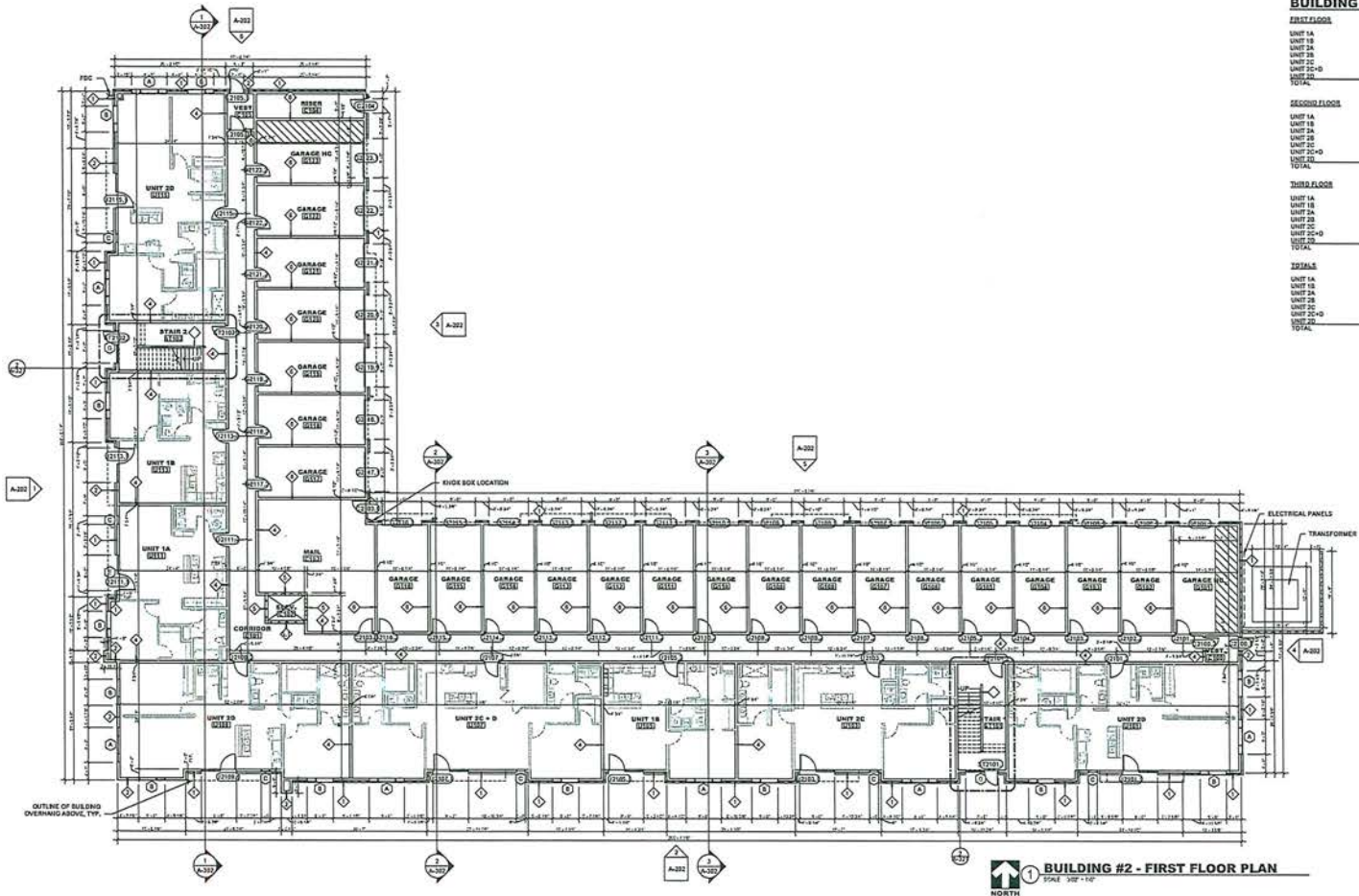
Job No. 21162  
 21162  
 SP  
 SHEET  
 BUILDING #1 -  
 THIRD FLOOR  
 PLAN  
 Sheet No.

**A-103**









**GENERAL FLOOR PLAN NOTES**

1. REFER TO ADDITIONAL NOTES AND REQUIREMENTS ON ALL OTHER DOCUMENTS AND OTHER DISCIPLINE DOCUMENTS.
2. ALL SITE AND FINISH MATERIALS AND EQUIPMENT SHOULD BE CONSIDERED CONCEPTUAL AND MAY HAVE TO BE PROVIDED FOR COORDINATION PURPOSES ONLY. REFER TO CIVIL, FIRE SUPPRESSION, PLUMBING, MECHANICAL, ELECTRICAL, COMMUNICATIONS, ELECTRONIC SAFETY & SECURITY DOCUMENTS AND CONSULTANTS FOR SPECIFIC DESIGN INFORMATION & REQUIREMENTS.
3. DIMENSIONS ARE TO FACE OF FINISH MATERIAL OR CENTER OF STRUCTURE UNLESS OTHERWISE SPECIFIED.
4. ALL CONDUITE, PIPES, WIRING, FURDIAL, ETC. SHOULD BE HELD AS TIGHT AS POSSIBLE TO THE FLOOR OR FLOOR DECK OR SHOULD BE KEPT WITHIN INTERIOR WALL CHASES. NOTING SHALL BE BELOW THE BOTTOM EDGE OF MAIN BUILDING FRAMES. NOTING SHALL BE CAPTURED IN FINISHED SPACES. NOTING SHALL BE CAPTURED IN COMMON AREAS. FINISH WALL OR CEILING AS NOTED ON THE DRAWING AND NO PLUMBING RUN IN EXTERIOR WALLS UNLESS PRIOR APPROVAL IS GRANTED BY THE ARCHITECT OR OWNER. PLACEMENT COORDINATION BETWEEN TRADES IS REQUIRED.
5. INSTALL VERTICAL EXPANDED JOINT METAL AT COLUMN/JOIST LINES AND AT THE HEAD OF DOOR JAMB/OPENINGS OR AS OTHERWISE NOTED OR WHERE REQUIRED PER STANDARD MATERIAL PRACTICE TO REDUCE STRESS CRACKING. NO CONTROL JOINT EXPANSION JOINT IN LOAD BEARING AREA. STAY AWAY 2'-0" MIN.
6. CRACKLE & SEAL ALL CONTROL EXPANSION, SAWCUT JOINTS AT ALL INTERIOR & EXTERIOR MASONRY & CONCRETE.
7. PROVIDE LOWER LEVEL AND/OR UNDERFLOOR DRAINAGE SYSTEM TO REDUCE HYDROSTATIC UPLIFT AND PROTECT AGAINST MOISTURE SEEPAGE. PROVIDE FOUNDATION WALL DRAINAGE SYSTEM TO REDUCE LATERAL PRESSURES AND PROTECT AGAINST MOISTURE SEEPAGE. BOTH SYSTEMS SHALL OUTLET TO APPROPRIATE RELIEF.
8. PROVIDE POSITIVE DRAINAGE OF SURFACE WATER AWAY FROM BUILDING WITHOUT PONDING OF WATER ADJACENT TO BUILDING OR ON PRECIPITANTS.

**BUILDING #2 UNIT MIX:**

**FIRST FLOOR**

UNIT 1A	1
UNIT 1B	1
UNIT 2A	1
UNIT 2B	1
UNIT 2C	1
UNIT 2D	1
TOTAL	6

21H REG. GARAGES, 2ADA GARAGES

**SECOND FLOOR**

UNIT 1A	2
UNIT 1B	2
UNIT 2A	2
UNIT 2B	2
UNIT 2C	2
UNIT 2D	2
TOTAL	14

**THIRD FLOOR**

UNIT 1A	2
UNIT 1B	2
UNIT 2A	2
UNIT 2B	2
UNIT 2C	2
UNIT 2D	2
TOTAL	14

**TOTALS**

UNIT 1A	3	11 BEDROOMS = 26%
UNIT 1B	3	
UNIT 2A	3	
UNIT 2B	3	
UNIT 2C	3	
UNIT 2D	3	
TOTAL	18	33 (18) GARAGES

**1 BUILDING #2 - FIRST FLOOR PLAN**  
SCALE: 1/8" = 1'-0"



**TELUS CONDOS**  
DES MOINES, IA

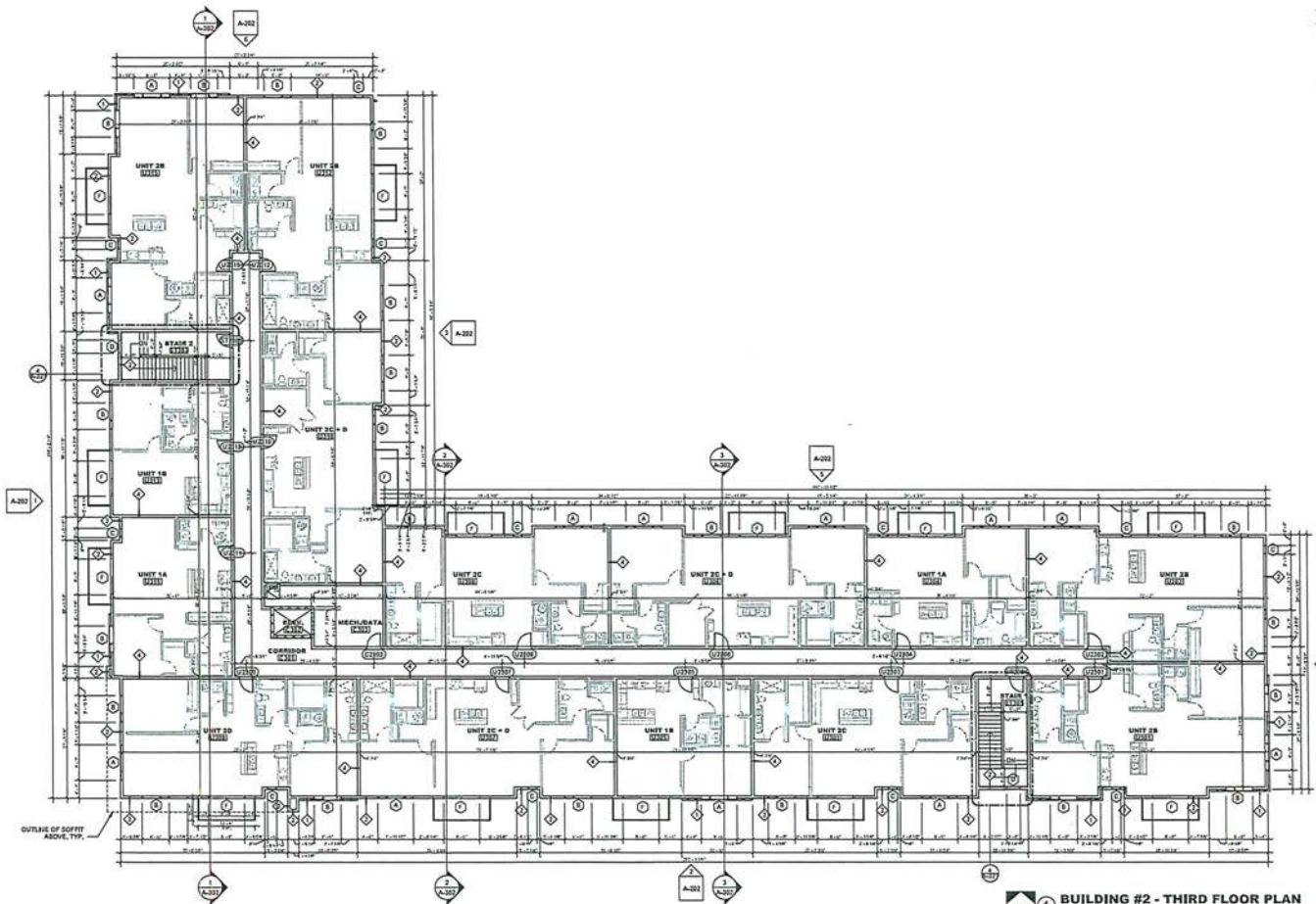
DATE	10/25/2010
BY	J.F. FRENCH
PROJECT NO.	10000000000000000000
DRAWING NO.	10000000000000000000

JOB NO. 21142  
SHEET NO. 2P  
PROJECT: BUILDING #2 - FIRST FLOOR PLAN  
DATE: 10/25/2010  
**A-111**









- GENERAL FLOOR PLAN NOTES**
1. REFER TO ADDITIONAL NOTES AND REQUIREMENTS ON ALL OTHER DOCUMENTS AND OTHER DISCIPLINE DOCUMENTS.
  2. ALL SITE AND FURNITURE, FIXTURES AND EQUIPMENT SHOULD BE CONSIDERED CONCEPTUAL AND MAY HAVE BE PROVIDED FOR COORDINATION PURPOSES ONLY. REFER TO CIVIL, FIRE SUPPRESSION, PLUMBING, MECHANICAL, ELECTRICAL, COMMUNICATIONS, ELECTRICAL SAFETY & SECURITY DOCUMENTS FOR CONTROL AND FINISH SPECIFICATIONS AND REQUIREMENTS.
  3. DIMENSIONS ARE TO FACE OF FINISH MATERIAL OR CENTER OF STRUCTURE UNLESS OTHERWISE SPECIFIED.
  4. ALL CONDUITS, PIPES, WIRING, ROUGH-IN, ETC. SHOULD BE HELD AS TIGHT AS POSSIBLE TO THE ROOF OR FLOOR DECK OR SHOULD BE KEPT WITHIN INTERIOR WALL CHASES. NOTHING SHALL BE BELOW THE BOTTOM EDGE OF MAIN BUILDING FRAMES. NOTHING SHALL BE EXPOSED BY FINISHED SPACES. NOTHING SHALL PREVENT A SMOOTH CONTINUOUS FINISHED WALL OR CEILING AS NOTED ON THE DRAWINGS AND NO PLUMBING SHALL BE EXPOSED IN WALLS UNLESS PRIOR APPROVAL IS OBTAINED BY THE ARCHITECT OR OWNER. PLACEMENT COORDINATION BETWEEN TRADES IS REQUIRED.
  5. INSTALL VERTICAL EXPANSION JOINT MINIMAL AT COLUMN/GRID LINES AND AT THE HEAD OF DOOR JAMB/EXPANSION OR AS OTHERWISE NOTED OR INDICATED REQUIRED PER STANDARD MATERIAL PRACTICE TO REDUCE STRESS CRACKING. NO CONTROL JOINT/EXPANSION JOINT IN LOAD BEARING AREAS. STAY AWAY 2'-0" MIN.
  6. CAULK & SEAL ALL CONTROL/EXPANSION, SANDWICH JOINTS AT ALL INTERIOR & EXTERIOR MASONRY & CONCRETE.
  7. PROVIDE LOWER LEVEL AND/OR UNDERFLOOR DRAINAGE SYSTEM TO REDUCE POTENTIAL LEAKY AND PROTECT AGAINST MOISTURE SEEPAGE. PROVIDE FOUNDATION WALL DRAINAGE SYSTEM TO REDUCE LATERAL PRESSURES AND PROTECT AGAINST MOISTURE SEEPAGE. BOTH SYSTEMS SHALL OUTLET TO APPROPRIATE RELIEF.
  8. PROVIDE POSITIVE DRAINAGE OF SURFACE WATER AWAY FROM BUILDING WITHOUT PONDING OF WATER ADJACENT TO BUILDING OR ON PAVEMENTS.

**BUILDING #2 UNIT MIX:**

FIRST FLOOR	
UNIT 1A	1
UNIT 1B	1
UNIT 2A	1
UNIT 2B	1
UNIT 2C	1
UNIT 2C-B	1
UNIT 3A	1
UNIT 3B	1
UNIT 3C	1
UNIT 3C-B	1
UNIT 4A	1
UNIT 4B	1
UNIT 4C	1
UNIT 4C-B	1
UNIT 5A	1
UNIT 5B	1
UNIT 5C	1
UNIT 5C-B	1
TOTAL	31 (31 REG. GARAGES, 2 ADA GARAGES)

SECOND FLOOR	
UNIT 1A	1
UNIT 1B	1
UNIT 2A	1
UNIT 2B	1
UNIT 2C	1
UNIT 2C-B	1
UNIT 3A	1
UNIT 3B	1
UNIT 3C	1
UNIT 3C-B	1
UNIT 4A	1
UNIT 4B	1
UNIT 4C	1
UNIT 4C-B	1
UNIT 5A	1
UNIT 5B	1
UNIT 5C	1
UNIT 5C-B	1
TOTAL	31

THIRD FLOOR	
UNIT 1A	1
UNIT 1B	1
UNIT 2A	1
UNIT 2B	1
UNIT 2C	1
UNIT 2C-B	1
UNIT 3A	1
UNIT 3B	1
UNIT 3C	1
UNIT 3C-B	1
UNIT 4A	1
UNIT 4B	1
UNIT 4C	1
UNIT 4C-B	1
UNIT 5A	1
UNIT 5B	1
UNIT 5C	1
UNIT 5C-B	1
TOTAL	31

TOTAL	
UNIT 1A	3
UNIT 1B	3
UNIT 2A	3
UNIT 2B	3
UNIT 2C	3
UNIT 2C-B	3
UNIT 3A	3
UNIT 3B	3
UNIT 3C	3
UNIT 3C-B	3
UNIT 4A	3
UNIT 4B	3
UNIT 4C	3
UNIT 4C-B	3
UNIT 5A	3
UNIT 5B	3
UNIT 5C	3
UNIT 5C-B	3
TOTAL	33 (33 GARAGES)

**1 BUILDING #2 - THIRD FLOOR PLAN**  
SCALE: 3/8" = 1'-0"



**TELUS CONDOS**  
DES MOINES, IA

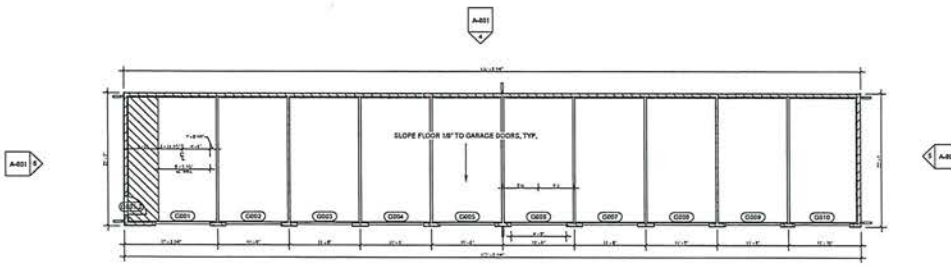
DATE	12/20/2023
BY	12/20/2023
SCALE	3/8" = 1'-0"
PROJECT	TELUS CONDOS
NO.	12/20/2023

Job No. 21182  
Sheet Title BUILDING #2 - THIRD FLOOR PLAN  
Drawn By: [Name]  
Date: 12/20/2023  
Scale: 3/8" = 1'-0"

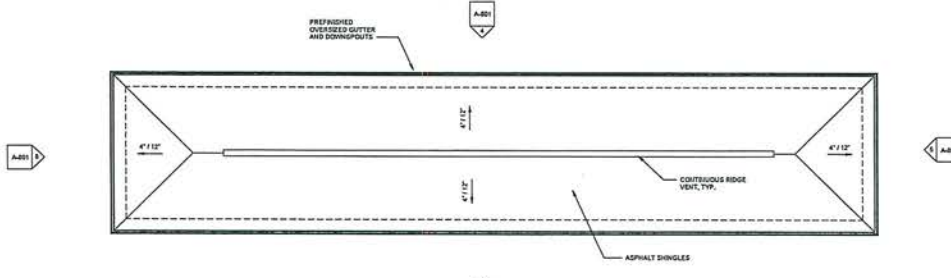




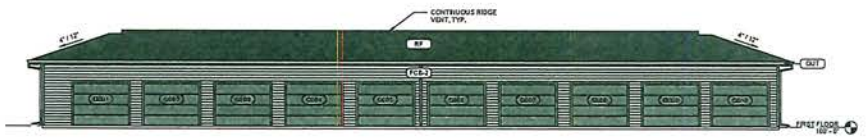




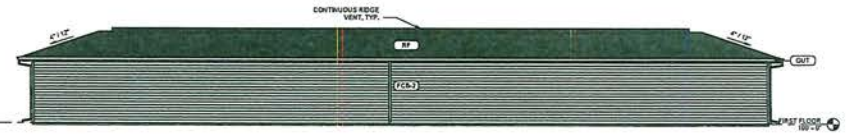
1 GARAGE PLAN  
SCALE: 1/8" = 1'-0" REFER TO CML FOR ORIENTATION



2 GARAGE ROOF  
SCALE: 1/8" = 1'-0" REFER TO CML FOR ORIENTATION



3 GARAGE DOOR (10 STALL) ELEVATION  
SCALE: 1/8" = 1'-0"



4 GARAGE (10 STALL) ELEVATION  
SCALE: 1/8" = 1'-0"



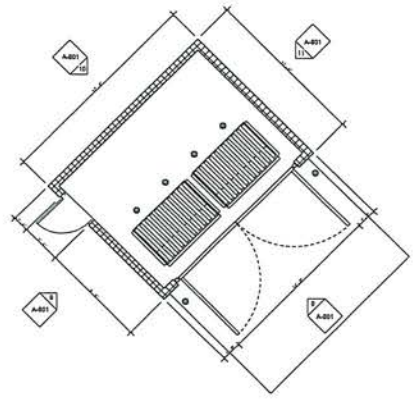
5 GARAGE SIDE 1 (10 STALL) ELEVATION  
SCALE: 1/8" = 1'-0"



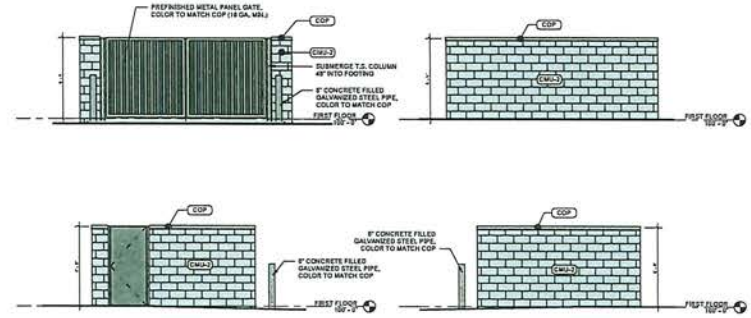
6 GARAGE SIDE 2 (10 STALL) ELEVATION  
SCALE: 1/8" = 1'-0"

EXTERIOR MATERIAL LEGEND - GARAGE	
MAT. LABEL	MATERIAL DESCRIPTION
BAI	PRE-FERRETTED BALCONY, CLEAR ANODIZED STEEL
BRE	PULL DOWN MODULAR BRICK, NATURAL, MORTAR COLOR - GRAY
CAI	PRE-FERRETTED CHIMNEY, BRICK, OR BRICK, COLOR - CLEAR ANODIZED
CBK	8\"/>

- MISC EXTERIOR MATERIAL FINISHES**
- TYPICAL U.O.N.
- EXPOSED CONCRETE: GRIND SMOOTH & RUB/SCAT WITH CEMENT SLURRY.
  - EXPOSED INTERIORS: GALVANIZED & PAINTED TO MATCH ADJACENT MATERIAL ABOVE.
  - METAL BALCONIES & BALINGS: GALVANIZED & PAINTED WHITE.
  - FABRIC & SOFFIT: METAL, PRE-FINISHED TO MATCH ADJACENT ROOF COLOR.
  - SCOPPING GUTTERS & DOWNSPOUTS: METAL, PRE-FINISHED TO MATCH ADJ. MATERIAL.
  - CHIMNEYS: CERAMIC, COLOR MATCH TO ADJACENT MATERIAL.
  - HOLLOW METAL MAN DOORS & FRAMES: PAINTED TO MATCH ADJACENT MATERIAL.
  - OVERHEAD PANEL DOORS: METAL, PRE-FINISHED TO MATCH ADJACENT MATERIAL.
  - LOUVERS: METAL, PRE-FINISHED TO MATCH GRAY.
  - LIGHT FIXTURES: METAL, PRE-FINISHED HOUSINGS TO MATCH ADJ. OPENING FRAMING.
  - MECHANICAL EQUIPMENT: SCREENED BY FRAME.
  - EXTERIOR UTILITY EQUIPMENT: IF ALLOWED, PAINTED TO MATCH ADJACENT MATERIAL.
- TRIM SIZES, TYP. U.O.N.
- VERTICAL: 5/8"
  - HORIZONTAL: 3/4"



7 TRASH ENCLOSURE PLAN  
SCALE: 1/8" = 1'-0"



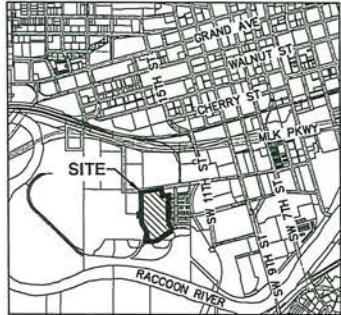


PRELIMINARY PLAT / DEVELOPMENT PLAN FOR:

# GRAY'S STATION PLAT 5

## DES MOINES, IOWA

VICINITY MAP  
NOT TO SCALE



DES MOINES, IOWA

**OWNER**

HRC NFS I LLC  
6900 WESTOWN PKWY  
WEST DES MOINES, IA 50266

**APPLICANT**

HUBBELL REALTY COMPANY  
6900 WESTOWN PKWY  
WEST DES MOINES, IA 50266  
CONTACT: CALIEB SMITH  
PH. (515) 243-3228

**ENGINEER**

CIVIL DESIGN ADVANTAGE  
4121 NW URBANDALE DRIVE  
URBANDALE, IA 50322  
CONTACT: RYAN HARDISTY  
EMAIL: RYANH@CDA-ENG.COM  
PH. (515) 369-4400

**SURVEYOR**

CIVIL DESIGN ADVANTAGE, LLC  
4121 NW URBANDALE DRIVE  
URBANDALE, IA 50322  
CONTACT: CHARLIE MCGLOTHLEN  
EMAIL: CHARLEM@CDA-ENG.COM  
PH. (515) 369-4400

**DATE OF SURVEY**

08/05/2022

**BENCHMARKS**

- CITY BENCHMARK #725: BRASS CAP IN THE NORTHEAST TRAFFIC SIGNAL BASE AT THE SOUTHWEST CORNER OF MARTIN LUTHER KING DRIVE AND SOUTHWEST 11TH STREET. ELEVATION=28.81
- CITY BENCHMARK #5581: BRASS TABLET IN THE CONCRETE WALL 43.8 FEET WEST OF THE EAST FACE NEAR THE SANITARY SEWER PUMP STATION. ELEVATION=25.46

**SUBMITTAL DATES**

FIRST SUBMITTAL: 10/19/2022  
SECOND SUBMITTAL: 01/04/2023

**LEGAL DESCRIPTION**

A PART OF LOTS 6, 7, AND 9 CENTRAL DES MOINES INDUSTRIAL PARK AND A PART OF PARCELS '2018-144' OF SAID LOTS 6, 7, AND 9, AS SHOWN ON THE PLAT OF SURVEY RECORDED IN BOOK 17673, PAGE 730, BOTH OFFICIAL PLATS IN THE CITY OF DES MOINES, POLK COUNTY, IOWA AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF LOT 12, GRAY'S STATION PLAT 2, AN OFFICIAL PLAT, THENCE SOUTH 08°20'42" EAST ALONG THE WEST LINE OF SAID LOT 12, A DISTANCE OF 145.50 FEET TO THE SOUTHWEST CORNER OF SAID LOT 12; THENCE NORTH 81°39'18" EAST ALONG THE SOUTHERLY LINE OF SAID LOT 12, A DISTANCE OF 128.11 FEET TO THE NORTHWEST CORNER OF OUTLOT 'Y', SAID GRAY'S STATION PLAT 2; THENCE SOUTH 00°01'40" EAST ALONG THE WESTERLY LINE OF SAID OUTLOT 'Y' AND THE WESTERLY LINE OF GRAY'S STATION PLAT 3, AN OFFICIAL PLAT, 65.73 FEET; THENCE SOUTH 69°58'20" WEST CONTINUING ALONG SAID WESTERLY LINE OF SAID GRAY'S STATION PLAT 3, A DISTANCE OF 12.77 FEET; THENCE SOUTH 00°01'40" EAST CONTINUING ALONG SAID WESTERLY LINE OF 107.91 FEET; THENCE SOUTH 69°58'20" WEST CONTINUING ALONG SAID WESTERLY LINE, 6.23 FEET; THENCE SOUTH 00°01'40" EAST CONTINUING ALONG SAID WESTERLY LINE AND THE WESTERLY LINE OF GRAY'S STATION PLAT 4, A DISTANCE OF 122.09 FEET; THENCE NORTH 89°58'20" EAST CONTINUING ALONG SAID WESTERLY LINE OF SAID GRAY'S STATION PLAT 4, A DISTANCE OF 17.81 FEET; THENCE SOUTH 00°01'40" EAST CONTINUING ALONG SAID WESTERLY LINE, 63.00 FEET; THENCE SOUTH 89°58'20" WEST CONTINUING ALONG SAID WESTERLY LINE, 8.11 FEET; THENCE SOUTH 00°01'40" EAST CONTINUING ALONG SAID WESTERLY LINE, 146.23 FEET; THENCE SOUTHERLY CONTINUING ALONG SAID WESTERLY LINE AND A CURVE CONCAVE WESTERLY WHOSE RADIUS IS 80.60 FEET, WHOSE ARC LENGTH IS 56.38 FEET AND WHOSE CHORD BEARS SOUTH 20°09'18" WEST, 55.20 FEET; THENCE SOUTH 49°20'41" EAST CONTINUING ALONG SAID WESTERLY LINE, 62.03 FEET; THENCE SOUTH 22°22'59" EAST CONTINUING ALONG SAID WESTERLY LINE, 105.61 FEET TO THE SOUTHWEST CORNER OF STREET LOT 'A', SAID GRAY'S STATION PLAT 4; THENCE SOUTHWESTERLY ALONG A CURVE CONCAVE SOUTHEASTERLY WHOSE RADIUS IS 298.00 FEET, WHOSE ARC LENGTH IS 32.35 FEET AND WHOSE CHORD BEARS SOUTH 57°30'24" WEST, 32.34 FEET; THENCE SOUTH 64°23'44" WEST, 248.83 FEET; THENCE WESTERLY ALONG A CURVE CONCAVE NORTHERLY WHOSE RADIUS IS 368.00 FEET, WHOSE ARC LENGTH IS 332.72 FEET AND WHOSE CHORD BEARS SOUTH 77°37'44" WEST, 294.28 FEET; THENCE NORTH 78°28'18" WEST, 26.20 FEET; THENCE NORTH 11°31'42" EAST, 70.00 FEET; THENCE SOUTH 78°28'18" EAST, 22.83 FEET; THENCE NORTH 00°01'11" WEST, 61.22 FEET; THENCE NORTHERLY ALONG A CURVE CONCAVE WESTERLY WHOSE RADIUS IS 224.00 FEET, WHOSE ARC LENGTH IS 131.34 FEET AND WHOSE CHORD BEARS NORTH 16°26'00" WEST, 129.48 FEET; THENCE NORTH 33°43'49" WEST, 43.36 FEET; THENCE NORTHERLY ALONG A CURVE CONCAVE EASTERLY WHOSE RADIUS IS 278.00 FEET, WHOSE ARC LENGTH IS 161.83 FEET AND WHOSE CHORD BEARS NORTH 18°58'00" WEST, 158.52 FEET; THENCE NORTH 00°08'11" WEST, 58.59 FEET; THENCE NORTH 89°19'18" WEST, 43.82 FEET; THENCE NORTH 00°40'42" EAST, 52.00 FEET TO THE SOUTH LINE OF LOT 8, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE SOUTH 89°19'18" EAST ALONG SAID SOUTH LINE, 43.18 FEET TO THE SOUTHEAST CORNER OF SAID LOT 8; THENCE NORTH 00°01'11" WEST ALONG THE EAST LINE OF SAID LOT 8, A DISTANCE OF 333.17 FEET; THENCE NORTHERLY CONTINUING ALONG SAID EAST LINE AND A CURVE CONCAVE WESTERLY WHOSE RADIUS IS 804.42 FEET, WHOSE ARC LENGTH IS 84.04 FEET AND WHOSE CHORD BEARS NORTH 03°28'39" WEST, 93.89 FEET; THENCE NORTH 08°58'08" WEST CONTINUING ALONG SAID EAST LINE, 15.78 FEET TO THE NORTHEAST CORNER OF SAID LOT 8, ALSO BEING A POINT ON THE SOUTHERLY LINE OF TUTTLE STREET RIGHT OF WAY; THENCE NORTH 81°10'49" EAST ALONG SAID SOUTHERLY RIGHT OF WAY LINE, 411.78 FEET TO THE POINT OF BEGINNING AND CONTAINING 11.47 ACRES (499,715 SQUARE FEET).

THE PROPERTY IS SUBJECT TO ANY AND ALL EASEMENTS OF RECORD.

**ZONING**

GRAY'S STATION PUD - ZON2017-00087

**EXISTING/ PROPOSED USE**

EXISTING USE: UNDEVELOPED  
PROPOSED USE: RESIDENTIAL

**TAX ABATEMENT - SUSTAINABILITY**

- BIKE RACKS FOR PUBLIC USE THAT PROVIDE A MINIMUM OF 10% OF THE AUTOMOBILE PARKING PROVIDED.
- 50% INCREASE IN REQUIRED LANDSCAPING.
- PRIMARY ENTRY WITHIN A MILE OF A DART TRANSIT STOP.
- REDEVELOPMENT OF A PREVIOUSLY DEVELOPED SITE.

**INDEX OF SHEETS**

NO.	DESCRIPTION
CO.1	COVER SHEET
C1.1	TOPOGRAPHIC SURVEY / DEMOLITION PLAN
C2.0-2.4	DIMENSION PLAN
C3.0-3.5	GRADING PLAN
C4.0-4.4	UTILITY PLAN
C5.1-5.2	DETAILS
LO2.01-LO2.05	RDG LANDSCAPE PLAN (SITE LAYOUT PLAN)
LO4.01-LO4.05	RDG LANDSCAPE PLAN (SITE PLANTING PLAN)
LO5.01	RDG LANDSCAPE PLAN (DETAILS)

**DEVELOPMENT SUMMARY**

TOTAL SITE AREA:	11.47 ACRES (499,715 SF)
EXISTING IMPERVIOUS AREA:	0.00 ACRES (0 SF)
OPEN SPACE CALCULATION:	
TOTAL SITE BUILDINGS	= 499,715 SF (11.47 AC)
STREETS	= 84,113 SF
DRIVEWAYS	= 12,745 SF
SIDEWALKS	= 24,388 SF
OPEN SPACE PROVIDED	= 282,622 SF (56.6%)

UNITS:  
65 TOWNHOME UNITS  
84 CONDOMINIUM UNITS (LOT 67)  
149 TOTAL UNITS (12.99 UNITS PER ACRE)

**NOTES**

- ALL PUBLIC IMPROVEMENTS, INCLUDING SPOT ELEVATIONS, ARE SHOWN FOR REFERENCE ONLY. REFER TO THE SEPARATE PRIVATE CONSTRUCTION CONTRACT DOCUMENTS FOR PUBLIC IMPROVEMENT PLANS. IF THERE ARE ANY DISCREPANCIES BETWEEN THE DEVELOPMENT PLAN AND THE PUBLIC IMPROVEMENT PLANS THE DETAILED PUBLIC IMPROVEMENT PLANS SHALL GOVERN.
- xxxxTHIS PROJECT IS ELIGIBLE FOR TAX ABATEMENT IN ACCORDANCE WITH THE URBAN RENEWAL DEVELOPMENT AGREEMENT - SECTION 5.02 (8K1628N P.0251-288).xxxx

**UTILITY WARNING**

ANY UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY AND RECORDS OBTAINED BY THIS SURVEYOR. THE SURVEYOR MAKES NO GUARANTEE THAT THE UTILITIES SHOWN COMPRISE ALL THE UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UTILITIES SHOWN ARE IN THE EXACT LOCATION SHOWN.



4121 NW URBANDALE DRIVE, URBANDALE, IA 50322  
PH: (515) 369-4400  
PROJECT NO. 2202.113

**GENERAL LEGEND**

PROPOSED	EXISTING
PROPERTY BOUNDARY	SANITARY MANHOLE
LOT LINE	WATER VALVE BOX
CENTER LINE	FIRE HYDRANT
RIGHT OF WAY	WATER CURB STOP
BUILDING SETBACK	WELL
PERMANENT EASEMENT	STORM SEWER MANHOLE
TEMPORARY EASEMENT	STORM SEWER SINGLE INTAKE
TYPE SW-501 STORM INTAKE	STORM SEWER DOUBLE INTAKE
TYPE SW-502 STORM INTAKE	FLARED END SECTION
TYPE SW-503 STORM INTAKE	DECIDUOUS TREE
TYPE SW-505 STORM INTAKE	CONIFEROUS TREE
TYPE SW-506 STORM INTAKE	DECIDUOUS SHRUB
TYPE SW-512 STORM INTAKE	CONIFEROUS SHRUB
TYPE SW-513 STORM INTAKE	ELECTRIC POWER POLE
TYPE SW-401 STORM MANHOLE	GUY ANCHOR
TYPE SW-402 STORM MANHOLE	STREET LIGHT
FLARED END SECTION	POWER POLE W/ TRANSFORMER
TYPE SW-301 SANITARY MANHOLE	UTILITY POLE W/ LIGHT
STORM/SANITARY CLEANOUT	ELECTRIC BOX
WATER VALVE	ELECTRIC TRANSFORMER
FIRE HYDRANT ASSEMBLY	ELECTRIC MANHOLE OR VAULT
SIGN	TRAFFIC SIGN
DETECTABLE WARNING PANEL	TELEPHONE JUNCTION BOX
WATER CURB STOP	TELEPHONE MANHOLE/VAULT
SANITARY SEWER	TELEPHONE POLE
SANITARY SERVICE	GAS VALVE BOX
STORM SEWER	CABLE TV JUNCTION BOX
STORM SERVICE	CABLE TV MANHOLE/VAULT
WATERMAN WITH SIZE	MAIL BOX
WATER SERVICE	BENCHMARK
SAWCUT (FULL DEPTH)	SOIL BORING
SILT FENCE	UNDERGROUND TV CABLE
USE AS CONSTRUCTED (UAC)	GAS MAIN
MINIMUM PROTECTION ELEVATION (MPE)	FIBER OPTIC
	UNDERGROUND TELEPHONE
	OVERHEAD ELECTRIC
	UNDERGROUND ELECTRIC
	FIELD TILE
	SANITARY SEWER W/ SIZE
	STORM SEWER W/ SIZE
	WATER MAIN W/ SIZE

THE PROJECT REQUIRES AN IOWA NPDES PERMIT #2 AND CITY OF DES MOINES GRADING PERMIT. CIVIL DESIGN ADVANTAGE WILL PROVIDE THE PERMITS AND THE INITIAL STORM WATER POLLUTION PREVENTION PLAN (SWPPP) FOR THE CONTRACTORS USE DURING CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR UPDATING THE SWPPP THROUGHOUT CONSTRUCTION AND MEETING LOCAL, STATE AND FEDERAL REQUIREMENTS.

ALL CONSTRUCTION MATERIALS, DUMPSTERS, DETACHED TRAILERS OR SIMILAR ITEMS ARE PROHIBITED ON PUBLIC STREETS OR WITHIN THE PUBLIC R.O.W.

THE 2022 EDITION OF THE SDAS STANDARD SPECIFICATIONS, THE PUBLIC RIGHTS-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG) AND ALL CITY SUPPLEMENTALS, IF APPLICABLE, SHALL APPLY TO ALL WORK ON THIS PROJECT UNLESS OTHERWISE NOTED.

**PRELIMINARY PLAT APPROVAL:**

APPROVED  APPROVED WITH CONDITIONS - SEE EXHIBIT "A"

IN ACCORDANCE WITH CHAPTER 106, DES MOINES MUNICIPAL CODE, AS AMENDED.

DEVELOPMENT SERVICES DIRECTOR DATE

I HEREBY CERTIFY THAT THIS ENGINEERING DOCUMENT WAS PREPARED BY ME OR UNDER MY DIRECT PERSONAL SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF IOWA.

**PROFESSIONAL ENGINEER**  
RYAN HARDISTY, P.E.  
20211  
IOWA

DATE: 10/19/2022  
EXPIRES: 10/19/2027  
PAGES OR SHEETS COVERED BY THIS SEAL: 2202.113  
C SERIES SHEETS

**SITE PLAN APPROVAL:**

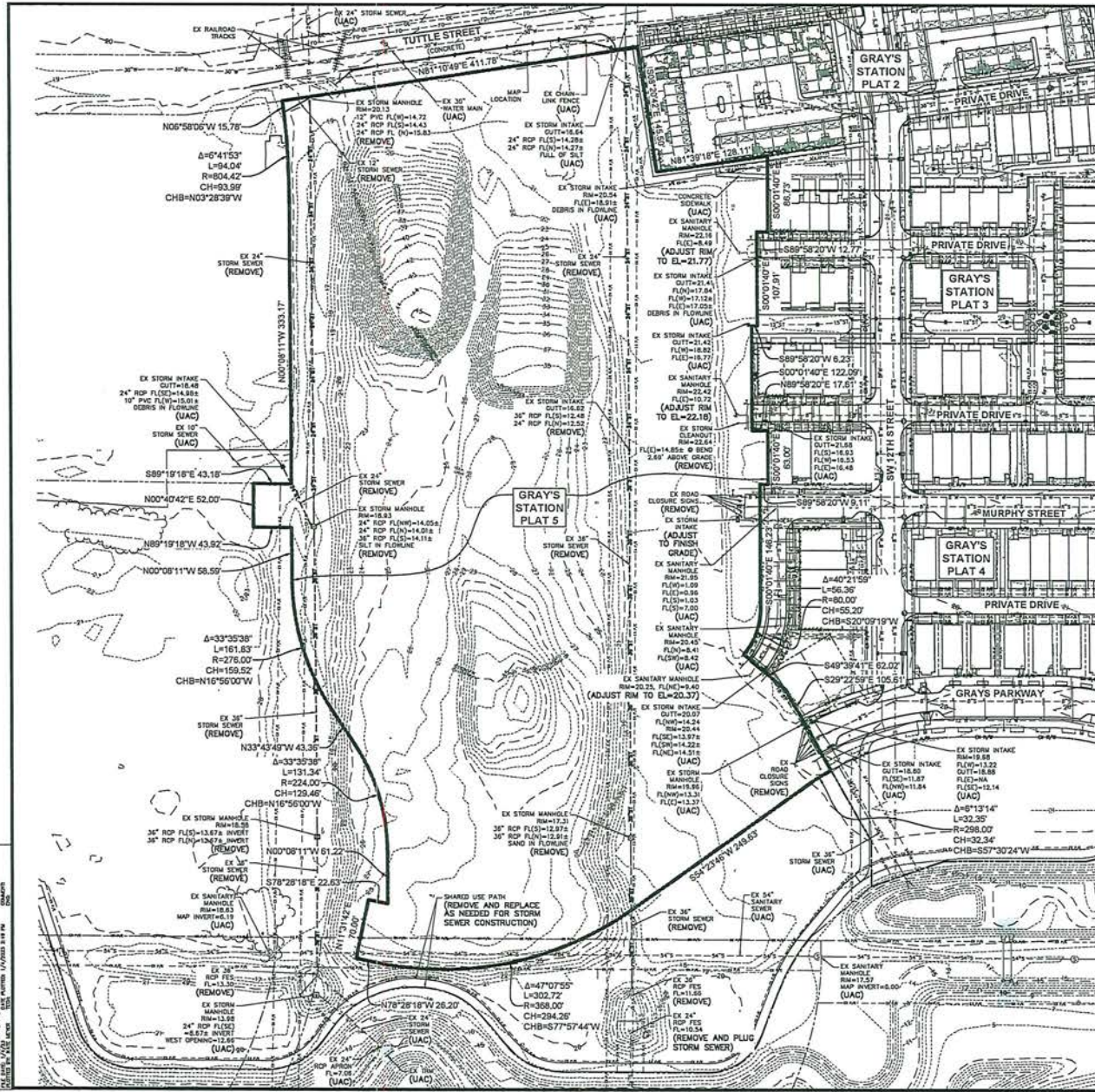
APPROVED  APPROVED WITH CONDITIONS - SEE EXHIBIT "A"

IN ACCORDANCE WITH SECTION 135-9, 2019 DES MOINES MUNICIPAL CODE, AS AMENDED.

NO CHANGES TO THIS PLAN UNLESS APPROVED IN WRITING FROM THE DEVELOPMENT SERVICES DIRECTOR.

DEVELOPMENT SERVICES DIRECTOR DATE





### DEMOLITION NOTES

1. PRIOR TO ANY WORK AT THE SITE, CONTRACTOR SHALL EXAMINE ANY APPLICABLE DRAWINGS AVAILABLE FROM THE OWNER OR ENGINEER AND CONSULT WITH OWNER'S PERSONNEL AND UTILITY COMPANY REPRESENTATIVES. NO COMPENSATION WILL BE ALLOWED FOR DAMAGE FROM FAILURE TO COMPLY WITH THIS REQUIREMENT.
2. PRIOR TO DEMOLITION, CONTRACTOR SHALL NOTIFY IN WRITING (48 HRS NOTICE) THE FOLLOWING:
  - CITY
  - APPROPRIATE UTILITY COMPANIES
  - OWNER
  - EXC. DESIGN ADVANTAGE ARCHITECT
3. DO NOT REMOVE EXISTING UTILITIES UNLESS OTHERWISE NOTED. COORDINATE REMOVAL OR ABANDONMENT OF ALL UTILITIES WITH THE APPROPRIATE UTILITY SUPPLIER AND REGULATORY AGENCIES.
4. PROTECT EXISTING UTILITIES THAT ARE TO REMAIN. THE LOCATIONS OF ALL UTILITIES INDICATED ON THE PLANS ARE TAKEN FROM EXISTING RECORDS. THE EXACT LOCATION AND ELEVATION OF ALL UTILITIES MUST BE DETERMINED BY THE CONTRACTOR. IT SHALL BE THE DUTY OF THE CONTRACTOR TO ASCERTAIN WHETHER ANY ADDITIONAL FACILITIES OTHER THAN THOSE SHOWN ON THE PLAN MAY BE PRESENT.
5. BACKFILL ALL EXCAVATIONS WITH CONCRETE MATERIAL COMPACTED TO 95% MAXIMUM STANDARD PROCTOR DENSITY AND MOISTURE RATIO OF OPTIMUM MOISTURE TO 4% ABOVE OPTIMUM MOISTURE. TESTING OF BACKFILL TO BE BY THE CONTRACTOR. BACKFILL ALL EXCAVATIONS WITH CONCRETE MATERIAL COMPACTED TO 95% MAXIMUM STANDARD PROCTOR DENSITY AND MOISTURE RATIO OF OPTIMUM MOISTURE TO 4% ABOVE OPTIMUM MOISTURE. TESTING OF BACKFILL TO BE BY THE CONTRACTOR.
6. FIELD VERIFY EXISTING GRADES AND LOCATION OF EXISTING UTILITIES, CONDUIT, LINES, POLES, TREES, FENCING, BUILDING AND OTHER SITE FEATURES PRIOR TO DEMOLITION AND IMMEDIATELY NOTIFY THE ENGINEER AND/OR OWNER OF ANY DISCREPANCIES.
7. DEMOLITION NOTES AS SHOWN ON THE PLAN ARE NOT ALL INCLUSIVE. CONTRACTOR TO ABANDON IN PLACE OR REMOVE AND DISPOSE OF ALL EXISTING SITE IMPROVEMENTS ABOVE AND BELOW GROUND TO COMPLY WITH THE GENERAL INTENT OF THIS DOCUMENT.
8. ALL CONSTRUCTION/DEMOLITION DEBRIS SHALL BE DISPOSED OF OFFSITE IN FULL COMPLIANCE WITH CURRENT ENVIRONMENTAL REGULATIONS.
9. CONSTRUCTION LIMITS SHALL BE CONFINED TO THE SITE BOUNDARY AS NOTED. ANY DAMAGE TO PROPERTIES OUTSIDE THE SITE BOUNDARY SHALL BE AT THE SOLE RESPONSIBILITY OF THE CONTRACTOR.
10. ALL WORK SHALL BE IN ACCORDANCE WITH OSHA STANDARDS. NOTICES INDICATED ON THE DRAWINGS SHALL RELIEVE THE CONTRACTOR FROM COMPLYING WITH ANY APPROPRIATE SAFETY REGULATIONS.

### TRAFFIC CONTROL NOTES

1. ALL APPLICABLE CITY PERMITS, INCLUDING BUT NOT LIMITED TO CLOSURE PERMITS, SHALL BE OBTAINED PRIOR TO ANY CONSTRUCTION WITHIN CITY LIMITS OR LAKE CLOSURES.
2. ALL TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
3. TEMPORARY SIGNING THAT CONVEYS A MESSAGE CONTRARY TO THE MESSAGE OF TEMPORARY SIGNING AND NOT APPLICABLE TO THE WORKING CONDITIONS SHALL BE COVERED BY THE CONTRACTOR WHEN DIRECTED BY THE CITY.
4. CONTRACTORS SHALL COORDINATE ITS TRAFFIC CONTROL WITH OTHER CONSTRUCTION PROJECTS IN THE AREA.
5. SCHEDULED CLOSURE SIGNS REQUIRED FOR ALL SIDEWALK CLOSURES, SIGNAGE AND TEMPORARY PEDIESTRIAN ACCESS ROUTE THROUGH THE CONSTRUCTION SHALL MEET THE REQUIREMENTS OF PUBLIC RIGHT-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG), SECTION 5020 AND OSHA DOT DESIGN MANUAL, CHAPTER 12A.
6. THE CONTRACTOR IS CAUTIONED NOT TO DISTURB OR REMOVE ANY EXISTING PAVEMENT, NOR TO DISTURB THE EXISTING TRAFFIC PATTERNS MORE THAN IS NECESSARY FOR THE PROPER DURATION OF THE WORK.
7. ALL SIGNING AND LAKE STRIPPING WILL NEED TO COMPLY WITH MUTCD, MAINTENANCE AND REPLACEMENT OF THE SIGNING AND STRIPPING WILL BE THE RESPONSIBILITY OF THE APPLICANT.

DATE	REVISIONS	RECORDS SUBMITTAL	DATE

4121 NW URBANDALE DRIVE  
URBANDALE, IA 50322  
PHONE: (515) 365-4400

TECH: ENGINEER: CIVIL DESIGN ADVANTAGE

**GRAY'S STATION PLAT 5**  
TOPOGRAPHIC SURVEY / DEMOLITION PLAN

DES. MOINES, IOWA

**C1.1**  
2202-113

NOTE: ALL PUBLIC IMPROVEMENTS, INCLUDING SPOT ELEVATIONS, ARE SHOWN FOR REFERENCE ONLY. REFER TO THE SEPARATE PRIVATE CONSTRUCTION CONTRACT DOCUMENTS FOR PUBLIC IMPROVEMENT PLANS. IF THERE ARE ANY DISCREPANCIES BETWEEN THE DEVELOPMENT PLAN AND THE PUBLIC IMPROVEMENT PLANS THE DETAILED PUBLIC IMPROVEMENT PLANS SHALL GOVERN.





NOTE: ALL PUBLIC IMPROVEMENTS, INCLUDING STREET LIGHTING, ARE SHOWN FOR REFERENCE ONLY. REFER TO THE SEPARATE PRIVATE CONSTRUCTION CONTRACT DOCUMENTS FOR FURTHER INFORMATION REGARDING THE REQUIREMENTS FOR PUBLIC IMPROVEMENTS. THE DEVELOPMENT PLAN AND THE PUBLIC IMPROVEMENT PLANS SHALL CONTROL. THE DETAIL PUBLIC IMPROVEMENT PLANS SHALL CONTROL.

PLAT	PRODUCT NUMBER	LOT NUMBER	COLOR PACKAGE NUMBER
5	1	51-53	1
5	2	54-56	1
5	3	57-59	1
5	4	60-62	1
5	5	63-65	1
5	6	66-68	1
5	7	69-71	1
5	8	72-74	1
5	9	75-77	1
5	10	78-80	1
5	11	81-83	1
5	12	84-86	1
5	13	87-89	1
5	14	90-92	1
5	15	93-95	1
5	16	96-98	1
5	17	99-101	1
5	18	102-104	1
5	19	105-107	1
5	20	108-110	1
5	21	111-113	1
5	22	114-116	1
5	23	117-119	1
5	24	120-122	1
5	25	123-125	1
5	26	126-128	1
5	27	129-131	1
5	28	132-134	1
5	29	135-137	1
5	30	138-140	1
5	31	141-143	1
5	32	144-146	1
5	33	147-149	1
5	34	150-152	1
5	35	153-155	1
5	36	156-158	1
5	37	159-161	1
5	38	162-164	1
5	39	165-167	1
5	40	168-170	1
5	41	171-173	1
5	42	174-176	1
5	43	177-179	1
5	44	180-182	1
5	45	183-185	1
5	46	186-188	1
5	47	189-191	1
5	48	192-194	1
5	49	195-197	1
5	50	198-200	1

PLAT	PRODUCT NUMBER	LOT NUMBER	COLOR PACKAGE NUMBER
5	1	51-53	1
5	2	54-56	1
5	3	57-59	1
5	4	60-62	1
5	5	63-65	1
5	6	66-68	1
5	7	69-71	1
5	8	72-74	1
5	9	75-77	1
5	10	78-80	1
5	11	81-83	1
5	12	84-86	1
5	13	87-89	1
5	14	90-92	1
5	15	93-95	1
5	16	96-98	1
5	17	99-101	1
5	18	102-104	1
5	19	105-107	1
5	20	108-110	1
5	21	111-113	1
5	22	114-116	1
5	23	117-119	1
5	24	120-122	1
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5	26	126-128	1
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5	31	141-143	1
5	32	144-146	1
5	33	147-149	1
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5	38	162-164	1
5	39	165-167	1
5	40	168-170	1
5	41	171-173	1
5	42	174-176	1
5	43	177-179	1
5	44	180-182	1
5	45	183-185	1
5	46	186-188	1
5	47	189-191	1
5	48	192-194	1
5	49	195-197	1
5	50	198-200	1

PLAT	PRODUCT NUMBER	LOT NUMBER	COLOR PACKAGE NUMBER
5	1	51-53	1
5	2	54-56	1
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5	4	60-62	1
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5	6	66-68	1
5	7	69-71	1
5	8	72-74	1
5	9	75-77	1
5	10	78-80	1
5	11	81-83	1
5	12	84-86	1
5	13	87-89	1
5	14	90-92	1
5	15	93-95	1
5	16	96-98	1
5	17	99-101	1
5	18	102-104	1
5	19	105-107	1
5	20	108-110	1
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5	22	114-116	1
5	23	117-119	1
5	24	120-122	1
5	25	123-125	1
5	26	126-128	1
5	27	129-131	1
5	28	132-134	1
5	29	135-137	1
5	30	138-140	1
5	31	141-143	1
5	32	144-146	1
5	33	147-149	1
5	34	150-152	1
5	35	153-155	1
5	36	156-158	1
5	37	159-161	1
5	38	162-164	1
5	39	165-167	1
5	40	168-170	1
5	41	171-173	1
5	42	174-176	1
5	43	177-179	1
5	44	180-182	1
5	45	183-185	1
5	46	186-188	1
5	47	189-191	1
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5	49	195-197	1
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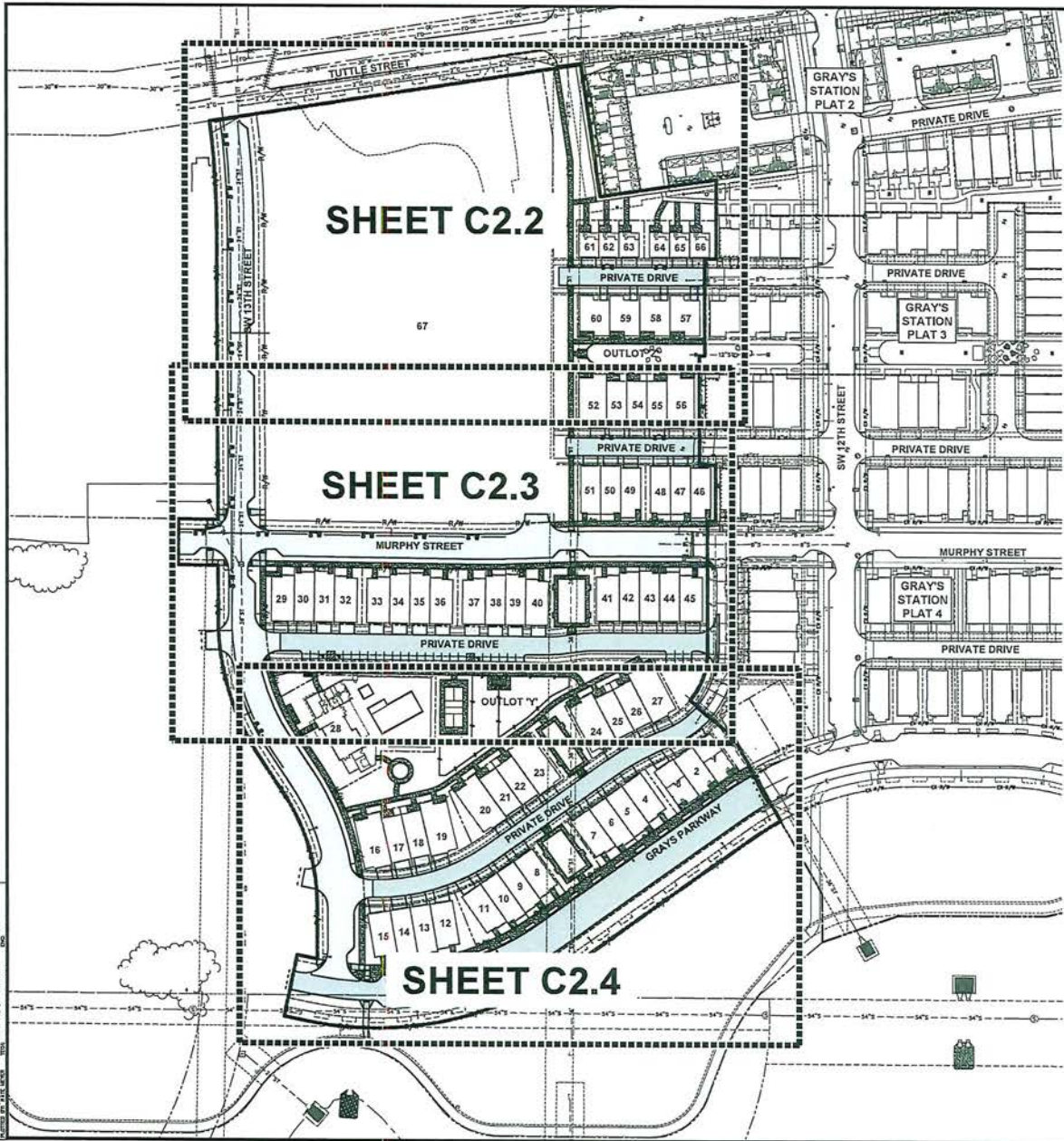
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5	14	90-92	1
5	15	93-95	1
5	16	96-98	1
5	17	99-101	1
5	18	102-104	1
5	19	105-107	1
5	20	108-110	1
5	21	111-113	1
5	22	114-116	1
5	23	117-119	1
5	24	120-122	1
5	25	123-125	1
5	26	126-128	1
5	27	129-131	1
5	28	132-134	1
5	29	135-137	1
5	30	138-140	1
5	31	141-143	1
5	32	144-146	1
5	33	147-149	1
5	34	150-152	1
5	35	153-155	1
5	36	156-158	1
5	37	159-161	1
5	38	162-164	1
5	39	165-167	1
5	40	168-170	1
5	41	171-173	1
5	42	174-176	1
5	43	177-179	1
5	44	180-182	1
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5	25	123-125	1
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5	29	135-137	1
5	30	138-140	1
5	31	141-143	1
5	32	144-146	1
5	33	147-149	1
5	34	150-152	1
5	35	153-155	1
5	36	156-158	1
5	37	159-161	1
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5	29	135-137	1
5	30	138-140	1
5	31	141-143	1
5	32	144-146	1
5	33	147-149	1
5	34	150-152	1
5	35	153-155	1
5	36	156-158	1
5	37	159-161	1
5	38	162-164	1
5	39	165-167	1
5	40	168-170	1
5	41	171-173	1
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5	19	105-107	1
5	20	108-110	1
5	21	111-113	1
5	22	114-116	1
5	23	117-119	1
5	24	120-122	1
5	25	123-125	1
5	26	126-128	1
5	27	129-131	1
5			





**GENERAL NOTES**

1. THE 2022 EDITION OF THE URBA DESIGN STANDARD SPECIFICATIONS AND ALL CITY SUPPLEMENTALS, IF APPLICABLE, SHALL APPLY TO ALL WORK ON THIS PROJECT UNLESS OTHERWISE NOTED.
2. ALL WORK SHALL COMPLY WITH ADA ACCESSIBILITY GUIDELINES FOR BUILDINGS.
3. ALL WORK SHALL BE IN ACCORDANCE WITH OSHA CODES AND STANDARDS.
4. PRIOR TO ANY WORK AT THE SITE, CONTRACTOR SHALL EXAMINE ANY APPLICABLE SAFETY REGULATIONS, ORDINANCES, AND/OR CITY ORDINANCES AND CONSULT WITH THE OWNER, ENGINEER, AND/OR ARCHITECT AND CONSULT WITH OWNER'S PERSONNEL AND UTILITY COMPANY REPRESENTATIVES. NO CONSTRUCTION WILL BE ALLOWED FOR CHARGE FROM FAILURE TO COMPLY WITH THIS REQUIREMENT.
5. PRIOR TO CONSTRUCTION WITHIN CITY BLOCK OR ANY CONNECTION TO PUBLIC SERVICES CONTRACTOR SHALL NOTIFY THE CITY'S CONSTRUCTION DIVISION, CONTRACTOR SHALL NOTIFY CITY OF DES MOINES ENGINEERING DEPT. AT LEAST 48 HOURS PRIOR TO COMMENCEMENT OF CONSTRUCTION TO COORDINATE INSPECTIONS. CALL 515-281-4177 OR 515-209-4177.
6. ALL CONSTRUCTION WITHIN PUBLIC BLOCK/ EASEMENTS, AND/OR ANY CONNECTION TO PUBLIC SERVICES AND ERECTIONS SHALL COMPLY WITH THE CITY'S STANDARDS. NO STOPS ARE ALLOWED IN SANITARY MAINS.
7. ALL ERECTIONS ARE TO BACK OF CURB, BUILDING FACE OR PROPERTY LINE UNLESS OTHERWISE NOTED.
8. CONTRACTOR TO VERIFY BUILDING DIMENSIONS WITH ARCHITECTURAL PLANS.
9. PLACE 3/4 INCH EXPANSION JOINT BETWEEN ALL P.C.C. PAVEMENT/SIDEWALKS P.C.C. PAVEMENT.
10. REMOVE ALL CURBS SPALLED INTO BLOCK AT THE END OF EACH WORK DAY.
11. ALL PROPERTY PINS SHALL BE PROTECTED FROM CRUISING OR OTHER OPERATIONS. ANY PINS DISTURBED SHALL BE RESET AT THE CONTRACTOR'S EXPENSE.
12. DO NOT STORE CONSTRUCTION MATERIALS AND EQUIPMENT IN THE RIGHT OF WAY.
13. THE CONTRACTOR SHALL NOT DISTURB DESIRABLE GRADE AREAS AND DEGRADABLE AREAS OUTSIDE THE CONSTRUCTION LIMITS. THE CONTRACTOR WILL NOT BE PERMITTED TO PARK OR SERVICE VEHICLES AND EQUIPMENT OR USE THESE AREAS FOR STORAGE OF MATERIALS AND STORAGE, PARKING AND SERVICE AREAS.
14. THE CONTRACTOR IS RESPONSIBLE FOR REPLACING ANY AREAS OF PAVEMENT OR EQUIPMENT ON THE PAVEMENT THAT IS DAMAGED DUE TO OPERATING.
15. THE CONTRACTOR SHALL MAINTAIN TEMPORARY WARNING SERVICES AND SAFETY FENCE AT CERTAIN LOCATIONS WHERE TOP ADJACENT PROPERTIES ARE THE GENERAL CONTRACTOR, AS DIRECTED BY THE ENGINEER OR THE CITY.
16. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE COORDINATION OF BILLING STAMPING OF CONSTRUCTION DETAILS.
17. THE CONTRACTOR SHALL NOTIFY THE CITY'S CONSTRUCTION DIVISION WITHIN NEAREST TRANSVERSE JOINT CONTRACTOR SHALL NOTIFY REMOVAL LIMITS WITH CITY'S CONSTRUCTION INSPECTOR PRIOR TO ANY CONCRETE REMOVAL.
18. ALL ERECTIONS AND ADDITIONAL DECKS REQUIRED TO COMPLETE THE WORK SHALL BE SCREENED BY THE CONTRACTOR.
19. CONTRACTOR SHALL APPLY FOR SPECIAL PARKING PERMIT IN ADVANCE OF WORK TO SCREENED PARKING METER REMOVAL.
20. ALL WORK WITHIN THE CITY RIGHT-OF-WAY REQUIRES A RIGHT-OF-WAY PERMIT.
21. CONTRACTOR SHALL APPLY FOR A SIDEWALK/DRIVEWAY APPROACH PERMIT IN ADVANCE OF WORK.

**CITY OF DES MOINES STANDARD NOTES**

1. ANY AMENDMENTS OR CHANGES TO THE PROJECT SITE THAT DO NOT MEET WHAT IS SHOWN ON THE SITE PLAN NEED TO BE APPROVED WITH THE PERMIT AND DEVELOPMENT CENTER PRIOR TO INSTALLATION/CONSTRUCTION.
2. LIGHTING SHALL CONSIST OF LOW-GLARE CUT OFF TYPE FIXTURES TO REDUCE THE RISK OF LIGHT POLLUTION ON SURROUNDING PROPERTIES.
3. THE REQUIRED LANDSCAPING, SOFT DESIGN AND PROPOSED, SHALL BE MAINTAINED FOR THE LIFE OF THE CERTIFICATE OF OCCUPANCY OR EQUIVALENT OF ZONING COMPLIANCE.
4. ALL EXISTING AREAS SHALL BE RESTORED BY SETTING OR SOONER.
5. THIS SITE SHALL BE MAINTAINED IN COMPLIANCE WITH ALL CITY CODES APPLICABLE ON THE DATE OF SITE PLAN APPROVAL.
6. ANY GROUND-MOUNTED OR WALL-MOUNTED UTILITIES AND METERS SHALL BE SCREENED AND NOT VISIBLE FROM A PUBLIC STREET.
7. ANY WALL-MOUNTED UTILITIES SHALL BE SCREENED ON ALL SIDES AND FINISHED TO MATCH BUILDING.
8. ALL UTILITIES SHALL BE UNDERGROUND.
9. ALL ROOFER MECHANICAL EQUIPMENT MUST BE SCREENED ON ALL SIDES WITH ARCHITECTURAL SCREENING EQUAL TO THE HEIGHT OF THE EQUIPMENT.
10. MECHANICAL EQUIPMENT SHALL NOT BE LOCATED IN THE FRONT YARD OR STREET SIDE YARD AND SHALL BE SCREENED FROM VIEW FROM ANY PUBLIC WAY WITH LANDSCAPING, FENCING, OR WALLS CONSISTENT WITH THE BUILDING DESIGN, COLOR, AND MATERIALS.
11. NO ROCK WILL BE UTILIZED AS A GROUNDCOVER MATERIAL, PER CITY CODE SECTION 132-7.2.
12. FOR TOWNHOMES, INDIVIDUAL RESIDENTIAL TOTES FOR TRASH AND RECYCLING PICKUP WILL BE STORED INDOORS, AND PLACED AT THE CURB UPON DAY OF PICKUP.
13. ANY BUILDING OR SITE SIGNAGE SHALL CONFORM WITH THE PROVISIONS DESCRIBED IN THE GRAY'S STATION PUD.

**PAVEMENT THICKNESS**

1. SIDEWALKS 4" P.C.C.
2. CLASS 'A' SIDEWALK 5" P.C.C.
3. CYCLE TRACK 5" P.C.C.
4. PRIVATE DRIVES 6" P.C.C.
5. SIDEWALK RAMPS 8" P.C.C.
6. PUBLIC STREETS 7 & 8" P.C.C.

**GRAY'S STATION ADDRESSING**

PLAT	LOT NUMBER	ADDRESS NUMBER	STREET NAME
5	1	XXX	XXX
5	2		
5	3		
5	4		
5	5		
5	6		
5	7		
5	8		
5	9		
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5	61		
5	62		
5	63		
5	64		
5	65		
5	66		
5	67		

NOTE: ALL PUBLIC IMPROVEMENTS, INCLUDING SPOT ELEVATIONS, ARE SHOWN FOR REFERENCE ONLY. REFER TO THE SEPARATE PRIVATE CONSTRUCTION CONTRACT DOCUMENTS FOR PUBLIC IMPROVEMENT PLANS. IF THERE ARE ANY DISCREPANCIES BETWEEN THE DEVELOPMENT PLAN AND THE PUBLIC IMPROVEMENT PLANS THE DETAILED PUBLIC IMPROVEMENT PLANS SHALL GOVERN.

DATE: \_\_\_\_\_  
 REVISIONS: \_\_\_\_\_  
 SECOND SUBMITTAL: \_\_\_\_\_  
 FIRST SUBMITTAL: \_\_\_\_\_  
 OUT OF DATE: \_\_\_\_\_

4121 NW URBANDALE DRIVE  
 URBANDALE, IA 50322  
 PHONE: (515) 899-4400

TECH: \_\_\_\_\_  
 ENGINEER: \_\_\_\_\_  
 CIVIL DESIGN ADVANTAGE

**GRAY'S STATION PLAT 5**  
 DIMENSION PLAN

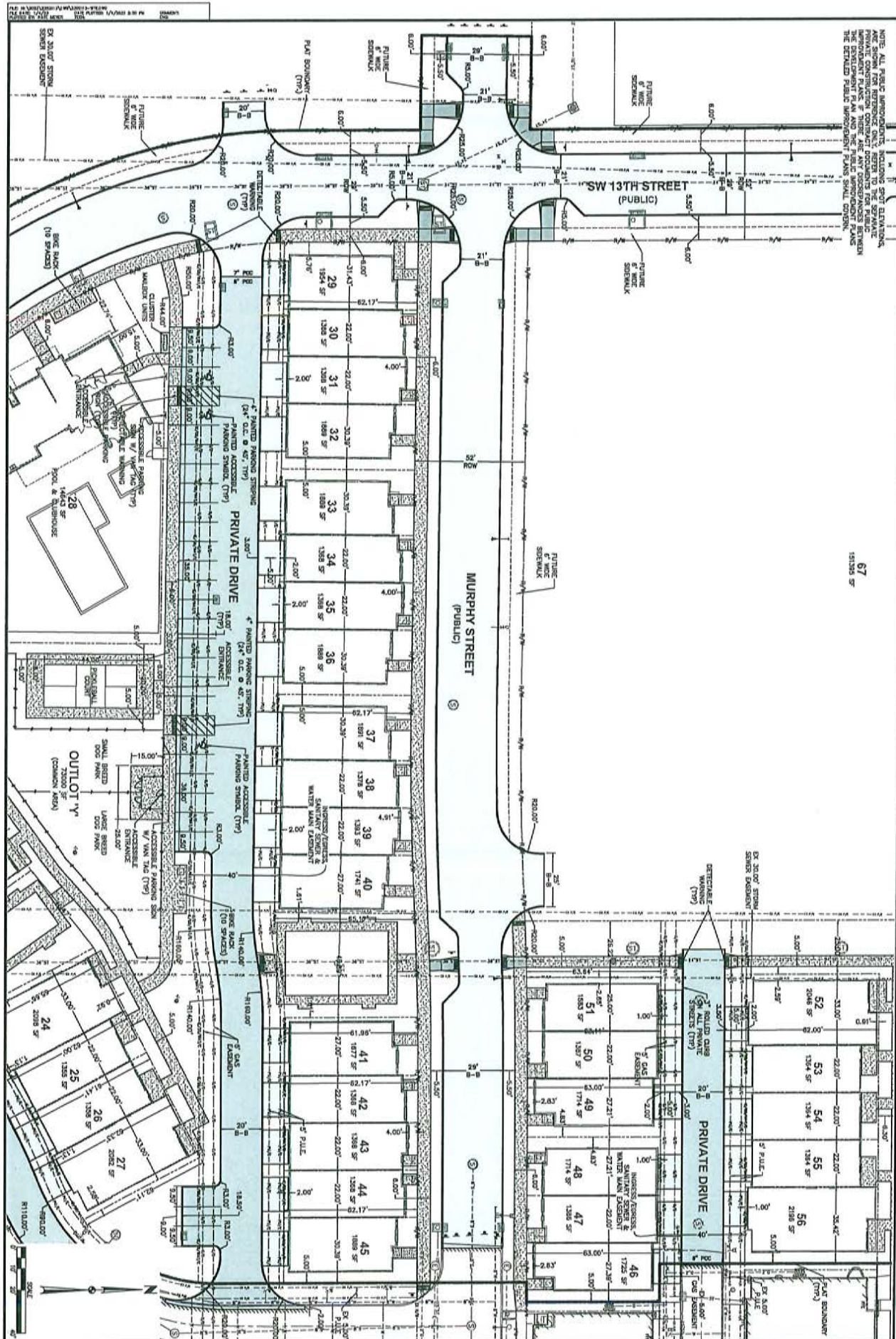
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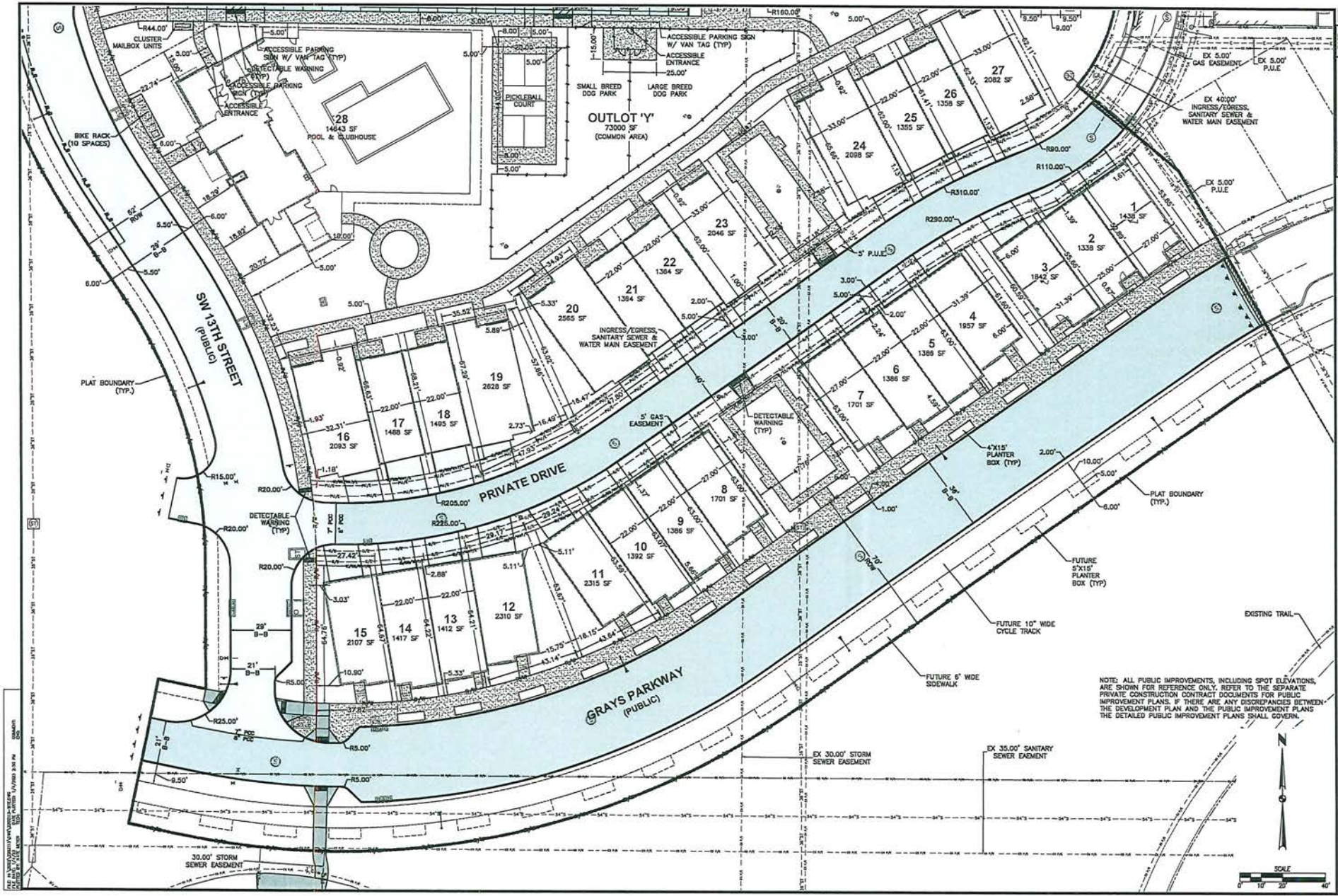
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67  
151265 SF

<b>C2.3</b> 2202-113	<b>GRAY'S STATION PLAT 5</b> DIMENSION PLAN	 CIVIL DESIGN ADVANTAGE	4121 NW URBANDALE DRIVE URBANDALE, IA 50322 PHONE: (515) 369-4400	REVISIONS	DATE
				SECOND SUBMITTAL FIRST SUBMITTAL	01/09/2022 10/19/2022
DES MOINES, IOWA			ENGINEER:	TECH:	





NOTE: ALL PUBLIC IMPROVEMENTS, INCLUDING SPOT ELEVATIONS, ARE SHOWN FOR REFERENCE ONLY. REFER TO THE SEPARATE PRIVATE CONSTRUCTION CONTRACT DOCUMENTS FOR PUBLIC IMPROVEMENT PLANS. IF THERE ARE ANY DISCREPANCIES BETWEEN THE DEVELOPMENT PLAN AND THE PUBLIC IMPROVEMENT PLANS, THE DETAILED PUBLIC IMPROVEMENT PLANS SHALL GOVERN.



DATE	
REVISIONS	
SECOND SUBMITTAL	03/09/2023
FIRST SUBMITTAL	04/10/2022

4121 NW URBANDALE DRIVE  
 URBANDALE, IA 50322  
 PHONE: (515) 969-4400



**GRAY'S STATION PLAT 5**  
**DIMENSION PLAN**

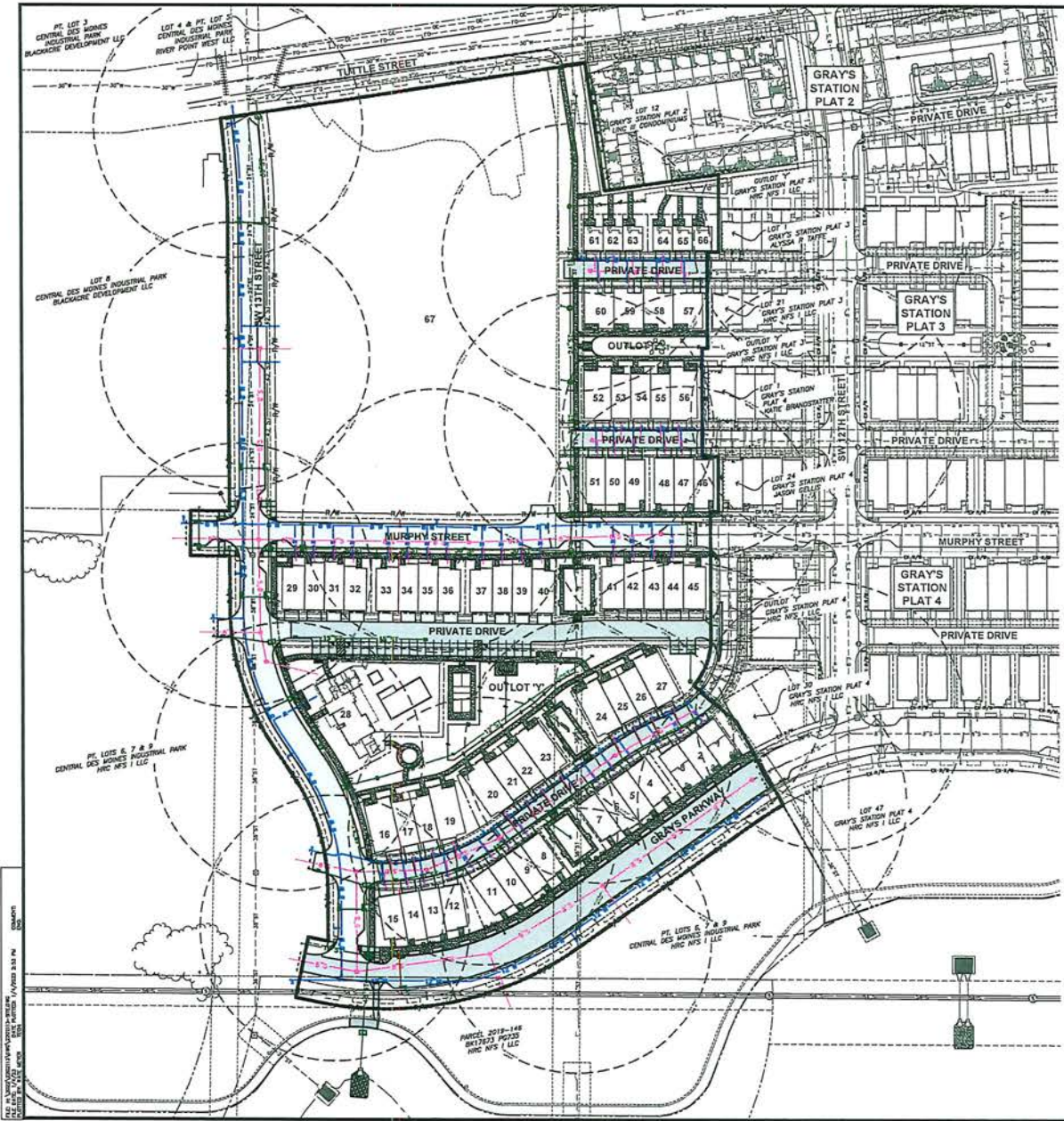
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DES MOINES, IOWA  
 ENGINEER: [Signature]  
 TECH: [Signature]









NOTE: ALL PUBLIC IMPROVEMENTS, INCLUDING SPOT ELEVATIONS, ARE SHOWN FOR REFERENCE ONLY. REFER TO THE SEPARATE PRIVATE CONSTRUCTION CONTRACT DOCUMENTS FOR PUBLIC IMPROVEMENT PLANS. IF THERE ARE ANY DISCREPANCIES BETWEEN THE DEVELOPMENT PLAN AND THE PUBLIC IMPROVEMENT PLANS THE DETAILED PUBLIC IMPROVEMENT PLANS SHALL GOVERN.

DESIGNED BY: [Name] DATE: [Date]  
 CHECKED BY: [Name] DATE: [Date]  
 DRAWN BY: [Name] DATE: [Date]

**GRAY'S STATION PLAT 5**

OVERALL UTILITY PLAN

CIVIL DESIGN ADVANTAGE

DES MOINES, IOWA

4121 NW URBANDALE DRIVE  
 URBANDALE, IA 50322  
 PHONE: (515) 385-4660

TECH: [Name]  
 ENGINEER: [Name]

REVISIONS

DATE	DESCRIPTION

SECOND SUBMITTAL  
 # 01/22/23

**C4.0**

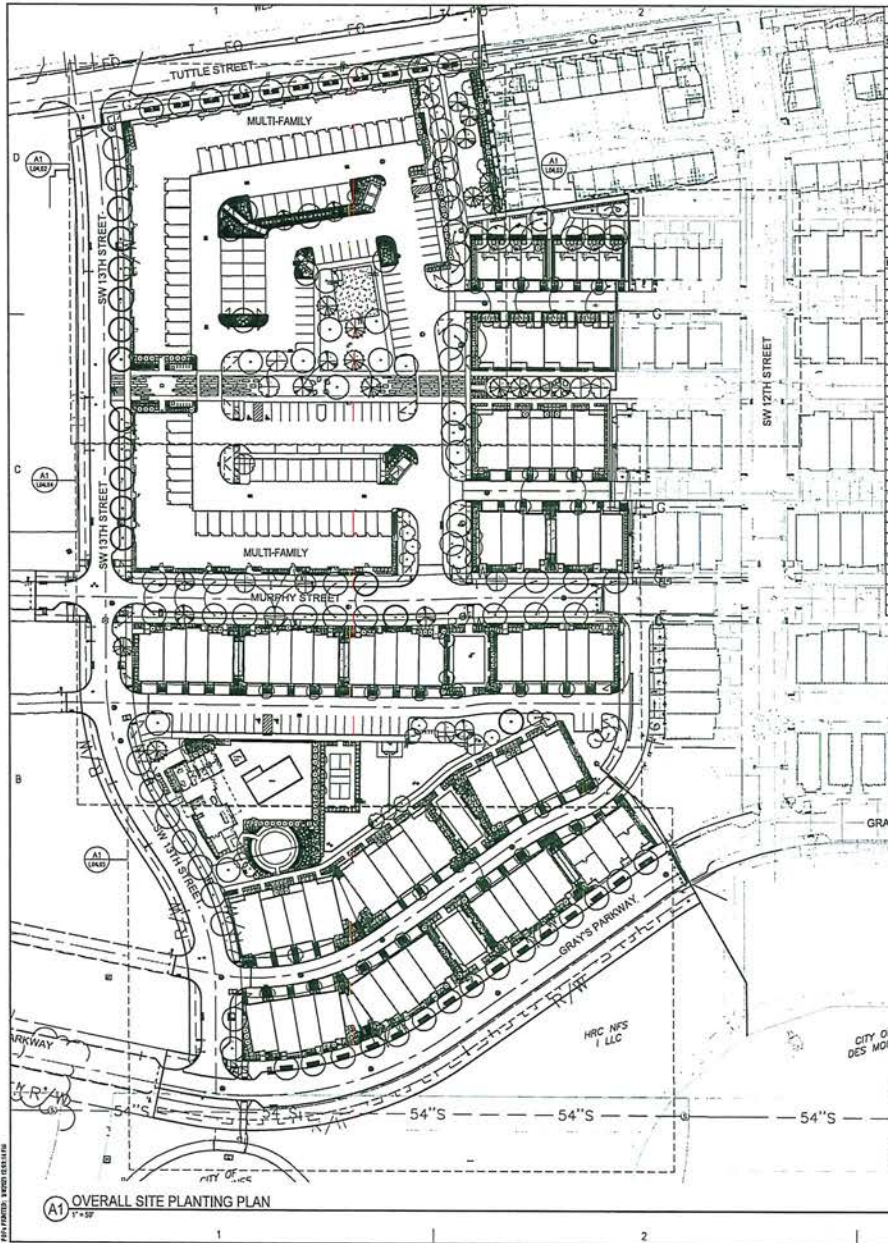
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DATE: [Date]









PERCENTAGE	QTY.	ABBR.	COMMON NAME	BOTANICAL NAME	SIZE	NOTES
<b>PLANT SCHEDULE</b>						
<b>SHADE TREES</b>						
7.53%	11	CO	Hackberry	<i>Celtis occidentalis</i>	2"	B&B
8.22%	13	GB	Maxillaria Tree	<i>Gingko biloba 'Golden Colonel'</i>	2"	B&B
9.22%	13	GO	Kentucky Coffeetree	<i>Gymnocladia dioica 'L'esper'</i>	2"	B&B
7.53%	11	CT	Crabapple	<i>Crataegus mollis 'Silver Queen'</i>	2"	B&B
8.22%	13	PA	London Planetree	<i>Platanus x acerifolia 'London Planetree'</i>	2"	B&B
8.22%	13	TD	Island Cypress	<i>Taxodium distichum</i>	2"	B&B
8.91%	13	OS	Red Oak	<i>Quercus rubra</i>	2"	B&B
8.22%	13	OM	Bur Oak	<i>Quercus macrocarpa</i>	2"	B&B
8.91%	13	AR	Red Maple	<i>Acer rubrum</i>	2"	B&B

<b>UNDERSTORY TREES</b>						
16.04%	10	CF	Flowering Dogwood	<i>Cornus florida</i>	1.5"	B&B
17.04%	10	CA	Common Anemone	<i>Anemone nemorosa</i>	1.5"	B&B
8.31%	8	CC	Common Redbud	<i>Amorpha canescens</i>	1.5"	B&B
8.31%	8	MI	Prairie Coneflower	<i>Rudbeckia hirta</i>	1.5"	B&B
15.98%	10	AG	Amelanchier canadensis	<i>Amelanchier canadensis</i>	1.5"	2 or 5 Stress, B&B
17.04%	10	AG	Azulena Brilliance Sweetgum	<i>Liquidambar styraciflua 'Aulom'</i>	1.5"	2 or 5 Stress, B&B
7.45%	7	KP	Goldenrain Tree	<i>Koeleria paniculata</i>	1.5"	B&B
7.45%	7	SR	Japanese Tree Lilac	<i>Syringa reticulata 'Hana no Yuki'</i>	1.5"	B&B

<b>EVERGREEN TREES</b>						
8.92%	13	PS	Noany Spruce	<i>Picea abies</i>	6"	B&B
8.92%	13	PS	White Pine	<i>Pinus strobus</i>	6"	B&B
7.53%	11	PF	Lumber Pine	<i>Pinus taeda 'Vanderwolf's Pyramid'</i>	6"	B&B

<b>TYPE ONE MIX</b>						
400	IV	Little Henry Sweetgum Shrub	<i>Liquidambar styraciflua 'Little Henry'</i>	5 gal	CONT.	
400	JS	Calgary Carpet Juniper	<i>Juniperus sibirica 'Calgary Carpet'</i>	5 gal	CONT.	
400	SO	Indolenziani Conifer	<i>Symphoricarpos obtusilobus 'Indolenziani'</i>	5 gal	CONT.	
400	BS	Blue Grama	<i>Bouteloua gracilis</i>	1 gal	CONT.	
300	CA	Red Fescue Feathered Grass	<i>Calamagrostis x acutifolia 'Karl Foerster'</i>	1 gal	CONT.	
450	SH	Prairie Dropseed	<i>Sporobolus heterolepis</i>	1 gal	CONT.	
420	AM	Bloodstain Yellow	<i>Achillea Millefolium</i>	1 gal	CONT.	
360	HO	French Hosta	<i>Hosta s. 'Frances'</i>	1 gal	CONT.	
360	VM	Perennial	<i>Veronica Minor 'Cousins'</i>	1 gal	CONT.	

<b>TYPE TWO MIX</b>						
300	SSB	Little Bluestem 'The Blues'	<i>Schizachyrium scoparium 'The Blues'</i>	1 gal	CONT.	
440	HS	Happy Returns Daylily	<i>Hemerocallis 'Happy Returns'</i>	1 gal	CONT.	
375	NS	Walters Low Cabinet	<i>Nepenthes 'Walters Low Cabinet'</i>	1 gal	CONT.	
415	SB	Blue HS Salvia	<i>Salvia x hybrid 'Blue Hill'</i>	1 gal	CONT.	

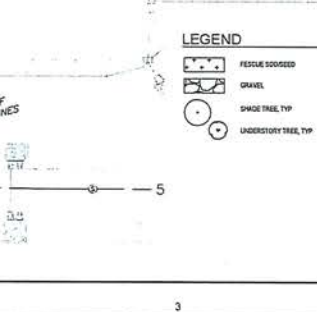
<b>TYPE THREE MIX</b>						
360	EG	Side Oaks Grama	<i>Bouteloua curtipendula</i>	1 gal	CONT.	
420	CA	Northern Saw Oats Grama	<i>Chenopodium millidatum</i>	1 gal	CONT.	
450	MS	Heaven Grass	<i>Urochloa tenax 'Crescendo'</i>	1 gal	CONT.	
420	PV	Shenandoah Sweetgrass	<i>Panicum umgatum 'Shenandoah'</i>	1 gal	CONT.	

<b>TYPE FOUR MIX</b>						
50	CS	Red Twig Dogwood	<i>Cornus sericea 'Randy'</i>	5 gal	CONT.	
50	RA	Rock Rose	<i>Rosa serotina 'Rock Rose'</i>	5 gal	CONT.	
50	VC	Koningspice Viburnum	<i>Viburnum cerasifolium</i>	5 gal	CONT.	

<b>SOD/SEED</b>						
Turf Type Fescue						

Plat 1 Area, including P&W	49973 sq		
Plat 1 Area, less P&W	36183 sq		
Outlet 2 Area	13147 sq		
Outlet 3 Area	14231 sq		
<b>Outlet 2</b>	<b>City</b>	<b>Factor</b>	<b>Total</b>
Overlaid Street Tree	35	1800	70000
Overlaid Interlor Tree	60	1800	60000
Overlaid Tree	37	1800	23960
Overlaid Tree	27	300	8100
<b>Outlet 2 Total Canopy Coverage</b>	<b>percentage</b>	<b>132360</b>	<b>66.54%</b>
<b>Outlet 3</b>	<b>City</b>	<b>Factor</b>	<b>Total</b>
Overlaid Street Tree	35	1800	70000
Overlaid Interlor Tree	3	1800	3000
Overlaid Tree	40	300	43000
Overlaid Tree	5	300	3750
<b>Outlet 3 Total Canopy Coverage</b>	<b>percentage</b>	<b>123250</b>	<b>53.57%</b>
<b>Total Plat Coverage, Less B.O.V.C.</b>	<b>percentage</b>	<b>259300</b>	<b>57.45%</b>
<b>Total Plat Coverage, including B.O.V.C.</b>	<b>percentage</b>	<b>413760</b>	<b>43.76%</b>

- GENERAL PLANTING PLAN NOTES**
- QUANTITIES SHOWN IN THE PLANT SCHEDULE ARE FOR INFORMATION ONLY. THE DRAWING SHALL PREVAIL IF A CONFLICT OCCURS.
  - FOR UNDERGROUND UTILITIES LOCATED CALL DOWN ONE CALL AT 811. 48 HOUR NOTICE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL UNDERGROUND LOCAGES NOT COVERED BY THE DIGGERS HOTLINE SYSTEM. NO COMPENSATION FOR DAMAGES TO EXISTING UTILITIES WILL BE ALLOWED RESULTING FROM FAILURE TO COMPLY WITH THIS REQUIREMENT.
  - THE LANDSCAPE CONTRACTOR SHALL PROTECT ALL UTILITIES, STRUCTURES AND SITE IMPROVEMENTS. ANY DAMAGE SHALL BE REPAIRED BY THE LANDSCAPE CONTRACTOR AT NO COST TO THE OWNER.
  - FRESH GRADES IN ALL LANDSCAPE AREAS ARE TO BE REVIEWED BY THE LANDSCAPE ARCHITECT PRIOR TO ANY SEEDING, SOODING, OR PLANTING. THE CONTRACTOR SHALL SUBMIT A WRITTEN REQUEST FOR REVIEW FIVE (5) DAYS PRIOR TO THE DESIRED DATE OF REVIEW. NO COMPENSATION FOR RE-SEEDING, RE-SOODING, OR RE-PLANTING OF AN AREA REQUIRING REMEDIAL GRADING SHALL BE ALLOWED RESULTING FROM FAILURE TO COMPLY WITH THIS REQUIREMENT.
  - INSTALL SOD IN THE AREAS INDICATED ON THE PLAN.
  - PRIOR TO SEEDING OR SOODING, VERIFY THAT SOIL ADJACENT TO PAVEMENTS AND CURBS ARE AT THE CORRECT ELEVATION TO ASSURE A LEVEL TRANSITION FROM THE PAVEMENT TO THE LAWN AREAS. NO COMPENSATION FOR REMOVAL AND REPLACEMENT OF INSTALLED LAWN, DUE TO UNACCEPTABLE TRANSITION FROM ONE SURFACE TO THE OTHER (I.E. SOD TOO HIGH OR TOO LOW), WILL BE ALLOWED RESULTING FROM FAILURE TO COMPLY WITH THIS REQUIREMENT.
  - SEED ALL OTHER DISTURBED AREAS DUE TO CONSTRUCTION OPERATIONS.
  - PRIOR TO ANY EXCAVATION AT THE SITE, THE CONTRACTOR SHALL EXAMINE ANY APPLICABLE DRAWINGS AVAILABLE FROM THE OWNER, ARCHITECT, LANDSCAPE ARCHITECT, AND/OR CIVIL ENGINEER AND, CONSULT WITH ALL UTILITY COMPANY REPRESENTATIVES TO DETERMINE POSSIBLE UTILITY LOCATIONS.
  - NOTIFY THE LANDSCAPE ARCHITECT IN WRITING FIVE (5) DAYS PRIOR TO THE ANTICIPATED DATE FOR REVIEW OF THE PLANT LOCATION STAKING. REVIEW TO BE REQUESTED PRIOR TO EXCAVATION OF THE PLANTING PITS.
  - ALL DECIDUOUS TREES SHALL BE PLANTED 5 FEET OR MORE FROM ANY R.O.W. AND ALL CONIFEROUS TREES AT LEAST 10 FEET AWAY, UNLESS INDICATED OTHERWISE ON THE DRAWING.
  - ALL SOIL AND OTHER DEBRIS SPILLED OR TRACKED ONTO THE PUBLIC R.O.W. OR ADJACENT PROPERTIES SHALL BE CLEANED UP BY THE CONTRACTOR AT THE END OF EACH WORK DAY.
  - PLANT LABELS AND TAGS SHALL BE LEFT ON THE PLANT MATERIAL UNTIL AFTER THE INITIAL INSPECTION BY THE LANDSCAPE ARCHITECT.
  - PRUNE ALL EXISTING TREES THAT REMAIN AS FOLLOWS:
    - REMOVE BRUSH LIMBS
    - REMOVE CROSSING LIMBS
    - REMOVE ROOT SPROUTS & SUCKERS
    - ANY MULTISTEMMED TREES - REMOVE STEMS LESS THAN 2" DIAMETER
    - ALL DECIDUOUS TREES - ELEVATE TREE LIMBS TO ALLOW 10' HT. CLEAR.
    - SHARP TREE TO ITS CHARACTERISTIC FORM
  - ALL SOD AREAS TO BE IRRIGATED; SEE SPECIFICATIONS.
  - CONTACT THE MUNICIPAL FORESTRY DIVISION PRIOR TO PLANTING AT 516.283.450.
  - ALL WIRE, TWINE, AND BURLAP SHALL BE REMOVED FROM THE ROOTBALL OF TREES PRIOR TO PLANTING.



OVERALL SITE PLANTING PLAN  
1"=50'

DESIGN DEVELOPMENT

PRELIMINARY - NOT FOR CONSTRUCTION

GRAY'S STATION PLAT 5

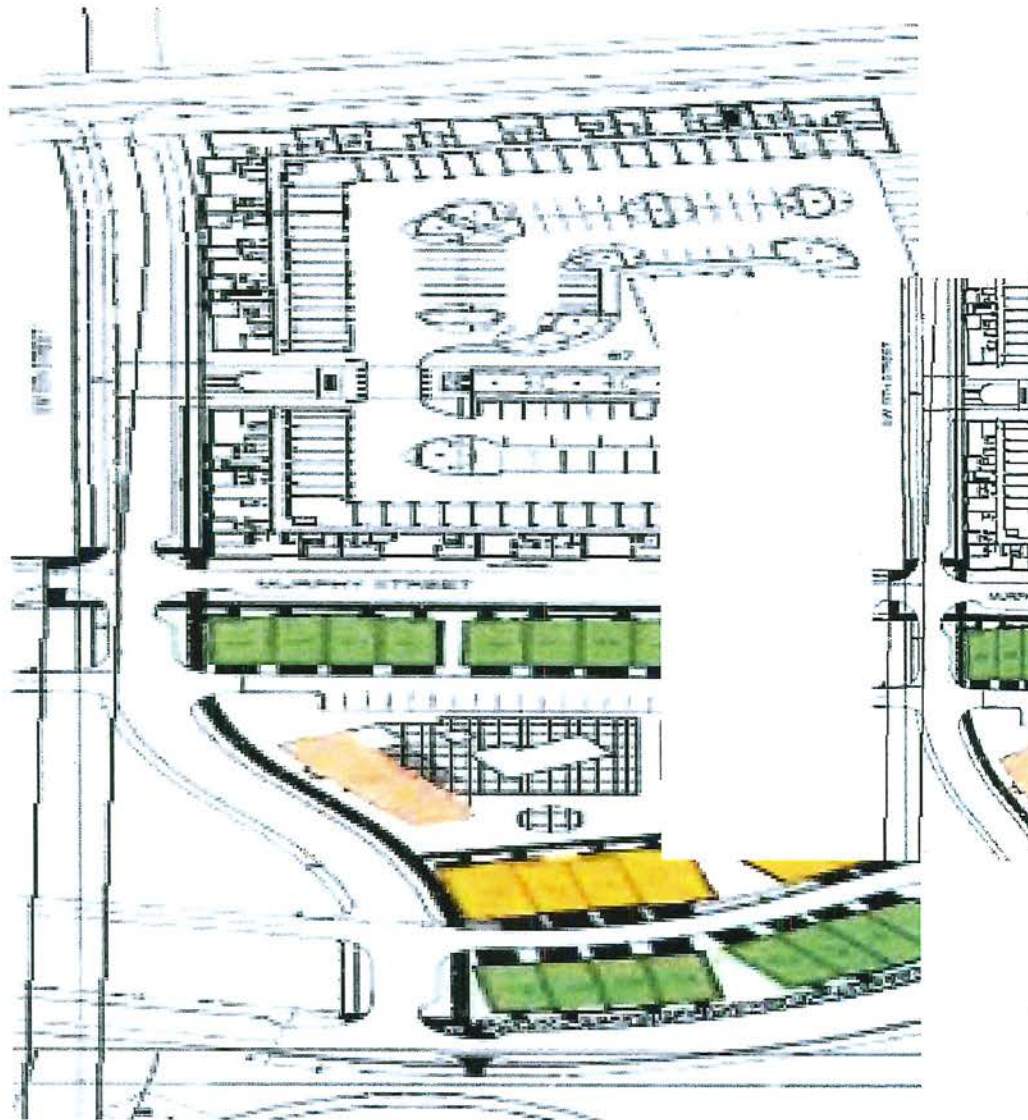


HUBBELL REALTY  
REPLAN

DATE	REVISION

SITE PLANTING PLAN  
L04.01









NEW FRONT ELEVATION



OLD FRONT ELEVATION

## Elevation Updates in Phase II

In Phase 2 we are proposing the same Product types/units as were approved for Phase 1. The floorplans for each unit type will remain the same. We made some revisions to the Elevations and Exterior Color Packages as we felt that they needed to be updated for Phase II. Some of the elevations didn't have enough intentional variations and the color packages were too light in color for an urban neighborhood.

The following pages show the new and the old front elevations of each product type and a description of the changes.

### Product 1

The desire was to create a darker color package for this Product and add some variation between the units.

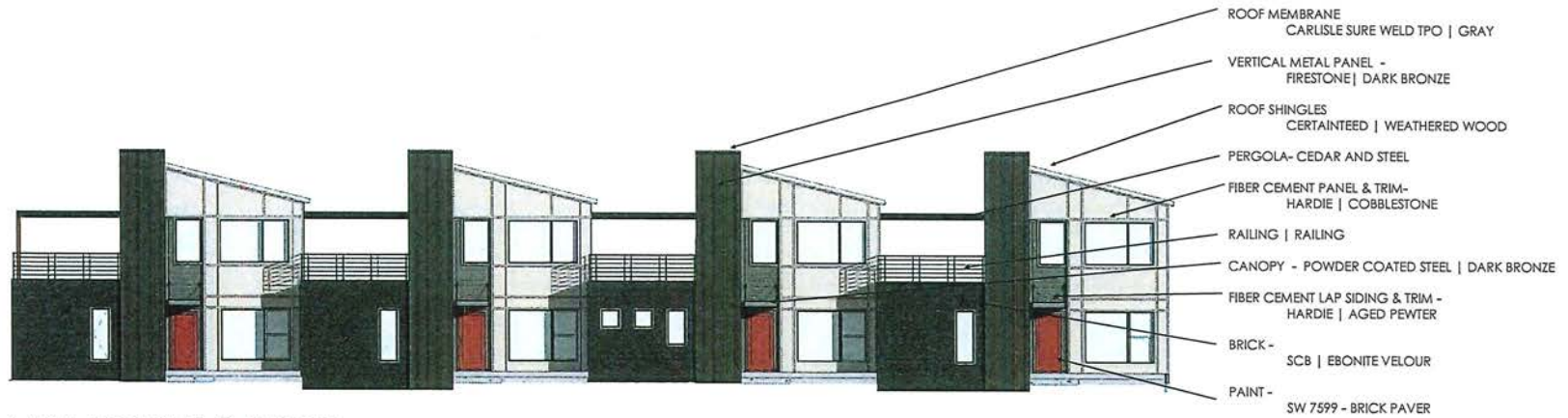
- Increased visual interest through varying heights of the entry element clad in metal panels.
- Revised the color scheme and made it darker to achieve a more urban look.

## PRODUCT 1 | COMPARISON

COLOR PACKAGE: Product 1—#1

GRAY'S STATION | 07.07.2022 | 3





NEW FRONT ELEVATION

**Product 3**

This elevation looked too busy and had too many design themes and variations.

- Revised vertical elements to all be the same color and revised material to dark metal panels with a vertical pattern.
- The cube-shaped building elements were revised from Hardie lap siding/stone to all stone, to unify the overall building design.



OLD FRONT ELEVATION

**PRODUCT 3 | COMPARISON**

COLOR PACKAGE: Product 3—#1

GRAY'S STATION | 07.07.2022 | 4



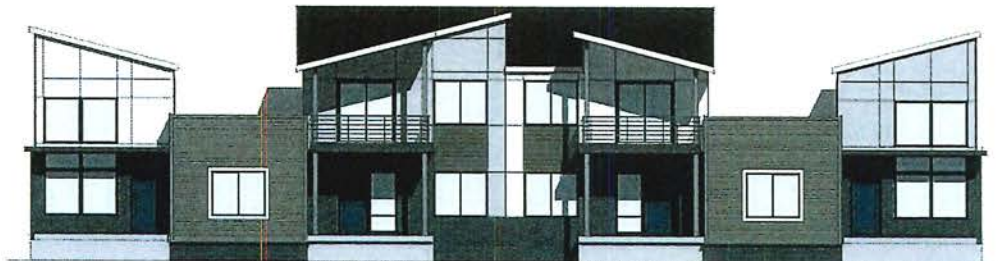


NEW FRONT ELEVATION

**Product 4**

This elevation was too busy and the color scheme too light for this urban environment.

- Changed to a darker color scheme to achieve a more urban look.
- Changed Hardie lap siding on front to metal panels.
- Eliminated the white-colored Hardie panels and matched the panel color on both sides to achieve a unified appearance.
- Steel metal posts will be painted dark gray.



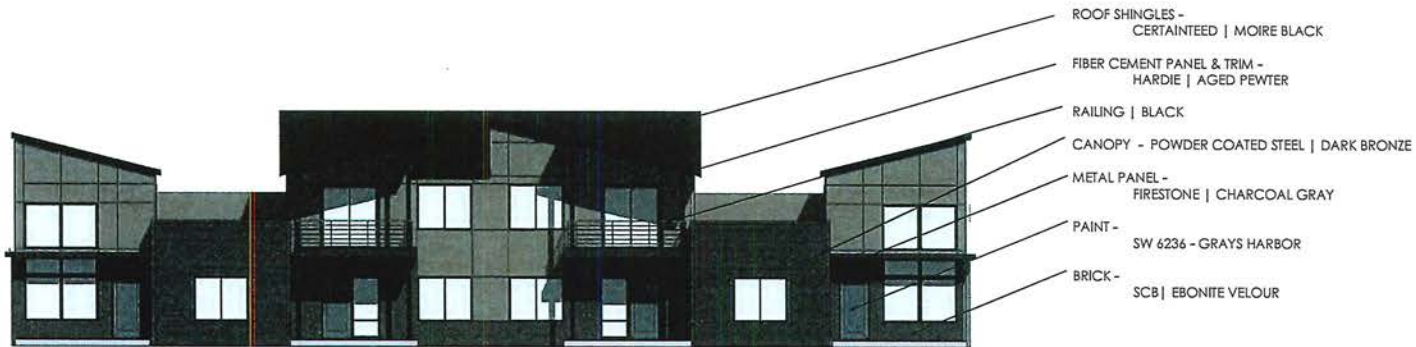
OLD FRONT ELEVATION

**PRODUCT 4 | COMPARISON**

COLOR PACKAGE: Product 4—#1

GRAY'S STATION | 07.07.2022 | 5





NEW FRONT ELEVATION

**Product 4**

This elevation was too busy and the color scheme too light for this urban environment.

- Changed to a darker color scheme to achieve a more urban look.
- Changed Hardie lap siding on front to metal panels.
- Eliminated the white-colored Hardie panels and matched the panel color on both sides to achieve a unified appearance.
- Steel metal posts will be painted dark gray.



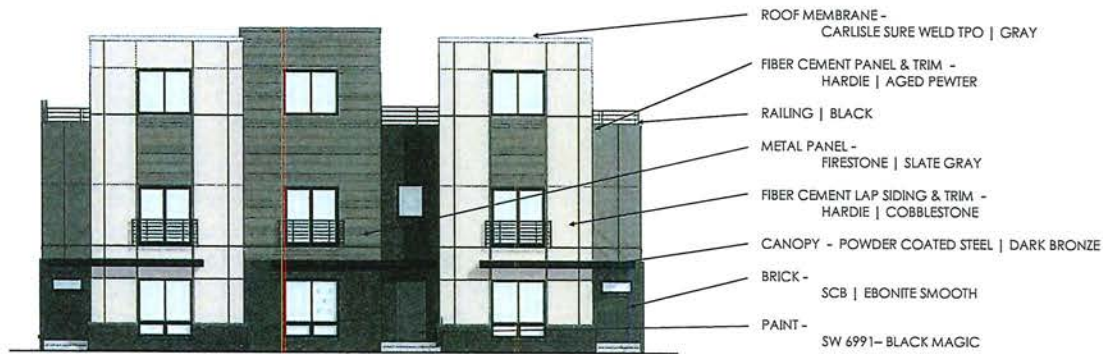
OLD FRONT ELEVATION

PRODUCT 4 | COMPARISON

COLOR PACKAGE: Product 4—#2

GRAY'S STATION | 07.07.2022 | 6





NEW FRONT ELEVATION



OLD FRONT ELEVATION

### Product 5

Elevations appear repetitive and uninspired. We only built one Product 5 building in Phase 1 so far. It was built without the 3<sup>rd</sup> level optional bonus room. We intend to build the bonus room on Product 5 in Phase 2.

- Varied the height of the brick to create diverse building elements and to reduce the effect of the stark vertical building elements.
- Replaced the Hardie panels of one of the main building elements with metal panels.
- Black windows

## PRODUCT 5 | COMPARISON

COLOR PACKAGE: Product 5—#1

GRAY'S STATION | 07.07.2022 | 7





NEW FRONT ELEVATION



OLD FRONT ELEVATION

### Product 6

Elevation appeared boring and repetitive. These needed additional visual interest and a more urban look.

- Varied the height of the brick/stone to create diverse building elements and to reduce the repetitive effect of the vertical building elements.
- Introduced building elements sided in metal panels with a vertical pattern.
- Replaced the Hardie panels of one or more of the main building elements with metal panels (horizontal pattern).
- Black windows
- Darkened the color packages

## PRODUCT 6 | COMPARISON

COLOR PACKAGE: Product 6—#1

GRAY'S STATION | 07.07.2022 | 8





- ROOF MEMBRANE - CARLISLE SURE WELD TPO | GRAY
- FIBER CEMENT PANEL & TRIM - HARDIE | AGED PEWTER
- RAILING | BLACK
- METAL PANEL - FIRESTONE | DARK BRONZE
- VERTICAL METAL PANEL - FIRESTONE | SLATE GRAY
- CANOPY - POWDER COATED STEEL | DARK BRONZE
- BRICK - SCB | MOUNTAIN SHADOW SMOOTH
- PAINT - SW 6991- BLACK MAGIC

NEW FRONT ELEVATION



OLD FRONT ELEVATION

### Product 6

Elevation appeared boring and repetitive. These needed additional visual interest and a more urban look.

- Varied the height of the brick/stone to create diverse building elements and to reduce the repetitive effect of the vertical building elements.
- Introduced building elements sided in metal panels with a vertical pattern.
- Replaced the Hardie panels of one or more of the main building elements with metal panels (horizontal pattern).
- Black windows
- Darkened the color packages

## PRODUCT 6 | COMPARISON

COLOR PACKAGE: Product 6—#2

GRAY'S STATION | 07.07.2022 | 9





NEW FRONT ELEVATION



OLD FRONT ELEVATION

### Product 6

Elevation appeared boring and repetitive. These needed additional visual interest and a more urban look.

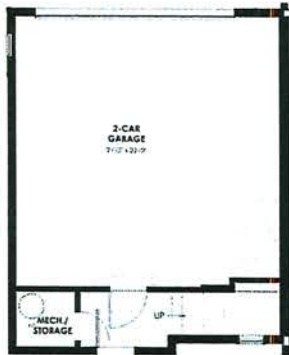
- Varied the height of the brick/stone to create diverse building elements and to reduce the repetitive effect of the vertical building elements.
- Introduced building elements sided in metal panels with a vertical pattern.
- Replaced the Hardie panels of one or more of the main building elements with metal panels (horizontal pattern).
- Black windows
- Darkened the color packages

## PRODUCT 6 | COMPARISON

COLOR PACKAGE: Product 6—#3

GRAY'S STATION | 07.07.2022 | 10





FIRST FLOOR



SECOND FLOOR



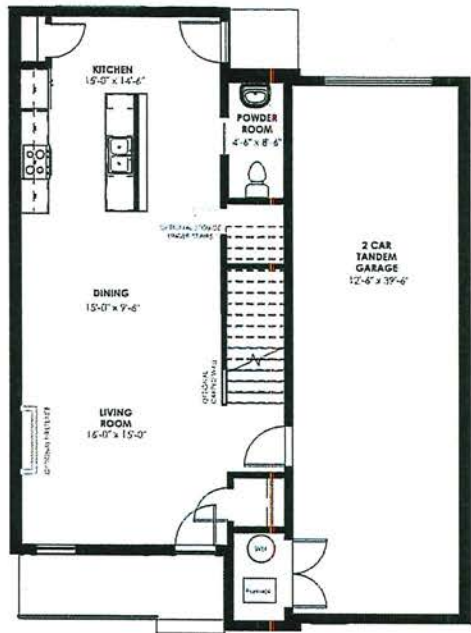
THIRD FLOOR

## PRODUCT 1 | PRESTON

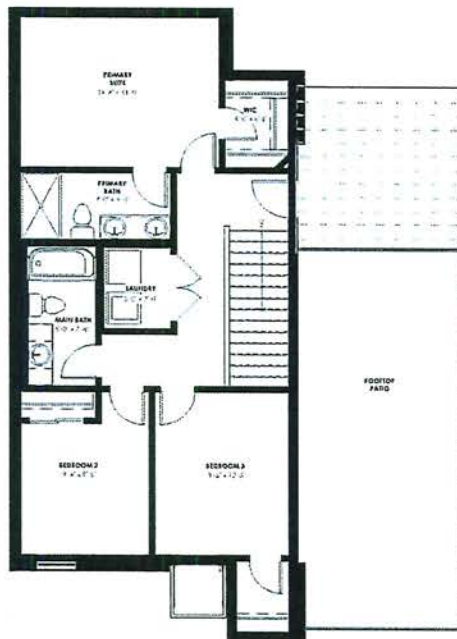
Lots: 61-63 and 64-66

GRAY'S STATION | 07.07.2022 | 45





FIRST FLOOR



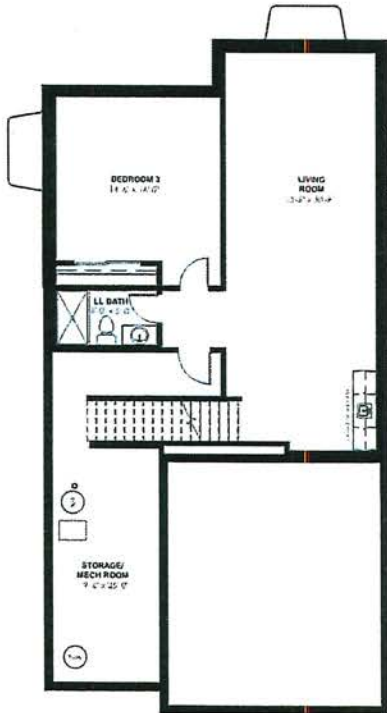
SECOND FLOOR

PRODUCT 3 | BEXLEY

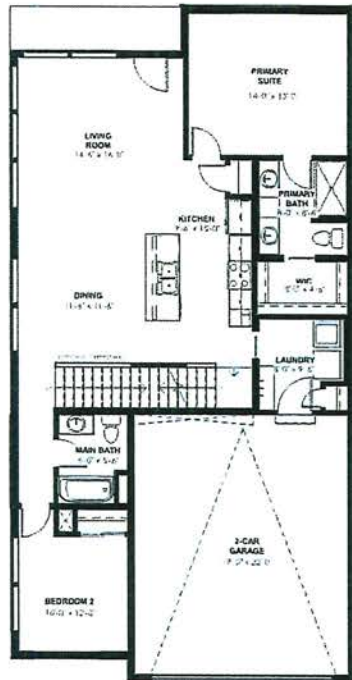
Lots: 57-60

GRAY'S STATION | 07.07.2022 | 46

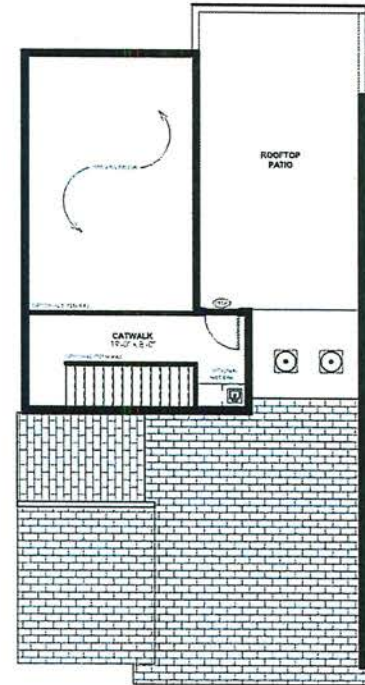




LOWER LEVEL

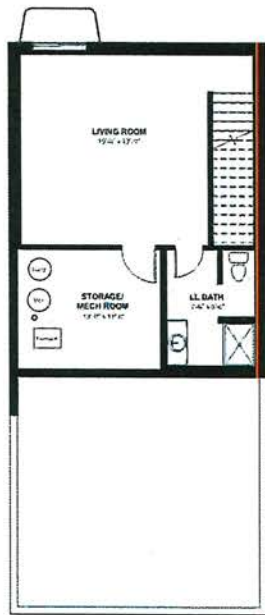


FIRST FLOOR



SECOND FLOOR

PRODUCT 4 | MAISON  
 Lots: 16&19, 20&23, 24&27 and 52&56  
 GRAY'S STATION | 07.07.2022 | 47



LOWER LEVEL



FIRST FLOOR



SECOND FLOOR

## PRODUCT 4 | MARLOW

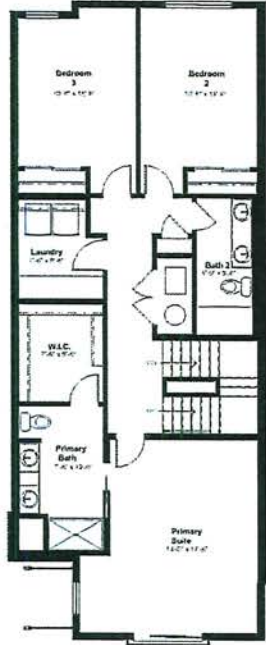
Lots: 17&18, 21&22, 25&26, and 53-55

GRAY'S STATION | 07.07.2022 | 48





FIRST FLOOR



SECOND FLOOR



THIRD FLOOR

PRODUCT 5 | CHAVA

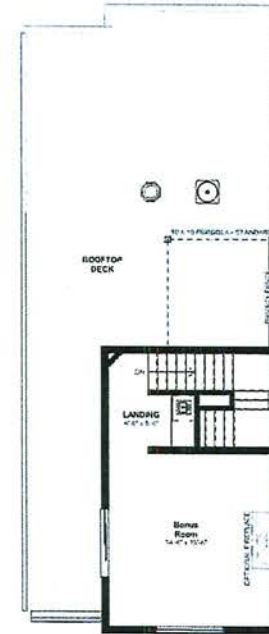
Lots: 46&48 and 49&51



FIRST FLOOR



SECOND FLOOR



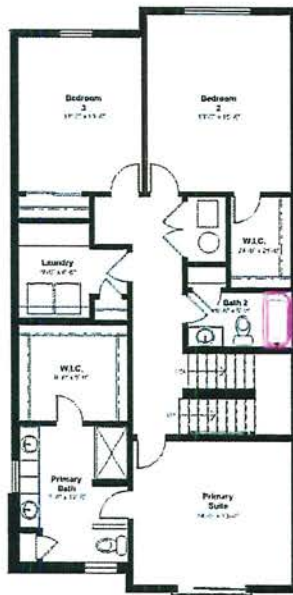
THIRD FLOOR

PRODUCT 5 | ASANA  
 Lots: 47 and 50  
 GRAY'S STATION | 07.07.2022 | 50

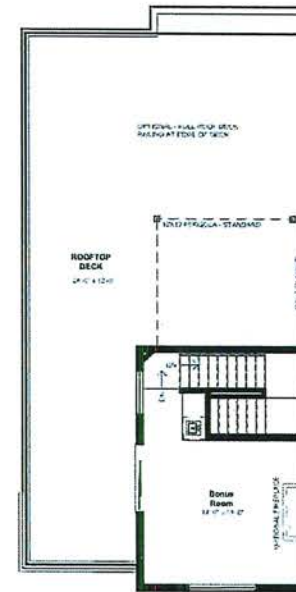




FIRST FLOOR



SECOND FLOOR



THIRD FLOOR

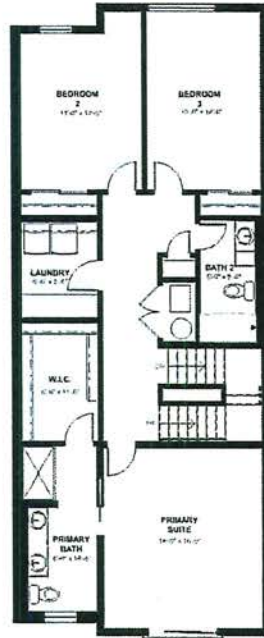
## PRODUCT 6 | GRAYSON

Lots: 1&3, 4&7, 8&11, 12&15, 29&32, 33&36, 37&40, and 41&45

GRAY'S STATION | 07.07.2022 | 51



FIRST FLOOR



SECOND FLOOR



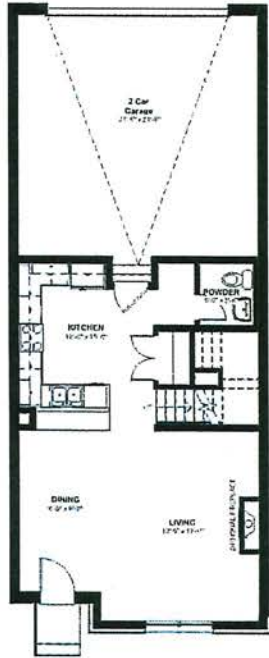
THIRD FLOOR

## PRODUCT 6 | CORDOVA

Lots: 2, 5, 9, 13, 30, 34, 38 and 42&44

GRAY'S STATION | 07.07.2022 | 52

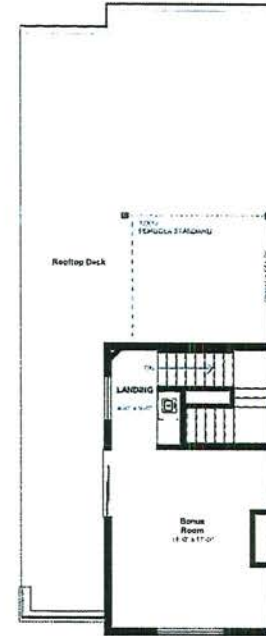




FIRST FLOOR



SECOND FLOOR



THIRD FLOOR

PRODUCT 6 | CORDOVA PLUS  
 Lots: 6, 10, 14, 31, 35, 39 and 43  
 GRAY'S STATION | 07.07.2022 | 53



Hubbell Homes  
Des Moines, Iowa

PERSPECTIVE

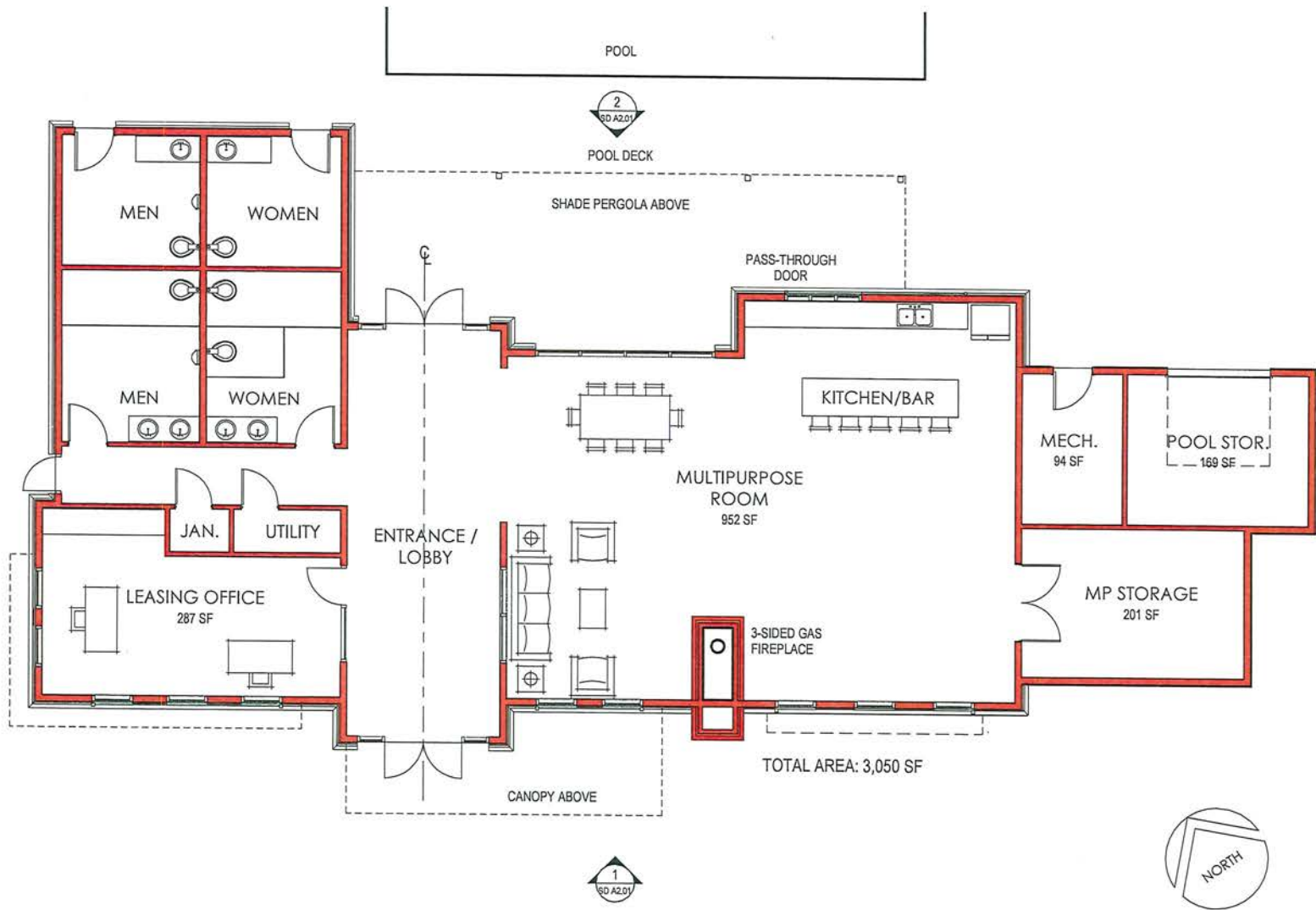
GRAY'S STATION CLUBHOUSE

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[AM220361]







Hubbell Homes  
Des Moines, Iowa

CONCEPTUAL FLOOR PLAN 7/14/2022

GRAY'S STATION CLUBHOUSE

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[AM220361]





1 SOUTH ELEVATION  
1/8" = 1'-0"



2 NORTH ELEVATION  
1/8" = 1'-0"

Hubbell Homes  
Des Moines, Iowa

ELEVATIONS

GRAY'S STATION CLUBHOUSE



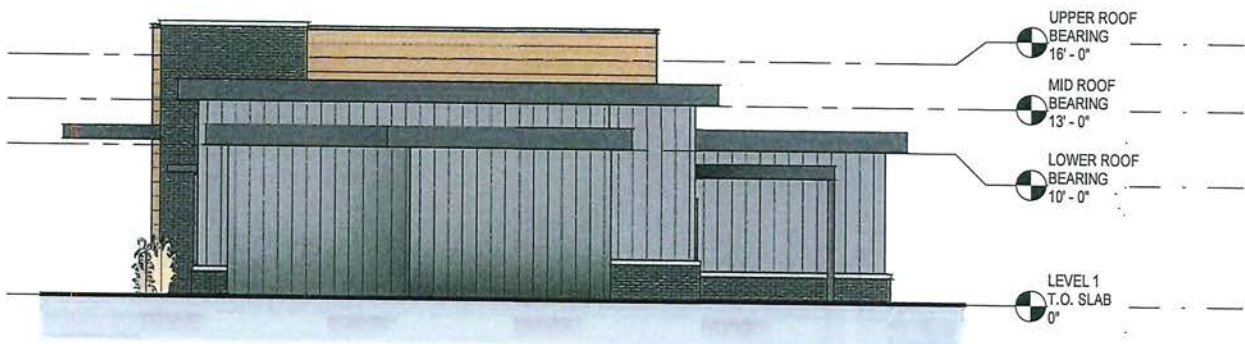
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1 WEST ELEVATION  
1/8" = 1'-0"



2 EAST ELEVATION  
1/8" = 1'-0"

ELEVATIONS

Hubbell Homes  
Des Moines, Iowa

GRAY'S STATION CLUBHOUSE

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